

PROJECT DESCRIPTION

THE PROJECT CONSISTS OF THE CONSTRUCTION OF A MULTI-USE PATH ALONG THE WEST SIDE OF DUBLIN ROAD FROM HERTFORD LANE TO RINGS ROAD. CONSTRUCTION OF THE MULTI-USE PATH WILL INCLUDE A PEDESTRIAN BRIDGE, DRIVE RECONSTRUCTION, AND THE RELOCATION OF STONE WALLS AND UTILITIES.

SPECIFICATIONS

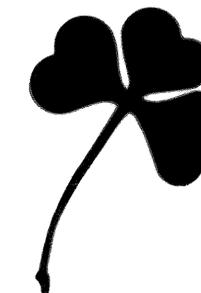
CITY OF COLUMBUS CONSTRUCTION AND MATERIALS SPECIFICATIONS (2012 EDITION) SHALL GOVERN ALL CONSTRUCTION ITEMS UNLESS OTHERWISE NOTED.

CITY OF DUBLIN, OHIO

DUBLIN ROAD SOUTH MULTI-USE PATH

HERTFORD LANE TO RINGS ROAD

08-009.2-CIP



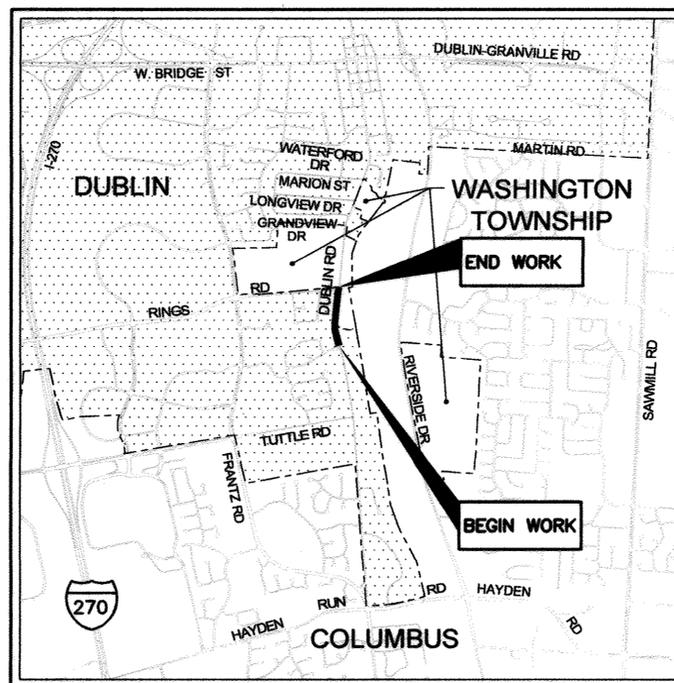
UNDERGROUND UTILITIES
 CONTACT BOTH SERVICES
 CALL TWO WORKING DAYS
BEFORE YOU DIG
 CALL 811 OR 1-800-362-2764
 OHIO OIL & GAS PRODUCERS PROTECTIVE
 SERVICE CALL: 1-800-925-0988

BENCHMARK (BASED ON NAVD 88)

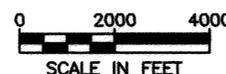
BM "A"	CHISELED SQUARE ON THE NW CORNER OF THE CONCRETE PAD OF THE TELEPHONE BOX, ON THE NORTH SIDE OF TUTTLE ROAD, BEING 82 FEET LEFT OF THE CENTERLINE OF DUBLIN ROAD RIGHT-OF-WAY. N: 757717.86 E: 1796648.55 ELEV = 835.53
BM "B"	P.K. NAIL IN THE WEST SIDE OF POWER POLE #178A12, IN FRONT OF RESIDENCE #5544, BEING 23 FEET RIGHT OF THE CENTERLINE OF DUBLIN ROAD RIGHT-OF-WAY. N: 758330.43 E: 1796650.68 ELEV = 828.82
BM "C"	NW CORNER OF A CONCRETE PAD AT THE NORTH END OF A GUARDRAIL, APPROXIMATELY 140 FEET SOUTH OF THE CENTERLINE OF HERTFORD LANE, BEING 15 FEET RIGHT OF THE CENTERLINE OF DUBLIN ROAD RIGHT-OF-WAY. N: 759043.42 E: 1796505.64 ELEV = 827.44
BM "D"	CHISELED SQUARE ON THE NORTHEAST WINGWALL OF THE FRANKLIN COUNTY ENGINEER CULVERT "WAS 10-10.04" FOR CRAMER DITCH, BEING 19 FEET RIGHT OF THE CENTERLINE OF DUBLIN ROAD RIGHT-OF-WAY. N: 759645.15 E: 1796376.49 ELEV = 829.40
BM "E"	FRANKLIN COUNTY ENGINEER BM R-3, BRASS PLUG ON THE CENTER OF THE WEST HEADWALL OF CULVERT "WAS-10-10.21", JUST NORTH OF RINGS ROAD, BEING 16 FEET LEFT OF THE CENTERLINE OF DUBLIN ROAD RIGHT-OF-WAY. N: 760484.58 E: 1796427.97 ELEV = 824.81
BM "F"	RAILROAD SPIKE IN THE EAST SIDE OF A POWER POLE, ON THE SOUTH SIDE OF GRANDVIEW DRIVE, BEING 32 FEET LEFT OF THE CENTERLINE OF DUBLIN ROAD RIGHT-OF-WAY. N: 761940.04 E: 1796600.84 ELEV = 808.60
BM "G"	CHISELED SQUARE ON THE SOUTHWEST WINGWALL OF A CULVERT ON THE NORTH SIDE OF MARION STREET, BEING 16 FEET LEFT OF THE CENTERLINE OF DUBLIN ROAD RIGHT-OF-WAY. N: 762753.43 E: 1796926.17 ELEV = 802.94
BM "H"	WEST BOLT OF FIRE HYDRANT ON THE SOUTH SIDE OF SHORT STREET, BEING 23 FEET RIGHT OF THE CENTERLINE OF DUBLIN ROAD RIGHT-OF-WAY. N: 763601.79 E: 1797058.12 ELEV = 815.26

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LOCATION MAP



PORTIONS TO BE IMPROVED _____

SUPPLEMENTAL PRINTS OF STANDARD CONSTRUCTION DRAWINGS

CITY OF DUBLIN	ODOT	CITY OF COLUMBUS
PD-02		1441
PD-03		2328
RD-01		
RD-06	TC-41.20 1-19-01	
	TC-42.20 1-21-11	
RD-07	MT-97.11 7-20-12	L-1001
	MT-101.60 7-20-12	L-6316A
RD-10		
ST-01		

LEGEND

	PROPOSED STORM SEWER, MANHOLE, CATCH BASIN
	EXISTING RIGHT-OF-WAY LINE
	PERMANENT EASEMENT
	EXISTING EASEMENT
	TEMPORARY EASEMENT
	WORK LIMITS
	RAILROAD SPIKE, IRON PIN, PK NAIL FOUND
	PROPERTY LINE
	BASELINE OR CENTERLINE
	EXISTING EDGE OF PAVEMENT
	EXISTING WATER MAIN, VALVE, AND HYDRANT
	EXISTING SANITARY SEWER, MANHOLE (FORCE MAIN)
	EXISTING GAS MAIN
	EXISTING STORM SEWER, MANHOLE AND INLET
	EXISTING UNDERGROUND TELEPHONE AND PEDESTAL
	EXISTING UNDERGROUND ELECTRIC AND PEDESTAL
	EXISTING FIBER OPTIC CABLE
	UTILITY POLES
	SIGNAL POLES
	POLE GUY WIRE
	SIGN
	TREE, TREE TO BE REMOVED



PLAN PREPARED BY:



Stantec Consulting Services Inc.
 1500 Lake Shore Drive, Suite 100
 Columbus, Ohio 43204
 (614) 486-4383

APPROVAL ON THE PART OF THE FRANKLIN COUNTY ENGINEER'S OFFICE IS GIVEN FOR WORK WITHIN THE FRANKLIN COUNTY R/W ONLY. THE MAINTENANCE OF THE MULTI-USE PATH, INCLUDING SIGNAGE, SHALL BE BY THE CITY OF DUBLIN, EVEN FOR SECTIONS IN UNINCORPORATED AREAS.

SIGNATURES BELOW SIGNIFY ONLY CONCURRENCE WITH THE GENERAL PURPOSES AND GENERAL LOCATION OF THE PROJECT. ALL TECHNICAL DETAILS REMAIN THE RESPONSIBILITY OF THE ENGINEER PREPARING THE PLANS.

Brian M. Hagerty 1/15/13
 REGISTERED ENGINEER DATE

John W. Espey 1/15/13
 REGISTERED ENGINEER DATE

APPROVED: *Gail A. Hammer* 1-17-2013
 CITY ENGINEER, CITY OF DUBLIN, OHIO DATE

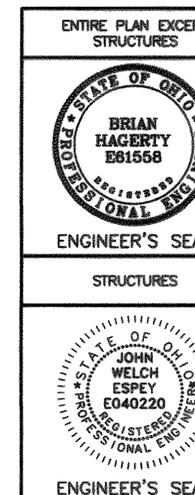
FRANKLIN COUNTY ENGINEER _____ DATE _____

FRANKLIN COUNTY CHIEF DEPUTY ENGINEER _____ DATE _____

APPROVAL ON THE PART OF THE CITY OF COLUMBUS IS GIVEN PURSUANT TO THE PROVISIONS OF THE WATER SERVICE AGREEMENT BETWEEN THE CITY OF DUBLIN, OHIO AND THE CITY OF COLUMBUS, OHIO, ON APRIL 13, 1993 AND ALL SUBSEQUENT AMENDMENTS THEREOF.

RC Waterfield 1-31-13
 ADMINISTRATOR, DIVISION OF WATER, CITY OF COLUMBUS, OHIO DATE

Greg J. Davies 2-26-2013
 DIRECTOR, DEPARTMENT OF PUBLIC UTILITIES, CITY OF COLUMBUS, OHIO DATE

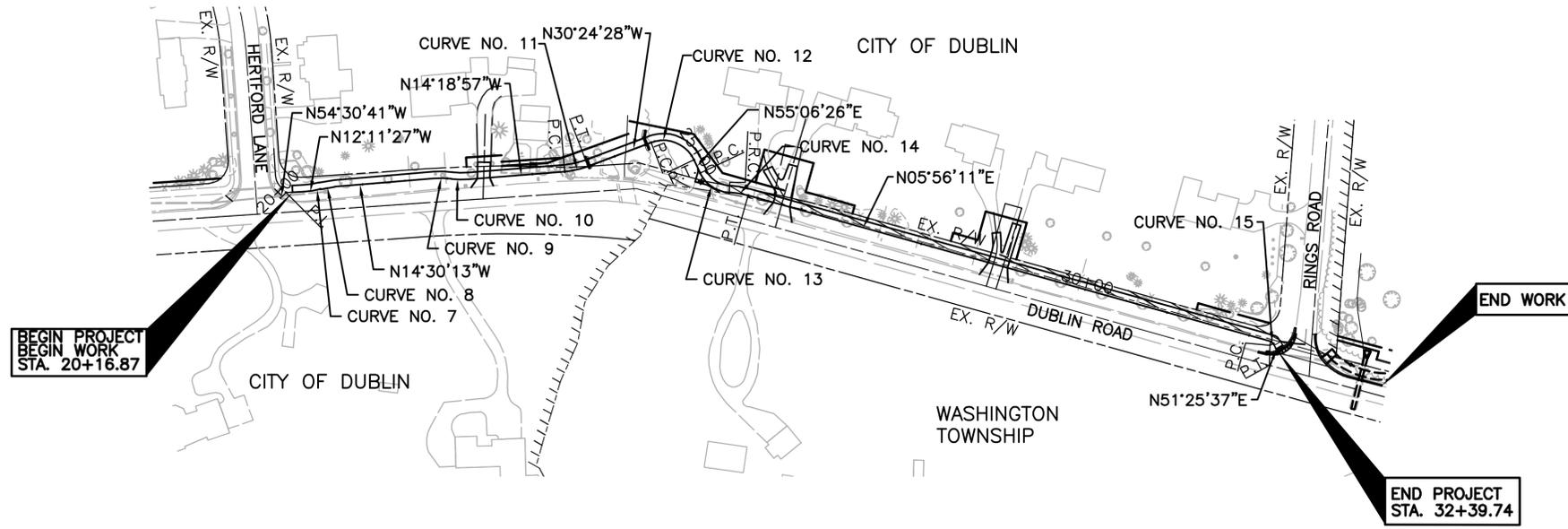


CIP NO. 08-009.2-CIP
 CONSTRUCTION PROJECT NO.

DUBLIN ROAD SOUTH MULTI-USE PATH

SHAR, JASON, U:\173809238\design\plan_eat\herford to rings\173809238.dwg TITLE SHEET User: jason Date: Jan 14, 2013 1:35 PM Plotter: Jan 15, 2013 7:45 AM

MCDANIEL, ARRON U:\173608736\design\plan_ssk\Hertford to Rings\173608736sc01.dwg SCHEMATIC PLAN Last Saved: Feb 13, 2013 10:01 AM Plotted: Mar 11, 2013 3:56 PM



CURVE DATA
CURVE NO. 7
 P.I. STA. 20+40.02
 $\Delta = 11^{\circ}59'32''$
 $Dc = 57'17'45''$
 $R = 100.00'$
 $T = 10.50'$
 $L = 20.93'$
 $E = 0.55'$
 P.C. STA. 20+29.52
 P.R.C. STA. 20+50.45

CURVE DATA
CURVE NO. 8
 P.I. STA. 20+58.91
 $\Delta = 09^{\circ}40'46''$
 $Dc = 57'17'45''$
 $R = 100.00'$
 $L = 16.89'$
 $E = 0.36'$
 P.R.C. STA. 20+50.45
 P.T. STA. 20+67.34

CURVE DATA
CURVE NO. 9
 P.I. STA. 21+94.28
 $\Delta = 11^{\circ}59'54''$
 $Dc = 57'17'45''$
 $R = 100.00'$
 $L = 20.94'$
 $E = 0.55'$
 P.C. STA. 21+83.78
 P.R.C. STA. 22+04.72

CURVE DATA
CURVE NO. 10
 P.I. STA. 22+15.06
 $\Delta = 11^{\circ}48'38''$
 $Dc = 57'17'45''$
 $R = 100.00'$
 $L = 20.61'$
 $E = 0.53'$
 P.R.C. STA. 22+04.72
 P.T. STA. 22+25.33

CURVE DATA
CURVE NO. 11
 P.I. STA. 23+52.39
 $\Delta = 16^{\circ}05'31''$
 $Dc = 57'17'45''$
 $R = 100.00'$
 $L = 28.09'$
 $E = 0.99'$
 P.C. STA. 23+38.26
 P.T. STA. 23+66.34

CURVE DATA
CURVE NO. 12
 P.I. STA. 24+82.89
 $\Delta = 85^{\circ}30'54''$
 $Dc = 143'14'22''$
 $R = 40.00'$
 $T = 36.99'$
 $L = 59.70'$
 $E = 14.48'$
 P.C. STA. 24+45.90
 P.T. STA. 25+05.60

CURVE DATA
CURVE NO. 13
 P.I. STA. 25+46.70
 $\Delta = 62^{\circ}01'25''$
 $Dc = 143'14'22''$
 $R = 40.00'$
 $T = 24.05'$
 $L = 43.30'$
 $E = 6.67'$
 P.C. STA. 25+22.65
 P.R.C. STA. 25+65.95

CURVE DATA
CURVE NO. 14
 P.I. STA. 25+77.22
 $\Delta = 12^{\circ}51'09''$
 $Dc = 57'17'45''$
 $R = 100.00'$
 $T = 11.26'$
 $L = 22.43'$
 $E = 0.63'$
 P.R.C. STA. 25+65.95
 P.T. STA. 25+88.39

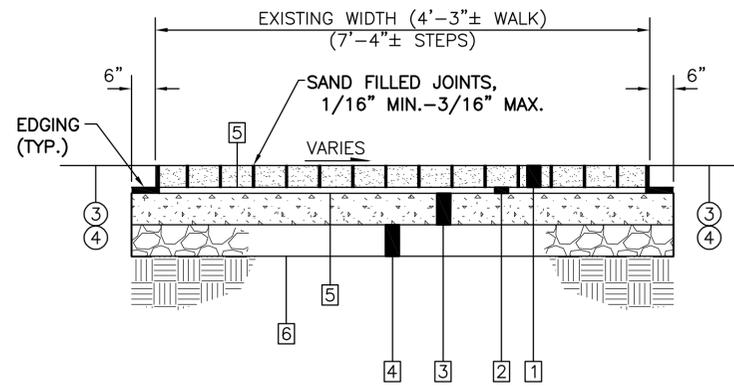
CURVE DATA
CURVE NO. 15
 P.I. STA. 32+28.59
 $\Delta = 45^{\circ}29'27''$
 $Dc = 1145'54'56''$
 $R = 5.00'$
 $T = 2.10'$
 $L = 3.97'$
 $E = 0.42'$
 P.C. STA. 32+26.50
 P.T. STA. 32+30.47



CALCULATED
 CNK
 CHECKED
 TJS

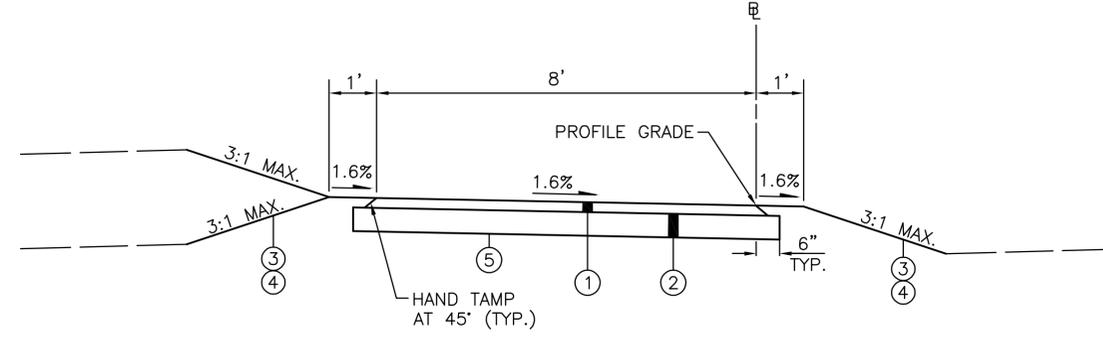
SCHEMATIC PLAN

DUBLIN ROAD SOUTH
 MULTI-USE PATH



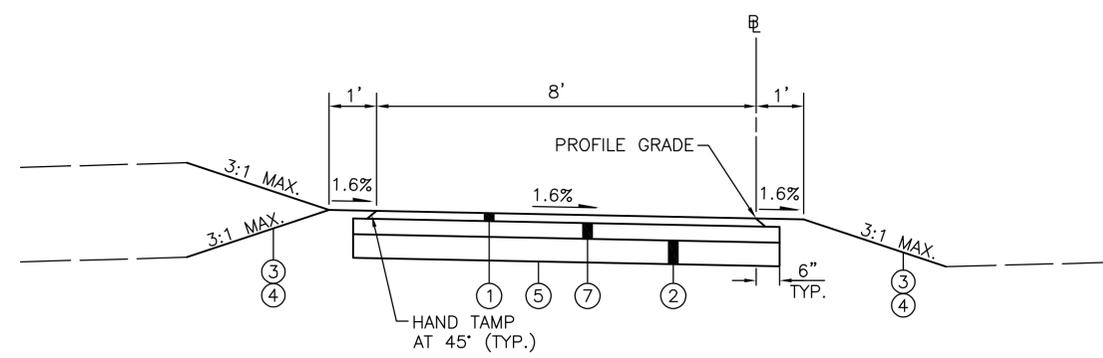
- ① SALVAGED EX. BRICK WALK
- ② 3/4" - BITUMINOUS ASPHALT SETTING BED (PG64-22)
- ③ 4" COLUMBUS CLASS "C" CONCRETE BASE
- ④ 4" - ITEM 304 AGGREGATE BASE
- ⑤ NEOPRENE - MODIFIED ASPHALT ADHESIVE
- ⑥ ITEM 204 - SUBGRADE COMPACTION

ITEM SPECIAL - BRICK WALK, A.P.P.
ITEM SPECIAL - BRICK STEPS, A.P.P.
N.T.S.



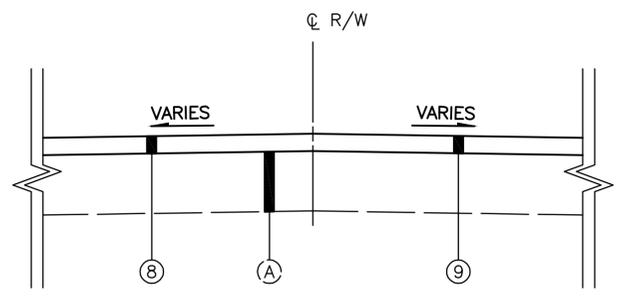
MULTI-USE PATH STANDARD SECTION

SECTION APPLIES:
STA. 20+16.87 TO STA. 32+37.71



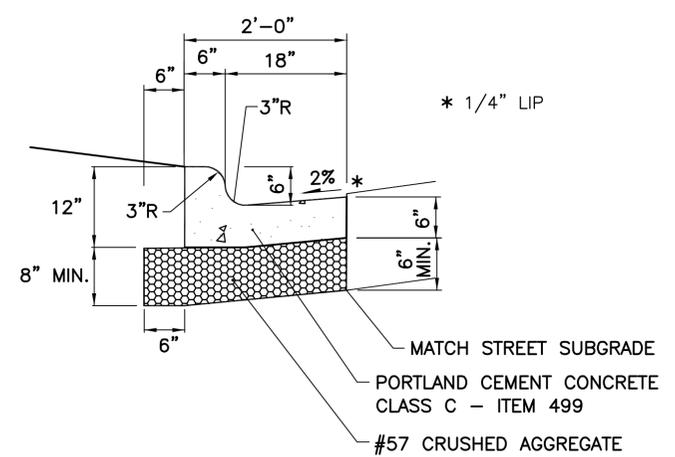
MULTI-USE PATH HEAVY DUTY SECTION

SECTION APPLIES:
STA. 24+70 TO STA. 25+50



MILL AND OVERLAY SECTION

SECTION APPLIES:
RINGS ROAD STA. 601+44.37 TO STA. 601+88.43



COMBINATION CURB & GUTTER DETAIL
N.T.S.

LEGEND

- ① ITEM 448 - 2-1/2" ASPHALT CONCRETE, SURFACE COURSE, TYPE 1, PG64-22, MEDIUM TRAFFIC
- ② ITEM 304 - 6" AGGREGATE BASE
- ③ ITEM 659 - SEEDING AND MULCHING, AS PER PLAN
- ④ ITEM 653 - 3" TOPSOIL FURNISHED AND PLACED, AS PER PLAN
- ⑤ ITEM 204 - SUBGRADE COMPACTION
- ⑥ NOT USED
- ⑦ ITEM 301 - 3-1/2" BITUMINOUS AGGREGATE BASE COURSE, PG64-22
- ⑧ ITEM 448 - 1-1/2" ASPHALT CONCRETE SURFACE COURSE, TYPE 1, PG64-22, MEDIUM TRAFFIC
- ⑨ ITEM 254 - 1-1/2" PAVEMENT PLANING, ASPHALT
- (A) EXISTING PAVEMENT

MCDANIEL, ARRON U:\173608736\design\plan_set\Herford to Rings\173608736ts01.dwg TYPICAL SECTIONS Last Saved: Feb 12, 2013 2:51 PM Plotted: Mar 11, 2013 3:56 PM

GENERAL NOTES

CITY OF COLUMBUS CONSTRUCTION AND MATERIAL SPECIFICATIONS, 2012 EDITION, AND ANY SUPPLEMENTS THERETO (HEREAFTER REFERRED TO AS STANDARD SPECIFICATIONS), SHALL GOVERN ALL CONSTRUCTION ITEMS UNLESS OTHERWISE NOTED. ITEM NUMBERS LISTED REFER TO CITY OF COLUMBUS ITEM NUMBERS UNLESS OTHERWISE NOTED.

IT IS THE INTENTION OF THE CONTRACT DOCUMENTS TO PROVIDE AND REQUIRE A COMPLETED PROJECT READY FOR OPERATION. ANY WORK ITEMS OMITTED FROM THE CONTRACT DOCUMENTS WHICH ARE CLEARLY NECESSARY FOR COMPLETION OF THE WORK AND ITS APPURTENANCES SHALL BE CONSIDERED A PART OF SUCH WORK, THOUGH NOT DIRECTLY SPECIFIED OR CALLED FOR IN THE CONSTRUCTION DOCUMENTS. THIS INCLUDES, BUT IS NOT LIMITED TO, SUCH INCIDENTAL ITEMS AS RELOCATION OF MAIL BOXES, SAW CUTTING, AND REMOVAL AND/OR RELOCATION OF SIGNS, SPRINKLERS, OR OTHER MISCELLANEOUS ITEMS.

ALL ITEMS OF WORK CALLED FOR ON THE PLANS FOR WHICH NO SPECIFIC METHOD OF PAYMENT IS PROVIDED SHALL BE PERFORMED BY THE CONTRACTOR WITH THE COST TO BE INCLUDED IN THE UNIT PRICE BID FOR THE VARIOUS RELATED ITEMS.

THE CONTRACTOR INTENDING TO SUBMIT A BID FOR CITY OF DUBLIN (HEREIN AFTER REFEREED TO AS "CITY") CAPITAL IMPROVEMENT CONTRACTS SHALL BE PREQUALIFIED WITH THE OHIO DEPARTMENT OF TRANSPORTATION AS PER SECTION 102 OF THE ODOT CONSTRUCTION AND MATERIAL SPECIFICATIONS AND CHAPTER 5525 OF THE OHIO REVISED CODE CONCERNING CONSTRUCTION CONTRACTS.

THE CONTRACTOR SHALL NOTIFY THE CITY OF DUBLIN, DIVISION OF ENGINEERING IN WRITING AT LEAST 3 WORKING DAYS PRIOR TO BEGINNING CONSTRUCTION.

THE CITY ENGINEER SHALL NOT BE RESPONSIBLE FOR MEANS, METHODS, PROCEDURES, TECHNIQUES, OR SEQUENCES OF CONSTRUCTION THAT ARE NOT SPECIFIED HEREIN. THE CITY ENGINEER SHALL NOT BE RESPONSIBLE FOR SAFETY ON THE WORK SITE, OR FOR FAILURE BY THE CONTRACTOR TO PERFORM WORK ACCORDING TO CONTRACT DOCUMENTS.

THE CITY SHALL BE RESPONSIBLE TO OBTAIN ALL NECESSARY PERMITS. A COPY OF THE COUNTY PERMIT SHALL BE POSTED ON THE JOB SITE.

THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR COMPLYING WITH ALL FEDERAL, STATE AND LOCAL SAFETY REQUIREMENTS INCLUDING THE OCCUPATIONAL SAFETY AND HEALTH ACT OF 1970. THE CONTRACTOR SHALL EXERCISE PRECAUTION ALWAYS FOR THE PROTECTION OF PERSONS (INCLUDING EMPLOYEES) AND PROPERTY. IT SHALL ALSO BE THE SOLE RESPONSIBILITY OF THE CONTRACTOR TO INITIATE, MAINTAIN AND SUPERVISE ALL SAFETY REQUIREMENTS, PRECAUTIONS AND PROGRAMS IN CONNECTION WITH THE WORK, INCLUDING THE REQUIREMENTS FOR CONFINED SPACES PER 29 CFR 1910.146.

ANY MODIFICATION TO THE WORK AS SHOWN ON THESE APPROVED PLANS SHALL HAVE PRIOR WRITTEN APPROVAL OF THE CITY ENGINEER.

THE CONTRACTOR SHALL RESTRICT CONSTRUCTION ACTIVITY TO PUBLIC RIGHT-OF-WAY AND AREAS DEFINED AS PERMANENT AND/OR TEMPORARY CONSTRUCTION EASEMENTS, AND/OR THE LIMITS OF DISTURBANCE SHOWN.

PROPERTY BOUNDARIES, INCLUDING PROPERTY LINES AND ROAD RIGHT-OF-WAY, ARE SHOWN FROM THE BEST INFORMATION AVAILABLE AND ARE NOT NECESSARILY COMPLETE OR CORRECT.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR HAVING THE FINISHED WORK CONFORM TO THE LINES, GRADE, ELEVATIONS, AND DIMENSIONS CALLED FOR ON THE DRAWINGS AND TYPICAL SECTIONS. PAYMENT FOR CONSTRUCTION LAYOUT SHALL BE MADE AT THE LUMP SUM PRICE BID FOR ITEM 623, CONSTRUCTION LAYOUT STAKES.

NON-RUBBER TIED VEHICLES SHALL NOT BE MOVED ON OR ACROSS PUBLIC STREETS OR HIGHWAYS WITHOUT THE PERMISSION OF THE CITY ENGINEER.

THE CONTRACTOR SHALL SUBSTANTIALLY RESTORE ALL DISTURBED AREAS TO EQUAL OR BETTER CONDITION THAN EXISTED BEFORE CONSTRUCTION. DRAINAGE DITCHES OR WATERCOURSES THAT ARE DISTURBED BY CONSTRUCTION SHALL BE RESTORED TO THE GRADES AND CROSS-SECTIONS THAT EXISTED BEFORE CONSTRUCTION.

TRACKING OR SPILLING MUD, DIRT OR DEBRIS UPON STREETS, RESIDENTIAL OR COMMERCIAL DRIVES, SIDEWALKS OR MULTI-USE PATHS IS PROHIBITED AND ANY SUCH OCCURRENCE SHALL BE CLEANED UP IMMEDIATELY BY THE CONTRACTOR AT NO COST TO THE CITY. IF THE CONTRACTOR FAILS TO REMOVE THE MUD, DIRT, DEBRIS, OR SPILLAGE, THE CITY OF DUBLIN RESERVES THE RIGHT TO REMOVE THESE MATERIALS AND CLEAN AFFECTED AREAS, THE COST OF WHICH SHALL BE WITHHELD FROM MONIES THAT ARE DUE OR MAY BECOME DUE TO THE CONTRACTOR.

EXCESS EXCAVATED MATERIAL FROM THIS PROJECT SHALL BE HAULED OFF-SITE BY THE CONTRACTOR AND COMPENSATION FOR HAULING SHALL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM 203.

TOP OF PAVEMENT ELEVATIONS SHOWN ON THE PAVEMENT DETAILS MAY REFLECT DEVIATIONS FROM THE ROADWAY PROFILES AND TYPICAL SECTION CROSS SLOPES IN SOME AREAS DUE TO PAVEMENT WARPING. IN ALL CASES, THE TOP OF PAVEMENT ELEVATIONS SHOWN ON THE PAVEMENT DETAILS SHALL GOVERN.

PAVEMENTS SHALL BE CUT IN NEAT, STRAIGHT LINES THE FULL DEPTH OF THE EXISTING PAVEMENT, OR AS REQUIRED BY THE CITY ENGINEER. PAVEMENT REPLACEMENT SHALL BE CONDUCTED ACCORDING TO CITY OF COLUMBUS STANDARD DRAWING 1441 DR. A AND APPLICABLE CITY OF DUBLIN STANDARD DRAWINGS. THE REPLACEMENT OF DRIVEWAYS, HANDICAPPED RAMPS, SIDEWALKS, MULTI-USE PATHS, PARKING LOT PAVEMENT, ETC. SHALL BE PROVIDED ACCORDING TO THE APPROVED CONSTRUCTION DRAWINGS AND CITY OF DUBLIN STANDARD CONSTRUCTION DRAWINGS.

THE CONTRACTOR SHALL SUBMIT TO THE CITY ENGINEER AT THE PRECONSTRUCTION MEETING HIS PROPOSED DESIGN MIX FORMULA FOR ALL BITUMINOUS MIXTURES TO BE PLACED ON THE PROJECT FOR REVIEW AND APPROVAL. A DESIGN MIX FORMULA SHALL BE SUBMITTED FOR EACH MIXTURE AND EACH PRODUCER AND SHALL PROVIDE GRADATION OF ALL COMPONENT AGGREGATES, PERCENTAGE OF BLENDING OF AGGREGATES, PERCENTAGE OF BITUMEN, ANY ADDITIVES AND APPLICATION RATE, NAMES AND ADDRESSES OF AGGREGATE SUPPLIERS, MARSHALL MIX DESIGN DATA, AND THE THEORETICAL LABORATORY DENSITY.

ALL SOIL SUBGRADES SHALL BE PREPARED AND COMPACTED IN ACCORDANCE WITH ITEM 204 SUBGRADE COMPACTION TO A DEPTH OF 12-INCHES BELOW THE SUBGRADE SURFACE. SUBGRADE SHALL BE SCARIFIED AND CONTAIN SUFFICIENT MOISTURE TO MEET ITEM 204 SUBGRADE COMPACTION REQUIREMENTS.

SUBGRADE COMPACTION SHALL ALSO BE REQUIRED UNDER THE MULTI-USE PATHS, AND SHALL FOLLOW THE REQUIREMENTS FOR COMPACTION UNDER DRIVEWAYS AS DESCRIBED IN SECTION 204.03. THE COST SHALL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM 204, SUBGRADE COMPACTION.

NO BLASTING WILL BE PERMITTED ON THIS PROJECT.

THE CONTRACTOR IS NOT PERMITTED TO USE ANY RECLAIMED MATERIALS IN ITEM 304.

THE CONTRACTOR SHALL DEFINE THE LIMITS OF ANY WEAK SOILS ENCOUNTERED BY PROOF ROLLING. WHERE SOFT SUBGRADE IS ENCOUNTERED IN CUTS, DUE TO NO FAULT OF THE CONTRACTOR, AND SATISFACTORY COMPACTION CANNOT BE OBTAINED, THE UNSTABLE MATERIAL SHALL BE REMOVED AND REPLACED PER ITEM 203.13. THE FOLLOWING QUANTITIES HAVE BEEN INCLUDED AS AN ALLOWANCE FOR BIDDING PURPOSES.

ITEM 203 EXCAVATION 40 CY
ITEM 203 SELECT GRANULAR EMBANKMENT, #2 STONE 40 CY

ITEM 202, "STRUCTURES REMOVED AND DISPOSED OF" SHALL INCLUDE REMOVAL AND DISPOSAL OF ALL HEADWALLS; UNDERGROUND CONCRETE DUCT, CONDUIT, OR CABLE; AND ANY OTHER STRUCTURES AND OBSTRUCTIONS NOT DESIGNATED TO REMAIN THAT ARE NOT COVERED UNDER OTHER ITEMS IN THE CONTRACT.

THE COST OF ALL ASPHALT PAVEMENT REMOVAL AND DISPOSAL SHALL BE INCLUDED IN THE PRICE BID PER CUBIC YARD FOR ITEM 203 - EXCAVATION. THE COST OF CONCRETE PAVEMENT REMOVAL AND DISPOSAL WILL BE PAID AT THE UNIT PRICE BID PER SQUARE YARD FOR ITEM 202 - PAVEMENT REMOVED AND DISPOSED OF.

PROOF SURVEY

FOLLOWING COMPLETION OF CONSTRUCTION, A PROOF SURVEY SHALL BE PROVIDED BY THE CONTRACTOR TO THE DIVISION OF ENGINEERING THAT DOCUMENTS AS-BUILT INFORMATION OF ALL ELEMENTS OF THIS PROJECT. THE SURVEY SHALL BE PREPARED AND SIGNED BY AN OHIO PROFESSIONAL SURVEYOR. THE CONTRACTOR SHALL REVISE THE ORIGINAL MYLARS IN RED INK, TO THE SATISFACTION OF THE CITY, SHOWING ALL CHANGES IN THE WORK. THE COST OF THE PROOF SURVEY, INCLUDING MYLAR REVISIONS, WILL BE PAID AT THE LUMP SUM PRICE BID FOR ITEM SPECIAL, PROOF SURVEY.

SURVEY MONUMENTATION

THE CONTRACTOR SHALL CONTACT THE FRANKLIN COUNTY ENGINEER'S OFFICE, SURVEY DEPARTMENT AT 614-525-3026 TWO WORKING DAYS BEFORE DISTURBING ANY FRANKLIN COUNTY GEODETIC MONUMENTS (VERTICAL AND/OR HORIZONTAL) FOR REFERENCE AND REPLACEMENT.

THE CONTRACTOR SHALL CAREFULLY PRESERVE BENCH MARKS, PROPERTY CORNERS, REFERENCE POINTS, STAKES AND OTHER SURVEY REFERENCE MONUMENTS OR MARKERS. IN CASES OF WILLFUL OR CARELESS DESTRUCTION, THE CONTRACTOR SHALL BE RESPONSIBLE FOR RESTORATIONS. RESETTING OF MARKERS SHALL BE PERFORMED BY AN OHIO PROFESSIONAL SURVEYOR AS APPROVED BY THE CITY ENGINEER AT THE CONTRACTOR'S EXPENSE.

CONSTRUCTION NOISE

ANY DEVICE SHALL NOT BE OPERATED AT ANY TIME IN SUCH A MANNER THAT THE NOISE CREATED SUBSTANTIALLY EXCEEDS THE NOISE CUSTOMARILY AND NECESSARILY ATTENDANT TO THE REASONABLE AND EFFICIENT PERFORMANCE OF SUCH EQUIPMENT. THE CONTRACTOR'S CONSTRUCTION ACTIVITIES SHALL BE CONDUCTED SO AS TO ELIMINATE ALL UNNECESSARY NOISE, DUST, AND ODORS. THE USE OF OIL OR OTHER MATERIAL FOR DUST CONTROL, WHICH MAY CAUSE TRACKING, IS NOT PERMITTED.

ITEM SPECIAL - GRASS PAVERS, AS PER PLAN

THIS ITEM SHALL INCLUDE PLACEMENT OF GRASS PAVERS, (GRASSCRETE, TYPE GC2, OR APPROVED EQUAL, INSTALLED PER MANUFACTURER SPECIFICATIONS), IN THE LOCATIONS NOTED IN THE PLANS. WORK SHALL INCLUDE MATCHING THE PROPOSED PAVERS FLUSH (VERTICALLY) AND ADJACENT (HORIZONTALLY) WITH THE EXISTING PAVERS IN THE AREA. ALL MATERIALS, LABOR, AND EQUIPMENT REQUIRED TO PERFORM THE WORK TO THE SATISFACTION OF THE ENGINEER SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE BID FOR ITEM SPECIAL - GRASS PAVERS, AS PER PLAN.

UTILITIES

THE CONTRACTOR SHALL GIVE NOTICE OF INTENT TO CONSTRUCT TO OHIO UTILITIES PROTECTION SERVICE (TELEPHONE NUMBER 800-362-2764), AND TO OWNERS OF UNDERGROUND UTILITIES THAT ARE NOT MEMBERS OF A REGISTERED UNDERGROUND PROTECTION SERVICE. NOTICE SHALL BE GIVEN AT LEAST TWO WORKING DAYS BEFORE START OF CONSTRUCTION.

THE IDENTITY AND LOCATIONS OF EXISTING UNDERGROUND UTILITIES IN THE CONSTRUCTION AREA HAVE BEEN SHOWN ON THE PLANS AS ACCURATELY AS PROVIDED BY THE OWNER OF THE UNDERGROUND UTILITY. THE CITY OF DUBLIN AND THE CITY ENGINEER ASSUME NO RESPONSIBILITY FOR THE ACCURACY OR DEPTHS OF UNDERGROUND FACILITIES SHOWN ON THE PLANS. IF DAMAGE IS CAUSED, THE CONTRACTOR SHALL BE RESPONSIBLE FOR REPAIR OF THE SAME AND FOR ANY RESULTING CONTINGENT DAMAGE.

LOCATION, SUPPORT, PROTECTION AND RESTORATION OF ALL EXISTING UTILITIES AND APPURTENANCES, SHOWN OR NOT SHOWN ON THE PLANS, SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR. THE COST OF THIS WORK SHALL BE INCLUDED IN THE PRICES BID FOR THE VARIOUS ITEMS OF THE CONTRACT.

WHEN UNKNOWN OR INCORRECTLY LOCATED UNDERGROUND UTILITIES ARE ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY AND THE CITY ENGINEER.

UTILITIES KNOWN TO BE LOCATED WITHIN THE LIMITS OF THIS PROJECT ARE LISTED BELOW WITH CONTACT INFORMATION.

AT&T (CONDUIT)
KEVIN GLASER
111 N. 4TH STREET, ROOM 802
COLUMBUS, OHIO 43215
(614) 223-6424
(614) 208-9312 (MOBILE)

AMERICAN ELECTRIC POWER
PAUL PAXTON
850 TECH CENTER DRIVE
GAHANNA, OHIO 43230-6605
(614) 883-6831

AT&T (CABLE)
TOM ZIOMEK
111 N. 4TH STREET, ROOM 802
COLUMBUS, OHIO 43215
(614) 223-7162

DUBLINK (TEAM FISHEL)
JOE TEPPER
1600 WALCUTT ROAD
COLUMBUS, OH 43228
(614) 921-8620

COLUMBIA GAS OF OHIO
DAVID KELLY
1600 DUBLIN ROAD
COLUMBUS, OHIO 43215
(614) 481-1058

CITY OF DUBLIN
DIVISION OF ENGINEERING
MIKE SWEDER, P.E.
5800 SHIER-RINGS ROAD
DUBLIN, OHIO 43016-1236
(614) 410-4621

TIME WARNER CABLE
RAY MAURER
3760 INTERCHANGE DRIVE
COLUMBUS, OHIO 43204
(614) 348-2979 (MOBILE)

CITY OF COLUMBUS
DIVISION OF POWER & WATER
910 DUBLIN ROAD
COLUMBUS, OHIO 43215
(614) 645-7788

WIDE OPEN WEST
JAYTEE NOVARIA
3675 CORPORATE DRIVE
COLUMBUS, OHIO 43231
(614) 668-7632 (MOBILE)

WORK LIMITS

THE WORK LIMITS SHOWN ON THESE PLANS ARE FOR PHYSICAL CONSTRUCTION ONLY. THE INSTALLATION AND OPERATION OF ALL TEMPORARY TRAFFIC CONTROL AND TEMPORARY TRAFFIC CONTROL DEVICES REQUIRED BY THESE PLANS SHALL BE PROVIDED BY THE CONTRACTOR WHETHER INSIDE OR OUTSIDE THE WORK LIMITS.

AMERICANS WITH DISABILITIES ACT (ADA)

ALL SIDEWALKS, PEDESTRIAN PATHS, CURB RAMPS, AND DRIVEWAYS SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE LATEST STANDARDS OF THE AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES (ADAGG).

THE ELEVATIONS AND SLOPES PROVIDED ON THE DRAWINGS ARE INTENDED TO CONVEY A DESIGN THAT IS COMPATIBLE WITH ADA GUIDELINES. THE CONTRACTOR SHALL DETERMINE THE EXISTING ELEVATIONS OF ADJACENT NEW OR EXISTING CONCRETE CURB AND ADJACENT NEW OR EXISTING WALK PRIOR TO THE START OF RAMP CONSTRUCTION. ADJUSTMENTS IN GRADE SHALL BE MADE BY THE CONTRACTOR BASED ON THE ELEVATION INFORMATION TO INSURE THAT THE FINISHED WORK IS IN ACCORDANCE WITH ADA GUIDELINES.

ALL DETECTABLE WARNINGS SHALL BE ADA SOLUTIONS TACTILE WARNING SURFACE MATS - REPLACEABLE WET-SET COMPOSITE TACTILE WITH A DOME SPACING OF 2.35 INCHES, MANUFACTURER PART NUMBER 2448REP. BRICK RED COLOR SHALL BE USED IN MOST LOCATIONS UNLESS OTHERWISE NOTED. THE COST OF THIS WORK WILL BE PAID AT THE UNIT PRICE BID PER EACH, AND SHALL BE IN ADDITION TO ANY OVERLAPPING PAYMENTS PER SQUARE FOOT FOR SIDEWALK OR PEDESTRIAN PATH.

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GENERAL NOTES

DUBLIN ROAD SOUTH
MULTI-USE PATH

TRENCH AND BACKFILL

TRENCH EXCAVATION SHALL BE ADEQUATELY MAINTAINED AND PROTECTED WITH DRUMS OR BARRICADES AT ALL TIMES. PLACEMENT OF PROPOSED SUBBASE AND BASE MATERIAL SHALL FOLLOW AS CLOSELY AS POSSIBLE BEHIND EXCAVATION OPERATIONS. THE LENGTH OF OPEN TRENCH AT ANY ONE TIME SHALL BE HELD TO A MINIMUM AND SHALL AT ALL TIMES BE SUBJECT TO THE APPROVAL OF THE CITY ENGINEER.

ALL TRENCHES WITHIN PUBLIC RIGHT-OF-WAY SHALL BE BACKFILLED OR SECURELY PLATED DURING NONWORKING HOURS. TRENCHES OUTSIDE THESE AREAS SHALL BE BACKFILLED OR SHALL BE PROTECTED BY APPROVED TEMPORARY FENCING OR BARRICADES DURING NONWORKING HOURS. CLEAN UP SHALL FOLLOW CLOSELY BEHIND THE TRENCHING OPERATION.

BACKFILL WITHIN A 1:1 INFLUENCE LINE OF EXISTING STRUCTURES (HOUSES, GARAGES, ETC.) OR PUBLIC INFRASTRUCTURE (PAVEMENTS, SIDEWALKS, CURBS, ETC.) SHALL BE ITEM 912 - COMPACTED GRANULAR MATERIAL, OR ITEM 636 - FLOWABLE CONTROLLED DENSITY FILL, TYPE 2.

GRANULAR BACKFILL SHALL BE COMPACTED GRANULAR MATERIAL ACCORDING TO ITEM 912 OF THE STANDARD SPECIFICATIONS OR CONTROLLED DENSITY BACKFILL ACCORDING TO ITEM 636, TYPE 2, OF THE STANDARD SPECIFICATIONS AS DIRECTED BY THE CITY ENGINEER. ITEM 912 MATERIAL SHALL CONSIST OF NATURAL, BROKEN, OR CRUSHED ROCK. SYNTHETIC OR MANMADE MATERIALS ARE UNACCEPTABLE.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE CONDITION OF TRENCHES WITHIN THE RIGHT-OF-WAY AND PUBLIC EASEMENTS FOR A PERIOD OF ONE YEAR FROM THE FINAL ACCEPTANCE OF THE WORK, AND SHALL MAKE ANY NECESSARY REPAIRS AT NO COST TO THE CITY.

CONTINGENCY QUANTITIES

THE CONTRACTOR SHALL NEITHER ORDER MATERIALS NOR PERFORM WORK FOR ITEMS DESIGNATED BY PLAN NOTE TO BE USED "AS DIRECTED BY THE CITY ENGINEER" UNLESS AUTHORIZED BY THE CITY ENGINEER.

TEMPORARY EROSION AND SEDIMENT CONTROL

THE FOLLOWING ESTIMATED QUANTITY IS TO BE USED AS DIRECTED BY THE CITY ENGINEER FOR TEMPORARY EROSION AND SEDIMENT CONTROL MEASURES:

ITEM 207 - FILTER FABRIC FENCE 2000 LF

ITEM SPECIAL - TREE PROTECTION FENCE

ALL TREES WITHIN THE CONSTRUCTION AREA NOT DESIGNATED FOR REMOVAL, WHETHER SHOWN ON THE PLANS OR NOT, SHALL BE PRESERVED WITHOUT DAMAGE. TREES TO BE PRESERVED SHALL BE PROTECTED WITH HIGH VISIBILITY FENCING PLACED A MINIMUM OF 15 FEET FROM THE TREE TRUNK. TREES 6 INCHES OR GREATER AT DBH (DIAMETER BREST HEIGHT) MUST BE PROTECTED WITH FENCING PLACED AT THE CRITICAL ROOT ZONE OR 15 FEET, WHICHEVER IS GREATER. WHERE 15 FEET CANNOT BE MAINTAINED, THE FENCING SHALL BE PLACED IN SUCH A WAY AS TO MAXIMIZE PROTECTION OF THE BRANCHES, TRUNK, AND ROOTS. A QUANTITY HAS BEEN INCLUDED IN THE GENERAL SUMMARY FOR SAID WORK.

TREES NOT INDICATED ON THE APPROVED CONSTRUCTION DRAWINGS FOR REMOVAL MAY NOT BE REMOVED WITHOUT PRIOR APPROVAL OF THE DIVISION OF ENGINEERING.

ITEM 609 - 6" CONCRETE COMBINATION CURB AND GUTTER, AS PER PLAN

THE 6" CONCRETE COMBINATION CURB AND GUTTER SHALL BE IN ACCORDANCE WITH CITY OF DUBLIN STANDARD DRAWING RD-02.

WHERE THESE ITEMS ARE TO BE PLACED WITHIN OR ADJACENT TO EXISTING PAVEMENT, THE PAVEMENT SHALL BE SAWCUT, REMOVED, AND REPLACED IN ACCORDANCE WITH THE PLAN LOCATIONS AND DETAILS. THE COST OF ALL LABOR, MATERIALS, AND EQUIPMENT REQUIRED TO SAWCUT, REMOVE AND REPLACE THE AFFECTED EXISTING PAVEMENT AREA SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR THE PERTINENT 609 ITEM.

ITEM 201 - TREES OR STUMPS REMOVED, 15" SIZE
ITEM 201 - TREES OR STUMPS REMOVED, 30" SIZE

ALL TREES AND STUMPS SPECIFICALLY MARKED FOR REMOVAL WITHIN THE CONSTRUCTION LIMITS OF THIS PROJECT SHALL BE REMOVED UNDER ITEM 201 - TREES OR STUMPS REMOVED, 15" SIZE, AND ITEM 201 - TREES OR STUMPS REMOVED, 30" SIZE. TREES WITH TRUNK DIAMETERS 12 INCHES AND LESS SHALL BE CONSIDERED BRUSH AND THEIR REMOVAL PAID UNDER ITEM 201 - CLEARING AND GRUBBING UNLESS DESIGNATED IN THE PLAN TO SAVE.

THE CITY OF DUBLIN RESERVES THE RIGHT TO ORDER THE REMOVAL OF ADDITIONAL TREES AND/OR STUMPS OUTSIDE THE LIMITS OF CONSTRUCTION BUT WITHIN THE RIGHT-OF-WAY AND/OR EASEMENT LINES.

TREE TRIMMING WITHIN THE CONSTRUCTION ZONE IS TO BE COMPLETED BY A CERTIFIED ARBORIST. AT THE COMPLETION OF THE PROJECT, THE ARBORIST IS TO RETURN AND TRIM ANY BROKEN BRANCHES AS NEEDED.

STORM SEWER

THE CONTRACTOR SHALL CONDUCT HIS OPERATIONS SO AS TO MAINTAIN AT ALL TIMES STORM SEWER, DRAIN, AND DITCH FLOWS THROUGH EXISTING FACILITIES TO REMAIN IN PLACE AND THROUGH EXISTING FACILITIES TO BE REPLACED UNTIL NEW FACILITIES ARE COMPLETED AND PUT INTO SERVICE. THE FLOW OF ALL STORM SEWERS, DRAINS, AND OTHER WATER COURSES ENCOUNTERED AND DISTURBED OR DESTROYED DURING CONSTRUCTION SHALL BE RESTORED BY THE CONTRACTOR TO A CONDITION SATISFACTORY TO THE CITY ENGINEER.

BEFORE ANY WORK IS STARTED ON THE PROJECT AND AGAIN BEFORE FINAL ACCEPTANCE BY THE CITY, REPRESENTATIVES OF THE CITY AND THE CONTRACTOR SHALL MAKE AN INSPECTION OF ALL EXISTING SEWERS WHICH ARE TO REMAIN IN SERVICE AND WHICH MAY BE AFFECTED BY THE WORK. THE CONDITION OF THE EXISTING CONDUITS AND THEIR APPURTENANCES SHALL BE DETERMINED FROM FIELD OBSERVATIONS. RECORDS OF THE INSPECTION SHALL BE KEPT IN WRITING BY THE CITY.

ALL NEW CONDUITS, INLETS, CATCH BASINS AND MANHOLES CONSTRUCTED AS A PART OF THE PROJECT SHALL BE FREE OF ALL FOREIGN MATTER AND IN A CLEANED CONDITION BEFORE THE PROJECT WILL BE ACCEPTED BY THE CITY.

ALL EXISTING SEWERS INSPECTED INITIALLY BY THE ABOVE MENTIONED PARTIES SHALL BE MAINTAINED AND LEFT IN A CONDITION REASONABLY COMPARABLE TO THAT DETERMINED BY THE ORIGINAL INSPECTION. ANY CHANGE IN THE CONDITION RESULTING FROM THE CONTRACTOR'S OPERATIONS SHALL BE CORRECTED BY THE CONTRACTOR TO THE SATISFACTION OF THE CITY ENGINEER.

PAYMENT FOR ALL OPERATIONS DESCRIBED ABOVE SHALL BE INCLUDED IN THE CONTRACT PRICE FOR THE PERTINENT CONTRACT ITEMS.

WHERE PLANS CALL FOR CONDUIT TO BE CONNECTED TO, OR CROSS OVER OR UNDER AN EXISTING SEWER OR UNDERGROUND UTILITY, THE CONTRACTOR SHALL LOCATE THE EXISTING PIPES OR UTILITIES FOR BOTH LINE AND GRADE PRIOR TO THE START OF PIPE LAYING.

IF IT IS DETERMINED THAT A PROPOSED CONDUIT WILL CONFLICT WITH AN EXISTING SEWER OR UNDERGROUND UTILITY WHEN CONSTRUCTED AS SHOWN ON THE PLAN, THE CITY ENGINEER SHALL BE NOTIFIED BEFORE STARTING CONSTRUCTION ON ANY PORTION OF THE PROPOSED CONDUIT WHICH WOULD BE AFFECTED BY THE CONFLICT.

PAYMENT FOR THE DETERMINATION OF LINE AND GRADE OF EXISTING UTILITIES AS REQUIRED SHALL BE INCLUDED IN THE CONTRACT PRICE FOR THE PERTINENT CONTRACT ITEMS.

ALL FIELD TILE BROKEN OR ENCOUNTERED DURING EXCAVATION SHALL BE REPLACED OR REPAIRED IN LIKE KIND AND CONNECTED TO THE STORM SEWER SYSTEM OR OPEN CHANNEL OUTLET, RESTORING NORMAL FUNCTION TO THE TILE, AS DIRECTED BY THE CITY ENGINEER. THE COST OF THIS WORK SHALL BE INCLUDED IN THE PRICES BID FOR THE VARIOUS ITEMS OF THE CONTRACT.

ALL PRECAST CONCRETE PRODUCTS SHALL BE INSPECTED AT THE LOCATION OF MANUFACTURE. APPROVED PRECAST CONCRETE PRODUCTS WILL BE STAMPED NOTING THAT INSPECTION HAS BEEN PERFORMED BY THE CITY OF DUBLIN. PRECAST CONCRETE PRODUCTS WITHOUT PROOF OF INSPECTION SHALL NOT BE APPROVED FOR INSTALLATION.

ALL STORM SEWERS SHALL BE REINFORCED CONCRETE PIPE CONFORMING TO ASTM DESIGNATION C76, WALL B, CLASS IV FOR PIPE DIAMETERS 12 INCHES TO 15 INCHES, CLASS III FOR 18 INCHES TO 24 INCH PIPES, AND 27 INCHES AND LARGER PIPE SHALL BE CLASS II, UNLESS OTHERWISE SHOWN ON THE PLANS.

THE CONTRACTOR SHALL PLACE CURB & GUTTER INLETS WITHIN THE CURB LINE IN ACCORDANCE WITH COLUMBUS STANDARD DRAWING AA-S125.

ALL INLETS, CATCH BASINS, AND MANHOLES SHALL BE CHANNELIZED.

ALL EXISTING AND PROPOSED CASTINGS SHALL BE ADJUSTED TO MATCH THE SURROUNDING FINISH GRADE BY THE CONTRACTOR. TOP OF CASTING ELEVATIONS PROVIDED ON THE PLANS ARE APPROXIMATE. PAYMENT UNDER ITEM 604 - MANHOLES ADJUSTED TO GRADE, SHALL ONLY BE FOR CASTING ADJUSTMENTS ON EXISTING MANHOLES THAT REQUIRE NO OTHER WORK. THE COST OF ALL ADDITIONAL ADJUSTMENTS SHALL BE INCLUDED IN THE VARIOUS SEWER ITEMS.

WHERE BACKFILLING WITH CONCRETE AROUND A STORM SEWER PIPE IS DETERMINED TO BE NECESSARY, BY EITHER CALL OUT ON THE PLANS, OR AS DIRECTED BY THE ENGINEER, THE WORK SHALL BE IN ACCORDANCE WITH THE CITY OF COLUMBUS STANDARD DRAWING AA-S151, TYPE 1 BEDDING FOR RIGID SEWER PIPE USING CLASS "A" CONCRETE. THE TOTAL LENGTH OF BACKFILL SHALL BE FOR ALL EXPOSED PORTIONS OF PIPE, OR AS DIRECTED BY THE ENGINEER.

UNRECORDED STORM WATER DRAINAGE

FURNISH A CONTINUANCE FOR ALL UNRECORDED STORM WATER DRAINAGE, SUCH AS ROOF DRAINS, FOOTER DRAINS, OR YARD DRAINS, DISTURBED BY THE WORK. FURNISH EITHER AN OPEN CONTINUANCE OR AN UNOBTSTRUCTED CONTINUANCE BY CONNECTING A CONDUIT THROUGH THE CURB OR INTO A DRAINAGE STRUCTURE. THE LOCATION, TYPE, SIZE AND GRADE OF THE NEEDED CONDUIT TO REPLACE OR EXTEND AN EXISTING DRAIN WILL BE DETERMINED BY THE ENGINEER. ALL SUCH CONTINUANCE REQUIRES A RIGHT OF WAY USE PERMIT.

THE FOLLOWING CONDUIT TYPES MAY BE USED: 707.33, 707.41 NON-PERFORATED, 707.42, 707.43, 707.45, 707.46, 707.47, 707.51, 707.52 SDR35.

THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN INCLUDED IN THE GENERAL SUMMARY FOR USE AS DIRECTED BY THE ENGINEER FOR THE WORK NOTED ABOVE:

ITEM 901 - 4" PIPE WITH TYPE 1 BEDDING 50 LF

ITEM 604 - INLETS & CATCH BASINS, A.P.P.

BICYCLE SAFE GRATES SHALL BE USED FOR ALL PROPOSED CURB AND GUTTER INLETS AND CATCH BASINS. ALL CATCH BASINS ARE TO BE EQUIPPED WITH EAST JORDAN #5110, TYPE M3 GRATES.

ITEM 630 - GROUND MOUNTED SUPPORT, NO. 3, TYPE S, AS PER PLAN

ALL SIGN SUPPORTS SHALL BE 2-INCH SQUARE GALVANIZED POSTS WITH DIE CUT KNOCK OUTS (ALLIED QUICK-PUNCH SUPPORTS OR APPROVED EQUAL). A SINGLE BREAKAWAY ANCHOR SHALL BE USED. ALL SIGNS SHALL BE ERECTED WITH A 7-FOOT VERTICAL CLEARANCE BETWEEN THE TOP OF CURB OR EDGE OF PAVEMENT AND THE BOTTOM OF EACH SIGN, UNLESS OTHERWISE DESIGNATED BY THE CITY ENGINEER. HORIZONTAL CLEARANCE FOR BOTH CURB AND DITCH SECTIONS SHALL BE AS PER ODOT STANDARDS. THE ANCHOR POST SHALL BE PAID FOR SEPARATELY. PAYMENT FOR THIS ITEM SHALL BE FOR THE LENGTH ONLY, INCLUDING THE 8" OVERLAP IN THE ANCHOR POST, AND ALL MISCELLANEOUS ATTACHMENT HARDWARE.

ITEM 630 - 2-1/4" SQUARE ANCHOR POST, AS PER PLAN

IN ADDITION TO ITEMS 630 AND 730, THE ANCHOR POST PROVIDED AND INSTALLED WITH THE GROUND MOUNTED SIGN SUPPORT SHALL BE AS PER ODOT SCD TC-41.20, AND SHALL BE 48" IN LENGTH. THE ANCHOR SHALL BE 2-1/4 INCHES SQUARE, 12 GA., WITH A 2-1/2-INCH OVERSLEEVE 18 INCHES LONG OVER TOP OF THE ANCHOR. THIS ITEM SHALL BE PAID FOR AT THE UNIT PRICE BID PER EACH.

ITEM 630 - SIGNS, FLAT SHEET

TRAFFIC CONTROL SIGNS, AS SHOWN ON THE DRAWINGS, SHALL BE DESIGNED AND FABRICATED IN ACCORDANCE WITH THE REQUIREMENTS OF THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES AND THE PROVISIONS OF ODOT ITEM 630, UNLESS OTHERWISE NOTED.

ITEM 630 - REMOVAL OF GROUND MOUNTED SIGN AND STORAGE

ALL EXISTING GROUND MOUNTED SIGNS WITHIN THE PROJECT AREA SHALL BE REMOVED BY THE CONTRACTOR AND DELIVERED TO THE CITY AT A LOCATION TO BE DETERMINED WITHIN THE DUBLIN CITY LIMITS. THE CONTRACTOR SHALL COMPENSATE THE CITY IN AN AMOUNT EQUAL TO THE REPLACEMENT COST OF ANY SIGNS DAMAGED AS A RESULT OF THE CONSTRUCTION OPERATIONS. PAYMENT FOR SIGN REMOVAL AND DELIVERY WILL BE MADE AT THE UNIT PRICE BID PER EACH FOR ITEM 630, REMOVAL OF GROUND MOUNTED SIGN AND DISPOSAL.

ALL ROADWAY SIGNAGE WILL BE FURNISHED AND INSTALLED BY THE CITY OF DUBLIN INCLUDING, BUT NOT LIMITED TO, SUPPORTS, ANCHOR POSTS, SUPPORT ASSEMBLIES, FLAT SHEET AND POST REFLECTORS.

ITEM 653 - TOPSOIL FURNISHED AND PLACED, AS PER PLAN

A MINIMUM 3 INCHES OF TOPSOIL SHALL BE PLACED IN ALL AREAS TO BE SEEDED. PRIOR TO PLACING TOPSOIL IN CUT AREAS, THE EARTH SHALL BE EXCAVATED TO A DEPTH SUFFICIENT TO PLACE 3 INCHES OF TOPSOIL. THE COST OF EXCAVATION AND DISPOSAL OF SURPLUS MATERIALS WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF THE VARIOUS CONTRACT ITEMS.

TOPSOIL SHALL BE REMOVED AND WASTED OR UTILIZED IN NON-LOAD BEARING FILLS IN ACCORDANCE WITH THE SPECIFICATIONS. NO EXTRA COMPENSATION SHALL BE PAID FOR THE REMOVAL OF EXCESS TOPSOIL AS REQUIRED TO OBTAIN A SUITABLE SUBGRADE. PAYMENT FOR TOPSOIL REMOVAL IS INCLUDED IN ITEM 203, EXCAVATION.

ALL TOPSOIL MATERIALS AND FINAL AREA OF SUBGRADE PREPARATION SHALL BE FREE FROM ROCK AND OTHER FOREIGN MATERIAL 1/2" OR GREATER IN ANY DIMENSION.

ITEM 659 - SEEDING AND MULCHING, A.P.P.

SEED - CERTIFICATION OF GRASS SEED SHALL BE PROVIDED BY SEED VENDOR FOR EACH GRASS-SEED MIXTURE STATING THE BOTANICAL AND COMMON NAME, PERCENTAGE BY WEIGHT OF EACH SPECIES AND VARIETY; AND PERCENTAGE OF PURITY, GERMINATION, AND WEED SEED. INCLUDE THE YEAR OF PRODUCTION AND DATE OF PACKAGING. FURNISH NATIONAL TURFGRASS EVALUATION PROGRAM (NTEP) DATA FOR EACH SPECIES TO BE USED.

GRASS SEED MUST BE FRESH, CLEAN, DRY, NEW-CROP SEED COMPLYING WITH THE A.O.S.A. "JOURNAL OF SEED TECHNOLOGY" RULES FOR TESTING SEEDS FOR PURITY AND GERMINATION TOLERANCES.

SEED SPECIES SHALL BE AS FOLLOWS, WITH NOT LESS THAN 90 PERCENT GERMINATION, NOT LESS THAN 98 PERCENT PURE SEED, AND NOT MORE THAN 0.5 PERCENT WEED SEED.

TURFGRASS SEED MIX PROPORTIONED BY WEIGHT:

- A. 80 PERCENT TALL FESCUE (FESTUCA ARUNDINACEA), WITH A MINIMUM OF 3 IMPROVED TURF-TYPE VARIETIES. KENTUCKY-31 AND ALTA VARIETIES ARE NOT APPROVED.
- B. 20 PERCENT PERENNIAL RYEGRASS (LOLIUM PERENNE).

SEEDING - SOW SEED AT A TOTAL RATE OF 7-9 LB. / 1,000 SF WITH A SPREADER OR SEEDING MACHINE. RAKE SEED LIGHTLY INTO TOP 1/8 INCH OF SOIL, ROLL LIGHTLY, AND WATER WITH FINE SPRAY. THOROUGHLY COVER WITH STRAW AND NETTING. NETTING SHALL BE ADEQUATELY PINNED TO PREVENT THE STRAW FROM BEING BLOWN OR WASHED AWAY.

PROTECT SEEDED AREAS WITH SLOPES EXCEEDING 3:1 WITH EROSION CONTROL BLANKETS AS DIRECTED BY THE ENGINEER. COST OF EROSION CONTROL BLANKETS, MATERIAL, AND LABOR SHALL BE PAID FOR BY THE CITY.

HYDRO-SEEDING & HYDRO-MULCHING ARE NOT PERMITTED.

TURF MAINTENANCE - MAINTAIN AND ESTABLISH TURF BY WATERING, FERTILIZING, WEEDING, MOWING, TRIMMING, AND REPLANTING TO ESTABLISH HEALTHY, VIABLE TURF. ROLL, REGRADE, AND REPLANT BARE OR ERODED AREAS AND REMULCH TO PRODUCE A UNIFORMLY SMOOTH TURF. PROVIDE THE SAME MATERIALS AND INSTALLATION AS THOSE USED IN THE ORIGINAL INSTALLATION. WATER TURF WITH FINE SPRAY AT A MINIMUM RATE OF 1 INCH PER WEEK UNLESS RAINFALL PRECIPITATION IS ADEQUATE.

MOW TURFGRASS SEED MIX AREAS AS SOON AS TOP GROWTH IS TALL ENOUGH TO CUT. REPEAT MOWING TO MAINTAIN SPECIFIED HEIGHT WITHOUT CUTTING MORE THAN 1/3 OF GRASS HEIGHT. MOW AREAS TO A HEIGHT OF 2 TO 3 INCHES.

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GENERAL NOTES

DUBLIN ROAD SOUTH
MULTI-USE PATH

ITEM 607 – FENCE REBUILT

CAREFULLY RECONDITION AND RE-ERECT FENCE AND COMPONENT PARTS AS DETAILED ON THE PLANS. DO NOT DAMAGE THE FENCE OR COMPONENT PARTS. ANY NEW PARTS WHICH ARE NEEDED, AS DETERMINED BY THE ENGINEER, WILL BE SUPPLIED BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CITY.

THE AMOUNT OF REBUILT FENCE TO BE PAID FOR WILL BE THE NUMBER OF FEET REBUILT, COMPLETE IN PLACE AND MEASURED AS PROVIDED FOR IN 607.10.

PAYMENT FOR THE ABOVE WILL BE PAID FOR AT THE CONTRACT PRICE PER FOOT FOR ITEM 607 – FENCE REBUILT.

FENCE LENGTHS

THE LENGTHS OF FENCE SHOWN IN THE PLANS ARE HORIZONTAL DIMENSIONS. MEASUREMENTS OF THE FINAL QUANTITIES WILL BE IN ACCORDANCE WITH ITEM 607.

PRIVATE LANDSCAPING ITEMS

CONTRACTOR SHALL CAREFULLY REMOVE AND DELIVER TO THE HOME OWNER PRIVATE LANDSCAPING ITEMS LOCATED WITHIN THE CONSTRUCTION LIMITS. SUCH ITEMS INCLUDE BUT ARE NOT LIMITED TO LANDSCAPE LIGHTING, ORNAMENTAL FENCES, STONE BOULDERS, ETC.

ROCK CUT EXCAVATION

ROCK CUT ELEVATIONS SHOWN IN THESE PLANS REPRESENT A CULMINATION OF GEOTECHNICAL INVESTIGATIONS PERFORMED FOR THIS PROJECT AND REPORTS COMPLETED FOR THE UPPER SCIOTO WEST INTERCEPTOR PROJECT. IT IS THE INTENT OF THIS PROJECT TO PROVIDE THE CONTRACTOR A QUANTITY OF ROCK CUT EXCAVATION BASED ON THE PREVIOUS AND CURRENT REPORTS. THEREFORE, IN ADDITION TO THE QUANTITIES PROVIDED IN THE CROSS SECTIONS, THE FOLLOWING CONTINGENCY QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY FOR USE AS DIRECTED BY THE ENGINEER.

ITEM 203 – ROCK CUT EXCAVATION 15 CY

ITEM SPECIAL – BRICK WALK, AS PER PLAN
ITEM SPECIAL – BRICK STEPS, AS PER PLAN

THIS ITEM SHALL INCLUDE THE REMOVAL OF EXISTING BRICK WALK, EXCAVATION TO THE APPROPRIATE DEPTH, CONSTRUCTION OF THE BASE AS DETAILED ON THE TYPICAL SECTIONS, AND PLACEMENT OF THE EXISTING BRICK. ALL MATERIALS, LABOR, AND EQUIPMENT REQUIRED TO PERFORM THE WORK DESCRIBED ABOVE SHALL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM SPECIAL-BRICK WALK, AS PER PLAN AND ITEM SPECIAL-BRICK STEPS, AS PER PLAN.

NEOPRENE-MODIFIED ASPHALT ADHESIVE: FURNISH NEOPRENE MODIFIED ASPHALT ADHESIVE THAT CONTAINS 2% GRADE WM1 NEOPRENE, OXIDIZED ASPHALT WITH A 150 DEGREE SOFTENING POINT (77 PENETRATION), AND 10% LONG-FIBERED INERT MATERIAL, AS SUPPLIED BY SEIDEL COMPANY, INC., NEWBURYPORT, MA, (617) 649-6740; HASTINGS PAVEMENT COMPANY, INC., LAKE SUCCESS, NY, (516) 379-3500; OR APPROVED EQUAL.

BITUMINOUS SETTING BED, 3/4-INCH: FURNISH ASPHALT CEMENT CONFORMING TO ASTM D 3381, PG64-22. FINE AGGREGATE SHALL BE NATURAL SAND AND/OR STONE SAND COMPOSED OF HARD, DURABLE, UNCOATED PARTICLES, FREE FROM CLAY, SILT, ORGANIC MATERIAL, OR OTHER DELETERIOUS SUBSTANCES. ALL SAND SHALL BE UNIFORMLY GRADED AND PASS A NO. 4 SIEVE, MEETING THE REQUIREMENTS OF ASTM C 136. THE DRIED FINE AGGREGATE SHALL BE MIXED WITH HOT ASPHALT CEMENT AT THE PLANT AND HEATED TO APPROXIMATELY 300 DEGREES (F). APPROXIMATE MATERIAL PROPORTIONS SHALL BE 7% ASPHALT CEMENT AND 93% FINE AGGREGATE; OR 140 LBS ASPHALT TO 1,860 LBS FINE AGGREGATE PER TON.

SAND: ALL JOINTS SHALL BE FILLED WITH A POLYMERIC SAND. SAND SHALL BE ALLIANCE XXX GATOR SAND AND BEIGE IN COLOR OR AN APPROVED EQUAL.

EXISTING STONE WALLS

THE INTENT OF THIS PROJECT IS TO RELOCATE THE EXISTING STONE WALLS USING THE STONE FROM THE EXISTING WALLS. PROVIDE A 2-FOOT OFFSET FROM THE EDGE OF PAVEMENT OF THE MULTI-USE PATH. THE CONTRACTOR SHALL CAREFULLY REMOVE, SALVAGE, AND STORE THE EXISTING STONE. SHOULD THE CONTRACTOR NEED TO SUPPLEMENT THE EXISTING STONE DUE TO A SHORTAGE OF USABLE STONE, HE SHALL ORDER SUPPLEMENT STONE THAT CLOSELY MATCHES EXISTING STONE IN SIZE, SHAPE AND COLOR. PAYMENT FOR THE STONE WALL REMOVAL, SALVAGE, AND STORAGE SHALL BE PAID FOR AT THE UNIT BID PRICE PER FOOT OF ITEM SPECIAL – WALL REMOVED AND SALVAGED.

THE FOLLOWING CONTINGENCY QUANTITY FOR SUPPLEMENTAL STONE, SUPPLIED BY NORTHSORE, HAS BEEN CARRIED TO THE GENERAL SUMMARY

ITEM SPECIAL – SUPPLEMENTAL STONE 1 TON

PAYMENT FOR THE PLACEMENT OF STONE WALLS, AS DETAILED IN THESE PLANS SHALL BE PAID FOR AT THE UNIT BID PRICE PER FOOT OF:

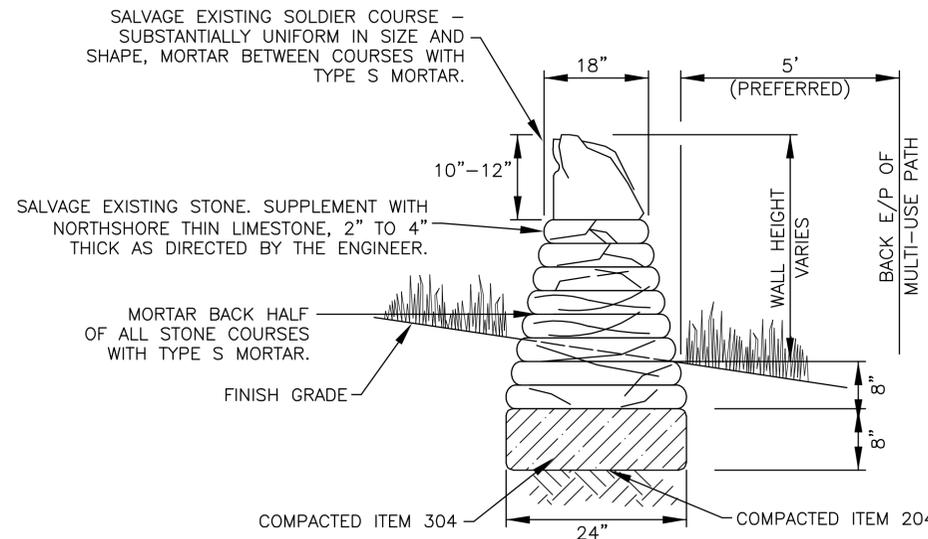
ITEM SPECIAL – RELOCATED STONE WALL, UP TO 18”
ITEM SPECIAL – RELOCATED STONE WALL, OVER 18” TO 36”
ITEM SPECIAL – RELOCATED STONE WALL, OVER 36”

STONE WALL RELOCATION					
WALL #	LOCATION	LENGTH	COLUMN(S)	WALL HEIGHT *	SHEET NO.
1	22+63.78 – 22+95.88	32'		24"	11
2	23+07.49 – 23+33.94	27'		18"	11

* WALL HEIGHT = EX. GRADE TO TOP OF WALL

REFERENCE POINTS – MULTI USE PATH			
DESCRIPTION	OFFSET	NORTHING	EASTING
P.O.T. 20+00.00	0'	759196.56	1796435.35
P.I. 20+09.37	0'	759202.00	1796427.73
P.C. 20+29.52	0'	759221.69	1796423.47
P.I. 20+40.02	0.55'	759231.96	1796421.25
P.R.C. 20+50.45	0'	759241.54	1796416.95
P.I. 20+58.91	0.36'	759249.27	1796413.48
P.T. 20+67.34	0'	759257.46	1796411.36
P.C. 21+83.78	0'	759370.19	1796382.20
P.I. 21+94.28	0.55'	759380.36	1796379.57
P.R.C. 22+04.72	0'	759390.86	1796379.11
P.I. 22+15.06	0.53'	759401.20	1796378.66
P.T. 22+25.33	0'	759411.22	1796376.10
P.C. 23+38.26	0'	759520.64	1796348.18
P.I. 23+52.39	0.99'	759534.33	1796344.68
P.T. 23+66.34	0'	759546.52	1796337.53
P.C. 24+45.90	0'	759615.14	1796297.26
P.I. 24+82.89	14.48'	759647.04	1796278.54
P.T. 25+05.60	0'	759668.20	1796308.87
P.C. 25+22.65	0'	759677.95	1796322.86
P.I. 25+46.70	6.67'	759691.71	1796342.58
P.R.C. 25+65.95	0'	759715.58	1796339.68
P.I. 25+77.22	0.63'	759726.76	1796338.33
P.T. 25+88.39	0'	759737.96	1796339.49
P.C. 32+26.50	0'	760372.65	1796405.49
P.I. 32+28.59	0.42'	760374.74	1796405.70
P.T. 32+30.47	0'	760376.04	1796407.34
P.O.T. 32+39.74	0'	760381.83	1796414.59

REFERENCE POINTS – RINGS ROAD			
DESCRIPTION	OFFSET	NORTHING	EASTING
P.O.T. 600+00.00	0'	760405.21	1796239.50
P.O.T. 602+00.00	0'	760424.13	1796438.60



NOTES:

- BATTER WALL-BOTH FACES. MAINTAIN COURSES LEVEL AND SHIM WITH STONE CHIPS TO PROVIDE STABLE COURSING. AMOUNT OF SHIMS TO BE APPROVED BY ENGINEER.
- NORTHSORE THIN LIMESTONE (BUFF TO BLUE VEIN COLOR) QUARRIED IN OHIO PROVIDED BY NORTHSORE OF COLUMBUS (OR APPROVED EQUAL).

DRY-LAID STONE WALL DETAIL

N.T.S.

MCDANIEL, ARRON U:\173608736\design\plan_set\herford to Rings\173608736gn01.dwg GENERAL NOTES AND REFERENCE POINTS Last Saved: Mar. 11, 2013 3:56 PM

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GENERAL NOTES AND REFERENCE POINTS

DUBLIN ROAD SOUTH
MULTI-USE PATH

GENERAL NOTES FOR WATER LINES

ALL WATER MAIN MATERIALS AND INSTALLATIONS SHALL BE IN ACCORDANCE WITH THE CURRENT RULES AND REGULATIONS OF THE CITY OF COLUMBUS, DIVISION OF POWER AND WATER (WATER).

IT SHALL BE UNLAWFUL FOR ANY PERSON TO PERFORM ANY WORK ON CITY OF COLUMBUS WATER LINE SYSTEMS WITHOUT FIRST SECURING LICENSE TO ENGAGE IN SUCH WORK, AS INDICATED IN COLUMBUS CITY CODE SECTION 1103.02 AND 1103.06. THIS WORK INCLUDES ANY ATTACHMENTS, ADDITIONS TO OR ALTERATIONS IN ANY CITY SERVICE PIPE OR APPURTENANCES (INCLUDING WATER SERVICE LINES AND TAPS). THIS REQUIREMENT MAY BE MET BY UTILIZATION OF A SUBCONTRACTOR WHO HOLDS A CITY OF COLUMBUS WATER CONTRACTOR LICENSE OR A COMBINED WATER/SEWER CONTRACTOR LICENSE TO PERFORM THIS WORK. UTILIZATION OF A SUBCONTRACTOR MUST MEET THE LICENSING REQUIREMENTS OF CITY OF COLUMBUS BUILDING CODE, IN PARTICULAR SECTION 4114.119 AND 4114.529.

THE CONTRACTOR SHALL OBTAIN THE PROPER HYDRANT PERMIT(S), AND PAY ANY APPLICABLE FEES, FOR ANY APPROVED HYDRANT USAGE DEEMED NECESSARY FOR WORK UNDER THIS IMPROVEMENT. PERMITS MAY BE OBTAINED THROUGH THE DIVISION OF POWER AND WATER (WATER) PERMIT OFFICE (645-7330). THE CONTRACTOR SHALL ADHERE TO ALL RULES AND REGULATIONS GOVERNING SAID PERMIT AND MUST HAVE THE ORIGINAL PERMIT ON SITE ANYTIME IN WHICH THE HYDRANT IS IN USE. COST TO BE INCLUDED IN THE VARIOUS BID ITEMS.

ALL WATER LINES WITH A DIAMETER 3" TO 8" SHALL BE DUCTILE IRON, CLASS 53. WATER LINES WITH A DIAMETER 12" OR LARGER SHALL BE DUCTILE IRON, CLASS 54.

THE CONTRACTOR SHALL WRAP ALL DUCTILE IRON PIPE WITH TUBE STYLE 8 MIL LINEAR LOW DENSITY POLYETHYLENE (LLDPE) FILM MADE FROM VIRGIN POLYETHYLENE ONLY (NO RECYCLED MATERIAL) PER A.W.W.A. C-105. THE COLOR SHALL BE BLACK WITH NOMINAL 2% CARBON BLACK UV INHIBITOR AND PRINTED PER THE A.W.W.A. C-105 STANDARD. INSTALLATION SHALL BE DONE IN ACCORDANCE WITH THE "POLYETHYLENE ENCASEMENT INSTALLATION GUIDE" WRITTEN BY THE DUCTILE IRON PIPE RESEARCH ASSOCIATION (DIPRA).

ALL LUMPS OF CLAY, MUD, CINDERS, ETC. ON THE PIPE SURFACE SHALL BE REMOVED BEFORE THE PIPE IS COVERED WITH POLYETHYLENE. WHEN LIFTING POLYETHYLENE-ENCASED PIPE USE A FABRIC TYPE SLING OR PADDED CABLE. JOINTS SHALL BE OVERLAPPED AND TAPED. FOLD EXCESS SLACK OVER THE TOP OF THE PIPE AND TAPE EVERY THREE (3) FEET. SMALL HOLES OR TEARS SHALL BE REPAIRED WITH A PIECE OF TAPE PLACED OVER THE HOLE. LARGE HOLES OR TEARS SHOULD BE REPAIRED BY TAPING ANOTHER PIECE OF POLYETHYLENE OVER THE HOLE. TO AVOID DAMAGE DURING BACKFILLING ALLOW ADEQUATE SLACK IN THE FILM TUBE AT JOINTS. BACKFILL MATERIAL SHALL BE FREE OF CINDERS, ROCKS, BOULDERS, NAILS, STICKS OR OTHER MATERIAL THAT WILL DAMAGE THE POLYETHYLENE SLEEVE.

APPURTENANCES SUCH AS BOLTED JOINTS, VALVES, SERVICE TAPS SHALL ALSO BE WRAPPED. FOR SERVICE TAPS, WRAP THREE LAYERS OF TAPE COMPLETELY AROUND THE POLYETHYLENE-ENCASED PIPE TO COVER THE AREA WHERE TAPPING MACHINE WILL CONTACT THE PIPE. INSTALL CORPORATION STOP DIRECTLY THROUGH THE TAPE AND POLYETHYLENE. WRAP COPPER SERVICE AT LEAST THREE FEET BACK FROM INSTALLATION WITH TAPE AND ADDITIONAL POLYETHYLENE FILM TO PREVENT ELECTROLYSIS.

ALL WORK NECESSARY TO FURNISH AND INSTALL THE POLYETHYLENE ENCASEMENT SHALL BE INCLUDED IN ITEM 801.

ONLY FIRE HYDRANTS CONFORMING TO THE CITY OF COLUMBUS STANDARDS WILL BE APPROVED FOR USE, EXCEPT THAT THE NOZZLE SHALL BE THREADED TO THE BARREL OF THE HYDRANT.

THE CONTRACTOR SHALL PAINT ALL FIRE HYDRANTS ACCORDING TO THE CITY OF DUBLIN STANDARDS WA-01. THE COST OF PAINTING FIRE HYDRANTS SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR FIRE HYDRANTS.

ALL WATER LINES SHALL BE PRESSURE TESTED IN ACCORDANCE WITH SECTION 801.12 OF THE CITY OF COLUMBUS CONSTRUCTION AND MATERIAL SPECIFICATIONS.

CITY OF COLUMBUS WATER DIVISION PERSONNEL ARE TO OPERATE ALL WATER VALVES.

WATER LINES SHALL BE DISINFECTED BY THE CITY OF COLUMBUS DIVISION OF WATER. REQUESTS FOR WATER LINE CHLORINATION SHALL BE MADE THROUGH THE CITY OF DUBLIN, DIVISION OF ENGINEERING. THE COST FOR CHLORINATION SHALL BE PAID FOR BY THE CONTRACTOR.

ALL WATER MAINS SHALL BE DISINFECTED IN ACCORDANCE WITH SECTION 801.13 OF THE CITY OF COLUMBUS CONSTRUCTION AND MATERIAL SPECIFICATIONS. SPECIAL ATTENTION IS DIRECTED TO APPLICABLE SECTIONS OF A.W.W.A. C-651. WHEN THE WATER MAINS ARE READY FOR DISINFECTION, **THE CITY OF DUBLIN SHALL SUBMIT TWO (2) SETS OF "AS-BUILT" PLANS**, AND A LETTER STATING THAT THE WATER MAINS HAVE BEEN PRESSURE TESTED AND NEED TO BE DISINFECTED, TO THE CITY OF COLUMBUS, DIVISION OF POWER AND WATER (WATER) THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL COSTS ASSOCIATED WITH THE DISINFECTION OF ALL WATER MAINS CONSTRUCTED UNDER THIS PLAN.

STATIONS AND OFFSETS FOR THE LOCATION OF THE PROPOSED WATER MAIN ARE REFERENCED TO THE SURVEY BASELINE.

ALL BENDS, JOINT DEFLECTIONS AND FITTINGS SHALL BE BACKED WITH CONCRETE PER CITY OF COLUMBUS STANDARDS.

ALL WATER LINES SHALL BE LOCATED AT LEAST 5 FEET HORIZONTALLY AND 18 INCHES VERTICALLY FROM SANITARY AND STORM SEWERS.

THE CONTRACTOR SHALL NOTIFY THE CITY OF COLUMBUS, DIVISION OF WATER AT 614-645-7788 AND THE CITY OF DUBLIN, DIVISION OF ENGINEERING AT LEAST 24 HOURS BEFORE THE SHUT DOWN OF ANY WATER MAIN OR TAPPING INTO EXISTING WATER LINES.

THE CONTRACTOR SHALL GIVE WRITTEN NOTICE TO ALL AFFECTED PROPERTY OWNERS AT LEAST 1 WORKING DAY BUT NOT MORE THAN 3 WORKING DAYS PRIOR TO ANY TEMPORARY INTERRUPTION OF WATER SERVICE. INTERRUPTION OF WATER SERVICE SHALL BE HELD TO A MINIMUM AND SHALL BE APPROVED BY THE CITY ENGINEER.

IF THE TOP OF THE OPERATING NUT OF ANY VALVE IS GREATER THAN 36 INCHES BELOW FINISHED GRADE, AN EXTENSION STEM SHALL BE FURNISHED TO BRING THE TOP OF THE OPERATING NUT TO WITHIN 24 INCHES OF FINISHED GRADE ELEVATION.

WATER LINES SHALL BE PLACED AT A MINIMUM DEPTH OF 4 FEET MEASURED FROM THE TOP OF FINISHED GRADE TO THE TOP OF THE WATER LINE. WATER LINES SHALL BE SET DEEPER AT ALL POINTS WHERE NECESSARY TO CLEAR EXISTING OR PROPOSED UTILITY LINES OR OTHER UNDERGROUND RESTRICTIONS BY A MINIMUM OF 18 INCHES.

ALL WATER LINE TRENCHES WITHIN RIGHT-OF-WAY SHALL BE BACKFILLED WITH COMPACTED GRANULAR MATERIAL PER ITEM 304.02. THE COST SHALL BE INCLUDED IN THE UNIT BID PRICE FOR ITEM 801 - DUCTILE IRON WATER PIPE AND FITTINGS.

GRANULAR BACKFILL AND BEDDING SHALL CONSIST OF NATURAL BROKEN OR CRUSHED ROCK. SYNTHETIC OR MAN-MADE MATERIALS ARE UNACCEPTABLE.

THE CONTRACTOR SHALL ADJUST ALL EXISTING VALVE BOXES TO FINAL GRADE IN BOTH PAVEMENT AND NON-PAVEMENT AREAS IN ACCORDANCE WITH ITEM 807.

ALL NEW AND EXISTING VALVES LOCATED WITHIN THE PROPOSED PAVEMENT AREA SHALL HAVE COLUMBUS STANDARD HEAVY DUTY VALVE BOXES IN ACCORDANCE WITH ITEM 807. EACH NEW OR REPLACEMENT HEAVY DUTY BOX WILL BE PAID FOR SEPARATELY AT THE PRICE BID PER EACH.

THE CONTRACTOR SHALL COORDINATE HIS WORK SUCH THAT NO WATER CUSTOMER WILL HAVE THEIR SERVICE DISRUPTED MORE THAN TWO (2) TIMES THROUGHOUT THE DURATION OF THIS PROJECT.

ITEM SPECIAL - SURVEY COORDINATES

THIS ITEM SHALL INCLUDE ALL MATERIAL, EQUIPMENT, AND LABOR NECESSARY TO OBTAIN HORIZONTAL AND VERTICAL (NORTHING, EASTING, AND ELEVATION) SURVEY COORDINATES FOR THE WATER MAIN IMPROVEMENTS. THE SURVEY COORDINATES SHALL BE OBTAINED FOR THE COMPLETED WATER MAIN CONSTRUCTION AND SHALL INCLUDE ALL VALVES, TEES, CROSSES, BENDS, DEFLECTIONS, PLUGS, REDUCERS, TAPPING SLEEVES, BLOW OFFS, CHLORINATION TAPS, FIRE HYDRANTS, AIR RELEASES, CURB STOPS, CASING PIPE TERMINI, AND OTHER FITTINGS. ADDITIONAL SURVEY COORDINATES ARE REQUIRED ON THE WATER MAIN EVERY 500' WHERE NO FITTING OR OTHER WATER MAIN STRUCTURE IS BEING INSTALLED WITHIN THAT LENGTH OF THE IMPROVEMENT.

ALL SURVEY COORDINATES SHALL BE REFERENCED TO THE APPLICABLE COUNTY ENGINEER'S MONUMENTS, AND SHALL BE BASED ON THE NORTH AMERICAN DATUM OF 1983 (NAD 83) WITH THE (1986) ADJUSTMENT, WITH FURTHER REFERENCE MADE TO THE OHIO STATE PLANE SOUTH COORDINATE SYSTEM, SOUTH ZONE, WITH ELEVATIONS BASED ON NAVD 88 DATUM. ALL COORDINATES (NORTHING, EASTING, ELEVATION) SHALL BE REFERENCED TO THE NEAREST HUNDREDTH (N XXXXXX.XX, E XXXXXX.XX, ELEV. XXX.XX). ALL SURVEY COORDINATES SHALL BE ACCURATE TO WITHIN 1.0 FOOT OR LESS.

THE COORDINATES SHALL BE DOCUMENTED TO THE MUNICIPALITY ENGINEER OR DESIGNATED REPRESENTATIVE IN DIGITAL SPREADSHEET FORM AND SHALL INCLUDE THE APPLICABLE ITEM, STATION, NORTHING, EASTING, AND ELEVATION. COORDINATES SHALL BE SUBMITTED TO THE MUNICIPALITY ENGINEER OR DESIGNATED REPRESENTATIVE ON A BI-WEEKLY BASIS. COORDINATES SHALL ALSO BE REQUIRED TO BE SUBMITTED TO THE DIVISION OF POWER AND WATER AS PART OF THE REQUEST FOR CHLORINATION.

LUMP SUM PAYMENT IS FULL COMPENSATION FOR ALL WORK INVOLVED IN OBTAINING AND DOCUMENTING THE SURVEY COORDINATES AS DESCRIBED IN THIS SPECIFICATION.

ITEM	STA.	NORTHING	EASTING	AS-BUILT		
				NORTHING	EASTING	C/L ELEV.
VALVE ATG	20+23.82	759214.27	1796416.07			
VALVE ATG	20+27.85	759218.14	1796414.89			
SERVICE BOX ATG	21+90.81	759375.24	1796370.81			
VALVE ATG W/ HD BOX	22+90.23	759472.53	1796353.91			
SERVICE BOX ATG	25+12.52	759684.35	1796306.04			
SERVICE BOX ATG	26+96.77	759846.83	1796340.43			
VALVE ATG W/ HD BOX	27+11.85	759861.18	1796348.30			
SERVICE BOX ATG	28+24.57	759973.95	1796353.61			
SERVICE BOX ATG	29+59.57	760108.23	1796367.57			
VALVE ATG W/ HD BOX	30+98.54	760245.78	1796388.33			
SERVICE BOX ATG	31+45.55	760292.97	1796389.15			
VALVE ATG	32+47.01	760393.05	1796400.69			

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GENERAL NOTES

DUBLIN ROAD SOUTH MULTI-USE PATH

MCDANIEL, ARRON U:\173608736\design\plan_set\Hertford to Rings\Maint of Traffic\173608736mtn01.dwg MAINTENANCE OF TRAFFIC Last Saved: Feb 05, 2013 10:51 AM, USNAR Plotted: Mar 11, 2013 3:56 PM

ITEM 614 – MAINTAINING TRAFFIC

ACCESS TO ALL ADJOINING PROPERTIES AS WELL AS ACCESS FOR MAIL, WATER, SANITARY SERVICE, AND EMERGENCY VEHICLES SHALL BE MAINTAINED THROUGHOUT THE DURATION OF THE PROJECT.

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATIONS, AS WELL AS THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR 614, MAINTAINING TRAFFIC, UNLESS SEPARATELY ITEMIZED IN THE PLAN.

IN THE EVENT THAT IT BECOMES NECESSARY FOR THE CITY TO PERFORM WORK OF AN IMMEDIATE NATURE (SUCH AS THE PLACEMENT OF BARRICADES OR REPLACEMENT OF SIGNS AND OTHER WARNING OR PROTECTIVE DEVICES) BECAUSE OF FAILURE OR REFUSAL OF THE CONTRACTOR TO PERFORM SUCH WORK AS REQUIRED BY THE CONTRACT, THE CONTRACTOR SHALL REIMBURSE THE CITY AT A RATE OF 2.5 TIMES THE ACTUAL COST OF THE LABOR, MATERIALS, AND EQUIPMENT NECESSARY TO PERFORM SUCH WORK. THE CITY SHALL BE REQUIRED TO NOTIFY OR ATTEMPT TO NOTIFY THE DESIGNATED REPRESENTATIVE OF THE CONTRACTOR OF THE NECESSITY TO PERFORM SUCH WORK. IF THE CONTRACTOR REFUSES OR FAILS WITHIN A REASONABLE TIME TO PERFORM OR CAUSE THE PERFORMANCE OF SUCH WORK, THE CITY SHALL BE REIMBURSED BY THE CONTRACTOR IN THE AMOUNT PROVIDED HEREIN BY WAY OF A DEDUCTION FROM THE CONTRACTOR'S NEXT PAYMENT UNDER THE CONTRACT. REASONABLE TIME FOR ALL STREETS INVOLVED ON THIS CONTRACT IS 2 HOURS FROM THE TIME OF NOTIFICATION BY THE CITY.

SIGNING FOR THE DUBLIN ROAD DETOUR ROUTE AS SHOWN ON SHEET 9 SHALL BE IN PLACE PRIOR TO THE COMMENCEMENT OF WORK.

ALTERNATE METHODS

IF THE CONTRACTOR SO ELECTS, THEY MAY SUBMIT ALTERNATE METHODS FOR THE MAINTENANCE OF TRAFFIC, PROVIDED THE INTENT OF THE ABOVE PROVISIONS IS FOLLOWED AND NO ADDITIONAL INCONVENIENCE TO THE TRAVELING PUBLIC RESULTS THEREFROM. NO ALTERNATE PLAN SHALL BE PLACED INTO EFFECT UNTIL APPROVAL HAS BEEN GRANTED, BY THE CITY ENGINEER.

ITEM 614 – LAW ENFORCEMENT OFFICER WITH PATROL CAR, A.P.P.

USE OF LAW ENFORCEMENT OFFICERS (LEOS) BY CONTRACTORS OTHER THAN THE USES SPECIFIED IN THIS NOTE WILL NOT BE PERMITTED AT PROJECT COST UNLESS PRIOR APPROVAL HAS BEEN OBTAINED FROM THE ENGINEER. LEOS SHOULD NOT BE USED WHERE THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (OMUTCD) INTENDS FOR FLAGGERS BE USED.

IN ADDITION TO THE REQUIREMENTS OF CMS 614 AND THE OMUTCD, A UNIFORMED LEO WITH AN OFFICIAL PATROL CAR (CAR WITH TOP-MOUNTED EMERGENCY FLASHING LIGHTS AND COMPLETE MARKINGS OF THE APPROPRIATE LAW ENFORCEMENT AGENCY) SHOULD BE PROVIDED FOR CONTROLLING TRAFFIC FOR THE FOLLOWING TASKS:

- A. FOR LANE CLOSURES: DURING INITIAL SET-UP PERIODS, TEAR DOWN PERIODS, SUBSTANTIAL SHIFTS OF A CLOSURE POINT OR WHEN NEW LANE CLOSURE ARRANGEMENTS ARE INITIATED. IN GENERAL, LEOS SHOULD BE POSITIONED AT THE POINT OF LANE RESTRICTION OR ROAD CLOSURE AND TO MANUALLY CONTROL TRAFFIC MOVEMENTS THROUGH INTERSECTIONS IN WORK ZONES.
- B. DURING THE ENTIRE ADVANCE PREPARATION AND CLOSURE SEQUENCE WHERE BLOCKAGE OF TRAFFIC IS REQUIRED.

LEOS WORK AT THE DIRECTION OF THE CONTRACTOR. THE CONTRACTOR IS RESPONSIBLE FOR SECURING THE SERVICES OF THE LEOS AND COMMUNICATING THE INTENTIONS OF THE PLANS WITH RESPECT TO DUTIES OF THE LEOS. THE ENGINEER SHALL HAVE FINAL CONTROL OVER THE LEOS' DUTIES AND PLACEMENT AND WILL RESOLVE ANY ISSUES THAT MAY ARISE BETWEEN THE TWO PARTIES. THE CONTRACTOR SHALL UTILIZE ANY OF THE FOLLOWING LAW ENFORCEMENT AGENCY(S): CITY OF DUBLIN, FRANKLIN COUNTY SHERIFF'S OFFICE, OR OHIO STATE HIGHWAY PATROL.

LAW ENFORCEMENT OFFICERS WITH PATROL CAR REQUIRED BY THE TRAFFIC MAINTENANCE TASKS ABOVE SHALL BE PAID FOR ON AN HOURLY BASIS UNDER ITEM 614 – LAW ENFORCEMENT OFFICE WITH PATROL CAR, AS PER PLAN. THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY.

ITEM 614 – LAW ENFORCEMENT OFFICER WITH PATROL CAR, A.P.P. 20 HOURS

THE HOURS PAID SHALL INCLUDE MINIMUM SHOW-UP TIME REQUIRED BY THE LAW ENFORCEMENT AGENCY INVOLVED. ANY ADDITIONAL COSTS (ADMINISTRATIVE OR OTHERWISE) INCURRED BY THE CONTRACTOR TO OBTAIN THE SERVICES OF LEOS ARE INCLUDED WITH THE BID UNIT PRICE FOR ITEM 614 – LAW ENFORCEMENT OFFICER WITH PATROL CAR, AS PER PLAN.

TRENCH FOR WIDENING

THE OPEN TRENCH SHALL BE ADEQUATELY MAINTAINED AND PROTECTED WITH DRUMS OR BARRICADES AT ALL TIMES. PLACEMENT OF PROPOSED SUBBASE AND BASE MATERIAL SHALL FOLLOW AS CLOSELY AS POSSIBLE BEHIND EXCAVATION OPERATIONS. THE LENGTH OF WIDENING TRENCH WHICH IS OPEN AT ANY ONE TIME SHALL BE HELD TO A MINIMUM AND SHALL AT ALL TIMES BE SUBJECT TO APPROVAL OF THE ENGINEER.

DROPOFFS IN WORKZONE

THE DROPOFF ADJACENT TO THE TRAVELED LANE SHALL BE NO GREATER THAN 1.5 INCHES BELOW THE EXISTING PAVEMENT BY THE END OF EACH WORK DAY. THIS REQUIREMENT MAY BE MET BY TEMPORARILY PLACING SUBBASE AND BASE MATERIAL TO WITHIN 1.5 INCHES OF THE EXISTING GRADE ADJACENT TO THE TRAVELED LANE AND SLOPING THE MATERIAL AT 3:1 OR FLATTER WITHIN THE EXCAVATED AREA. PLACEMENT OF PROPOSED SUBBASE AND BASE MATERIAL SHALL FOLLOW AS CLOSELY AS POSSIBLE BEHIND EXCAVATION OPERATIONS DURING WORKING HOURS. THESE REQUIREMENTS SHALL BE MET AT NO ADDITIONAL COST.

DUST CONTROL

THE CONTRACTOR SHALL FURNISH AND APPLY WATER FOR DUST CONTROL AS DIRECTED BY THE ENGINEER. THE FOLLOWING CONTINGENCY QUANTITY HAS BEEN INCLUDED FOR DUST CONTROL PURPOSES:

ITEM 616 – WATER 5 M GAL.

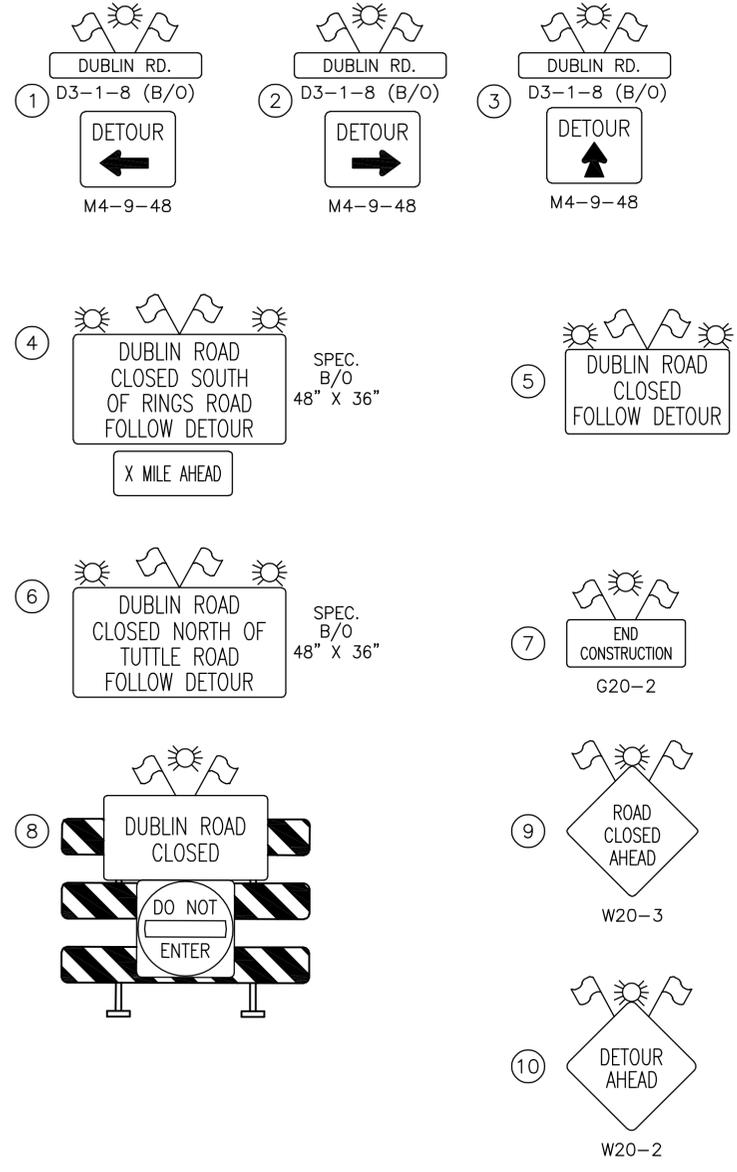
DRUM REQUIREMENTS

PAYMENT FOR DRUMS SHALL BE INCLUDED IN THE LUMP SUM BID FOR MAINTAINING TRAFFIC, UNLESS SEPARATELY ITEMIZED.

CALCULATED
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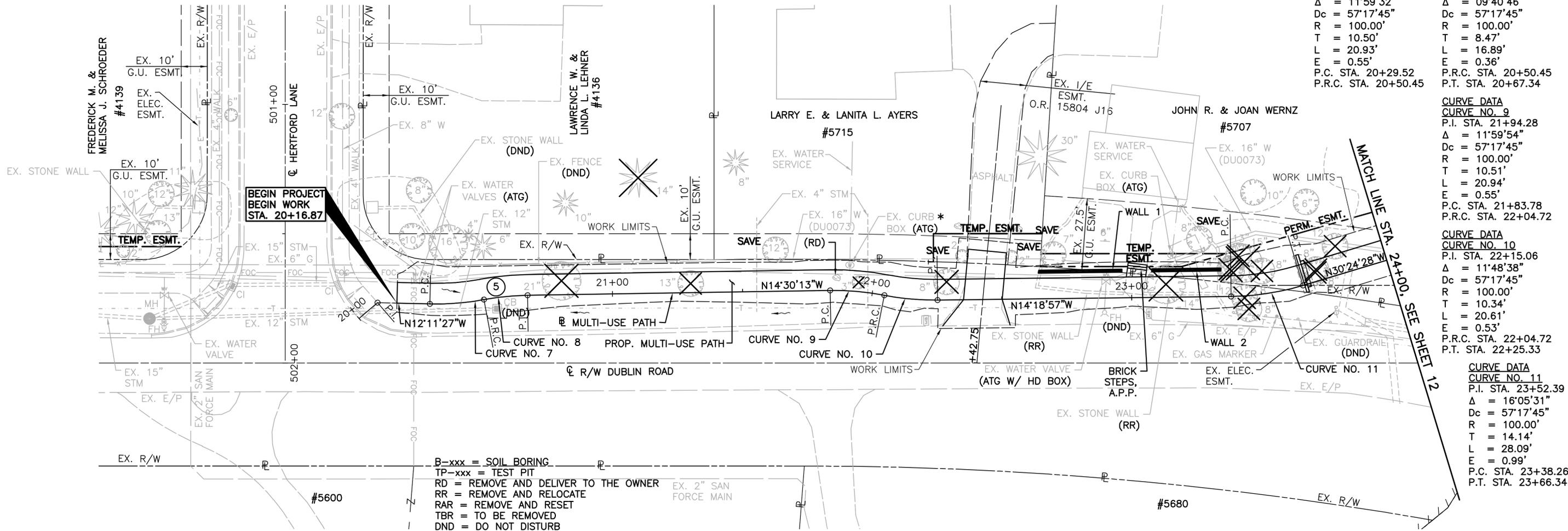
MAINTENANCE OF TRAFFIC
GENERAL NOTES

DUBLIN ROAD SOUTH
MULTI-USE PATH CONSTRUCTED ON
THE WEST SIDE OF DUBLIN RD



LEGEND:
 T - SIGN ON POST
 H - TYPE III BARRICADE
 —> - DETOUR ROUTE

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CURVE DATA CURVE NO. 7		CURVE DATA CURVE NO. 8	
P.I. STA.	20+40.02	P.I. STA.	20+58.91
Δ	11°59'32"	Δ	09°40'46"
Dc	57'17'45"	Dc	57'17'45"
R	100.00'	R	100.00'
T	10.50'	T	8.47'
L	20.93'	L	16.89'
E	0.55'	E	0.36'
P.C. STA.	20+29.52	P.R.C. STA.	20+50.45
P.R.C. STA.	20+50.45	P.T. STA.	20+67.34

CURVE DATA CURVE NO. 9	
P.I. STA.	21+94.28
Δ	11°59'54"
Dc	57'17'45"
R	100.00'
T	10.51'
L	20.94'
E	0.55'
P.C. STA.	21+83.78
P.R.C. STA.	22+04.72

CURVE DATA CURVE NO. 10	
P.I. STA.	22+15.06
Δ	11°48'38"
Dc	57'17'45"
R	100.00'
T	10.34'
L	20.61'
E	0.53'
P.C. STA.	22+04.72
P.T. STA.	22+25.33

CURVE DATA CURVE NO. 11	
P.I. STA.	23+52.39
Δ	16°05'31"
Dc	57'17'45"
R	100.00'
T	14.14'
L	28.09'
E	0.99'
P.C. STA.	23+38.26
P.T. STA.	23+66.34

- B-xxx = SOIL BORING
- TP-xxx = TEST PIT
- RD = REMOVE AND DELIVER TO THE OWNER
- RR = REMOVE AND RELOCATE
- RAR = REMOVE AND RESET
- TBR = TO BE REMOVED
- DND = DO NOT DISTURB
- ATG = ADJUST TO GRADE
- * = PER RECORD PLAN, CONTRACTOR TO LOCATE
- OHWM = ORDINARY HIGH WATER MARK
- G.U. ESMT = GENERAL UTILITY EASEMENT
- DRN. ESMT = DRAINAGE EASEMENT
- I/E = INGRESS/EGRESS EASEMENT

FOR DRIVEWAY DETAILS, SEE SHEET 20
 FOR GRADING PLAN, SEE SHEET 23
 FOR TREE PRESERVATION PLAN, SEE SHEET 26
 FOR TRAFFIC CONTROL PLAN, SEE SHEET 28
 FOR BRIDGE PLAN, SEE SHEETS 30-34

PROPOSED GRADE	845	840	835	830	825	820	815	EXISTING GRADE
845	845	840	835	830	825	820	815	845
840	840	840	835	830	825	820	815	840
835	835	840	835	830	825	820	815	835
830	830	840	835	830	825	820	815	830
825	825	840	835	830	825	820	815	825
820	820	840	835	830	825	820	815	820
815	815	840	835	830	825	820	815	815
EXISTING GRADE	830.83	830.76	830.84	830.90	830.67	830.43	830.33	830.20
	829.46	829.63	828.75	827.86	826.82	825.16	824.80	824.85
	827.07	828.55	828.24	828.54	828.06	827.81		
	20+00	16+00	17+00	18+00	19+00			

CALCULATED	CNCK	CHECKED	TJS
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PLAN AND PROFILE
STA. 24+00 BACK

DUBLIN ROAD SOUTH
MULTI-USE PATH

11
37

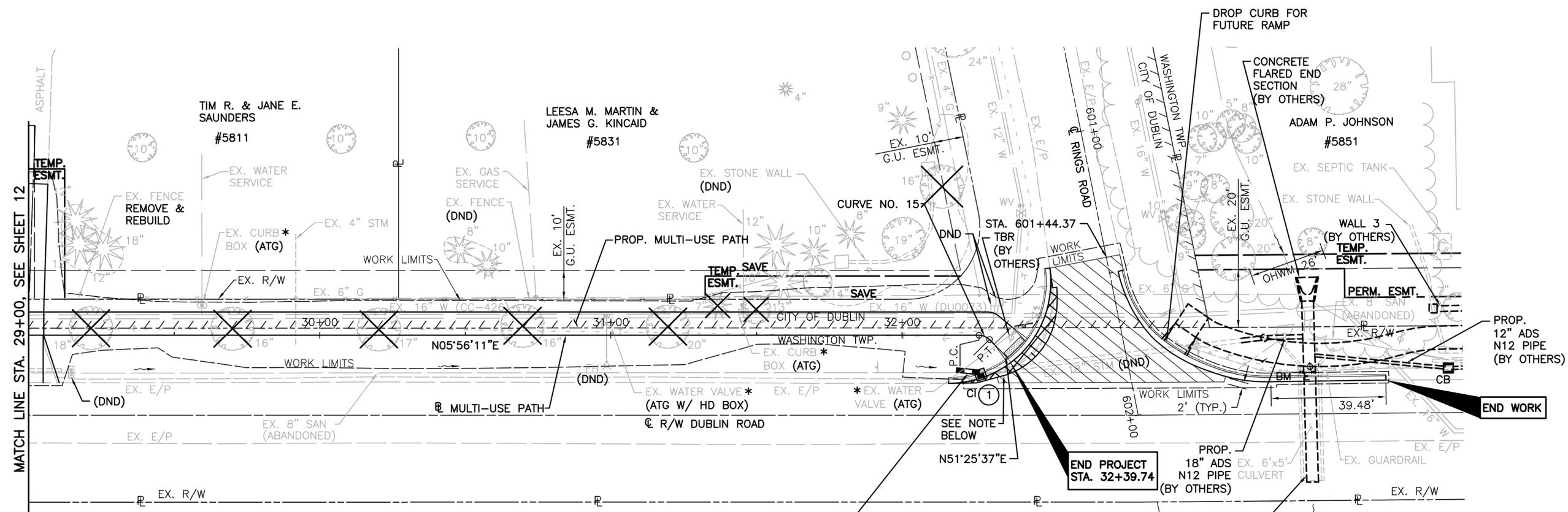


CALCULATED
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CHECKED
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PLAN AND PROFILE
STA. 29+00 AHEAD

DUBLIN ROAD SOUTH
MULTI-USE PATH

CURVE DATA
CURVE NO. 15
 P.I. STA. 32+28.59
 $\Delta = 45^{\circ}29'27''$
 $D_c = 1145^{\circ}54'56''$
 $R = 5.00'$
 $T = 2.10'$
 $L = 3.97'$
 $E = 0.42'$
 P.C. STA. 32+26.50
 P.T. STA. 32+30.47



B-xxx = SOIL BORING
 TP-xxx = TEST PIT
 RD = REMOVE AND DELIVER TO THE OWNER
 RR = REMOVE AND RELOCATE
 RAR = REMOVE AND RESET
 TBR = TO BE REMOVED
 DND = DO NOT DISTURB
 ATG = ADJUST TO GRADE
 * = PER RECORD PLAN, CONTRACTOR TO LOCATE
 OHWM = ORDINARY HIGH WATER MARK
 G.U. ESMT = GENERAL UTILITY EASEMENT
 DRN. ESMT = DRAINAGE EASEMENT
 I/E = INGRESS/EGRESS EASEMENT

FOR STORM SEWER PROFILE, SEE SHEET 19
 FOR INTERSECTION DETAILS, SEE SHEET 19
 FOR DRIVEWAY DETAILS, SEE SHEET 22
 FOR GRADING PLAN, SEE SHEET 24
 FOR TREE PRESERVATION PLAN, SEE SHEET 27
 FOR TRAFFIC CONTROL PLAN, SEE SHEET 29

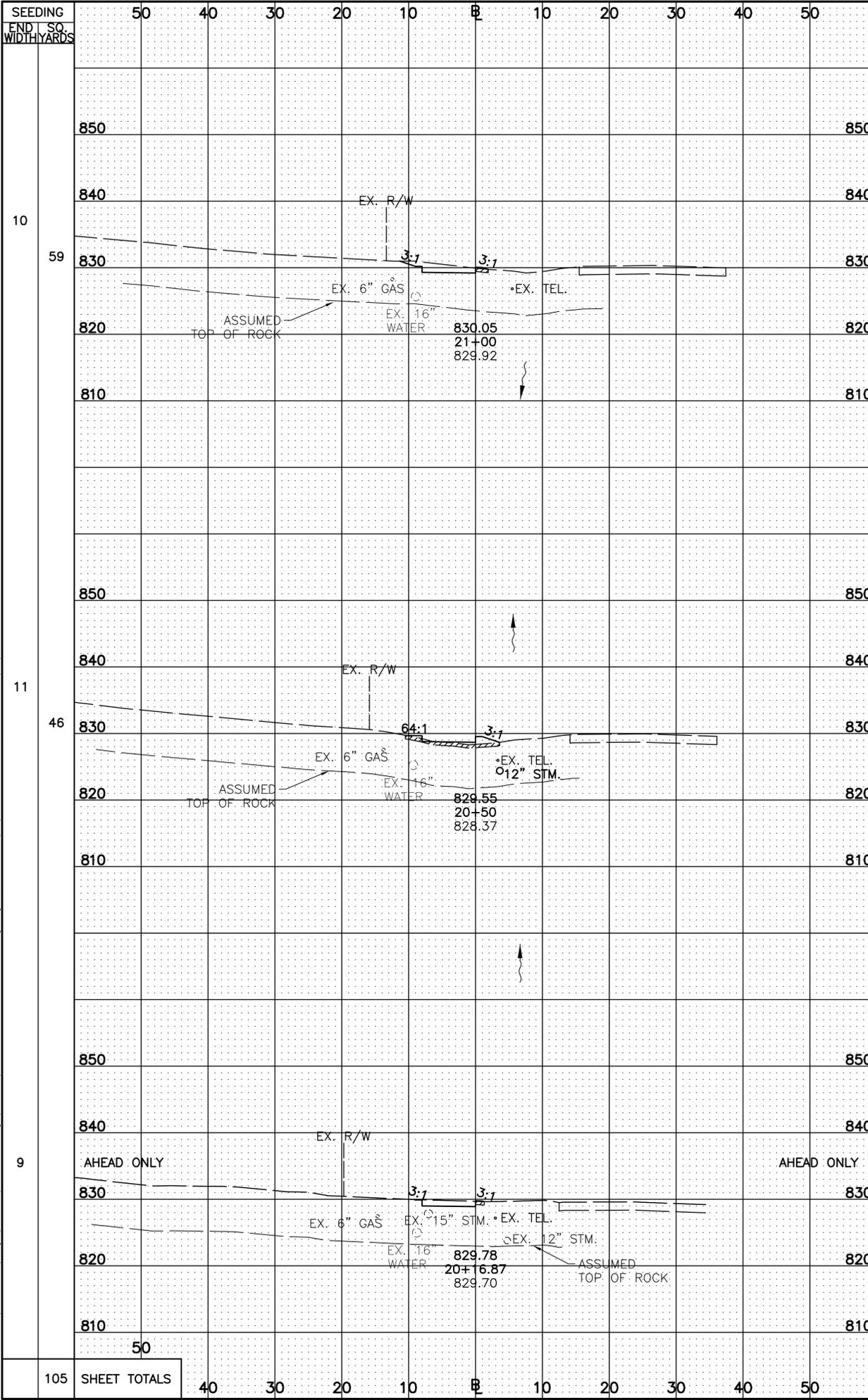
NOTE: CURB AND GUTTER TO BE REMOVED AND REPLACED IN SAME LOCATION ON THE NORTH AND SOUTH RADIUS RETURNS PER DETAIL IN TYPICAL SECTIONS. ON NORTH RADIUS RETURN OF RINGS RD., EXTEND CURB AND GUTTER AS NOTED.

- ITEM 452, 8" NON-REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT (TOP OF CONCRETE TO MATCH TOP OF MILLED PAVEMENT)
- PLANING AND OVERLAY
- RELOCATED STONE WALL

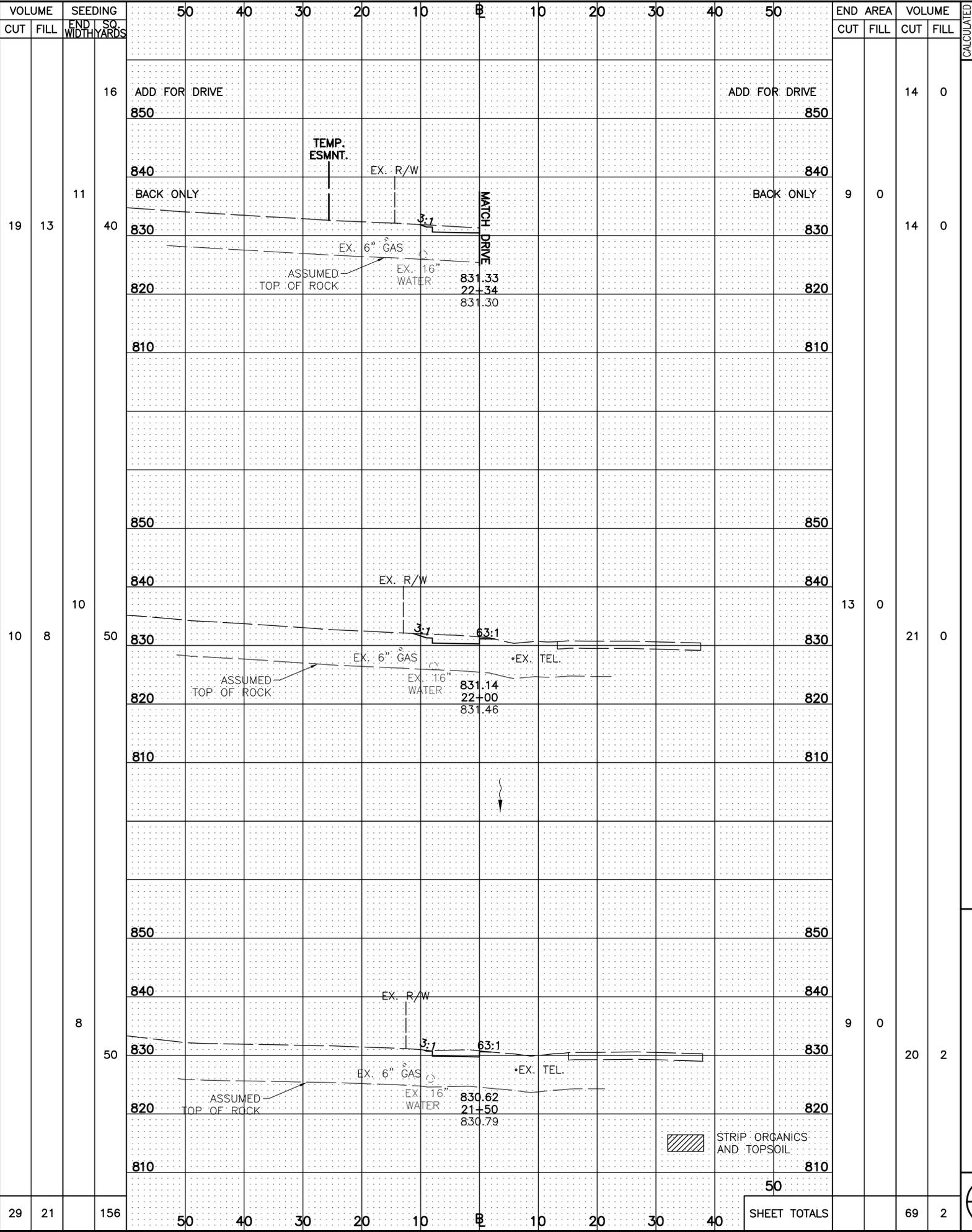
BM "E": FRANKLIN COUNTY ENGINEER BM R-3, BRASS PLUG ON THE CENTER OF THE WEST HEADWALL OF CULVERT "WAS-10-10.21", JUST NORTH OF RINGS ROAD, BEING 16 FEET LEFT OF THE CENTERLINE OF DUBLIN ROAD RIGHT-OF-WAY.
 N: 760484.58 E: 1796427.97 ELEV = 824.81

PROPOSED GRADE																			PROPOSED GRADE
845																			845
840																			840
835	<div style="position: absolute; top: 10px; left: 10px;"> $10'$ VC PVI STA. 29+10 ELEV. 829.46 LP STA. 29+05.24 LP ELEV. 829.46 </div>																		835
830	<div style="position: absolute; top: 10px; left: 500px;"> PROP. GRADE </div>																		830
825	<div style="position: absolute; top: 10px; left: 200px;">EX. 4" STM.</div> <div style="position: absolute; top: 10px; left: 500px;">EX. TEL.</div> <div style="position: absolute; top: 10px; left: 550px;">EX. TEL.</div>																		825
820	<div style="position: absolute; top: 10px; left: 350px;">EX. GRADE</div> <div style="position: absolute; top: 10px; left: 400px;">EX. 6" WATER</div> <div style="position: absolute; top: 10px; left: 700px;">EX. 18" STM.</div>																		820
815	<div style="position: absolute; top: 10px; left: 700px;">826.14</div>																		815
EXISTING GRADE																			EXISTING GRADE

McDaniel, Aron U:\73608736\design\plan_sct\Hertford to Rings\Section\Cross Sections - Hertford to Rings.dwg LAST SAVED: 2/12/2013 4:06 PM LAST PLOTTED: 3/11/2013 4:20 PM



END CUT	AREA	VOLUME		SEEDING END SO. WIDTH	SEEDING SO. YARDS
		CUT	FILL		
12	2	19	13	11	16
8	12	10	8	10	40
8	1	8	1	8	50
105 SHEET TOTALS		29	21	156	



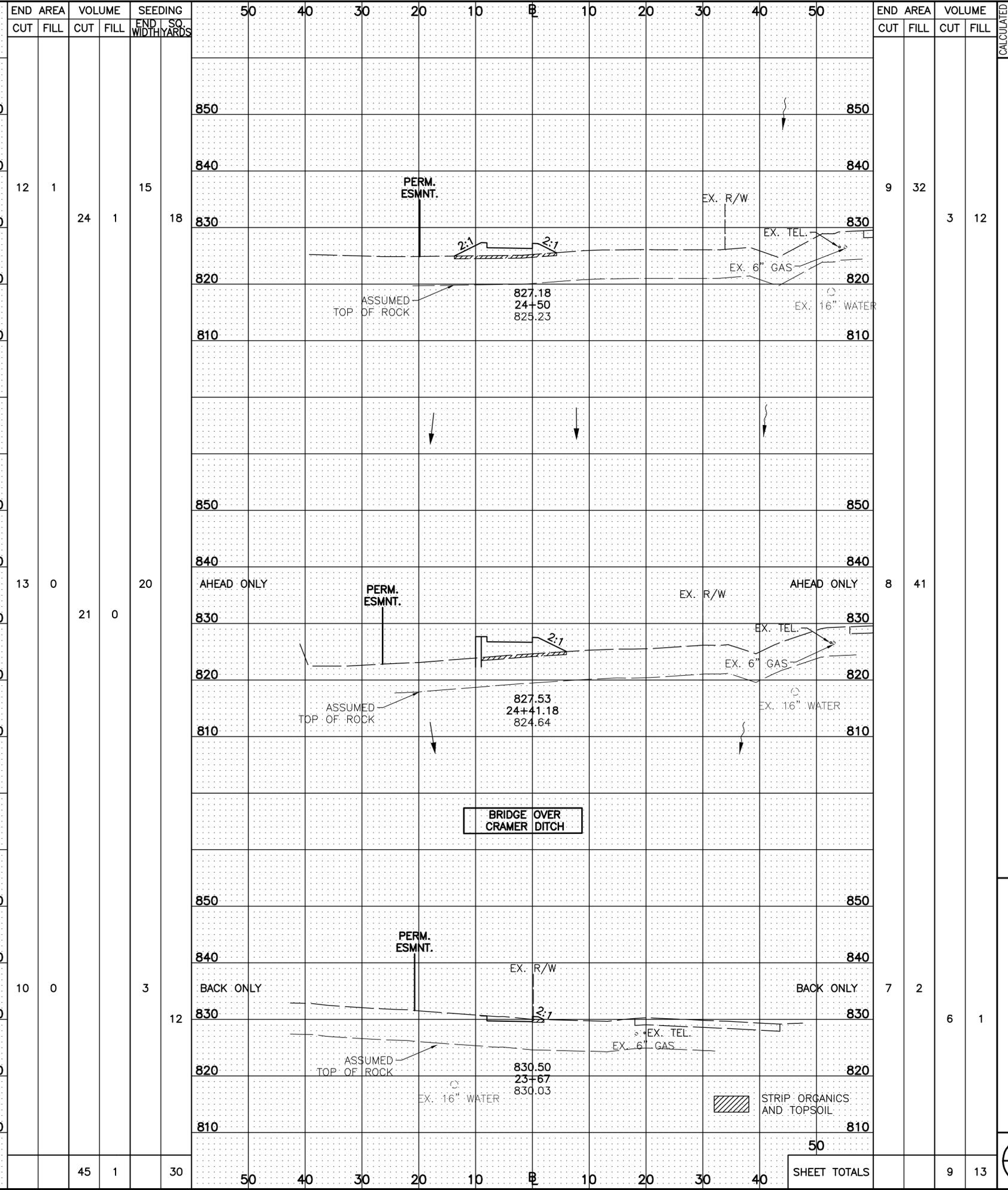
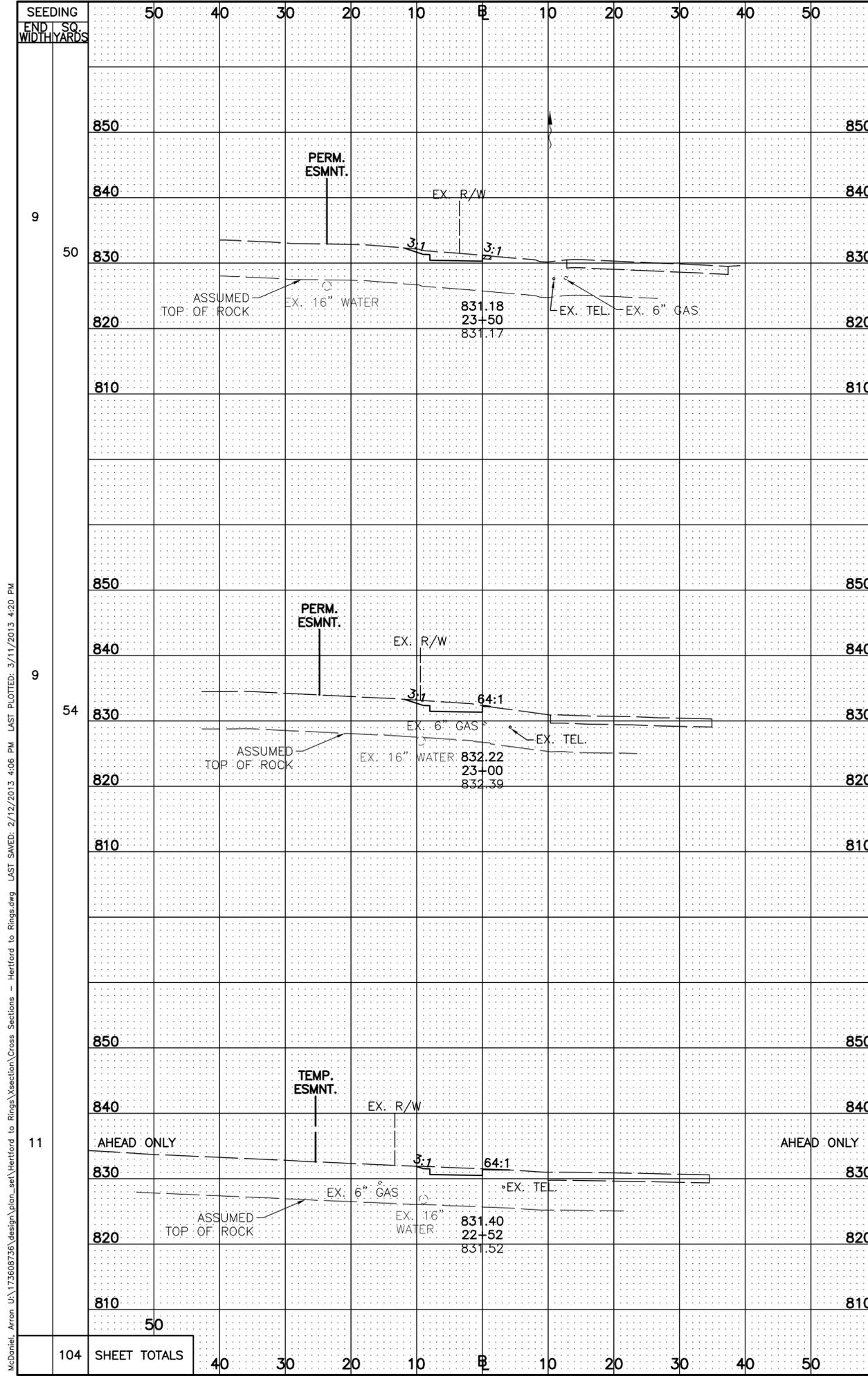
END CUT	AREA	VOLUME		SEEDING END SO. WIDTH	SEEDING SO. YARDS
		CUT	FILL		
9	0	14	0	9	16
13	0	21	0	10	40
9	0	20	2	8	50
105 SHEET TOTALS		69	2	156	

CROSS SECTIONS - HERTFORD LANE TO RINGS ROAD
STA. 20+09.37 TO STA. 22+34

DUBLIN ROAD SOUTH
MULTI-USE PATH

CALCULATED
CNK
CHECKED
TJS

14
37



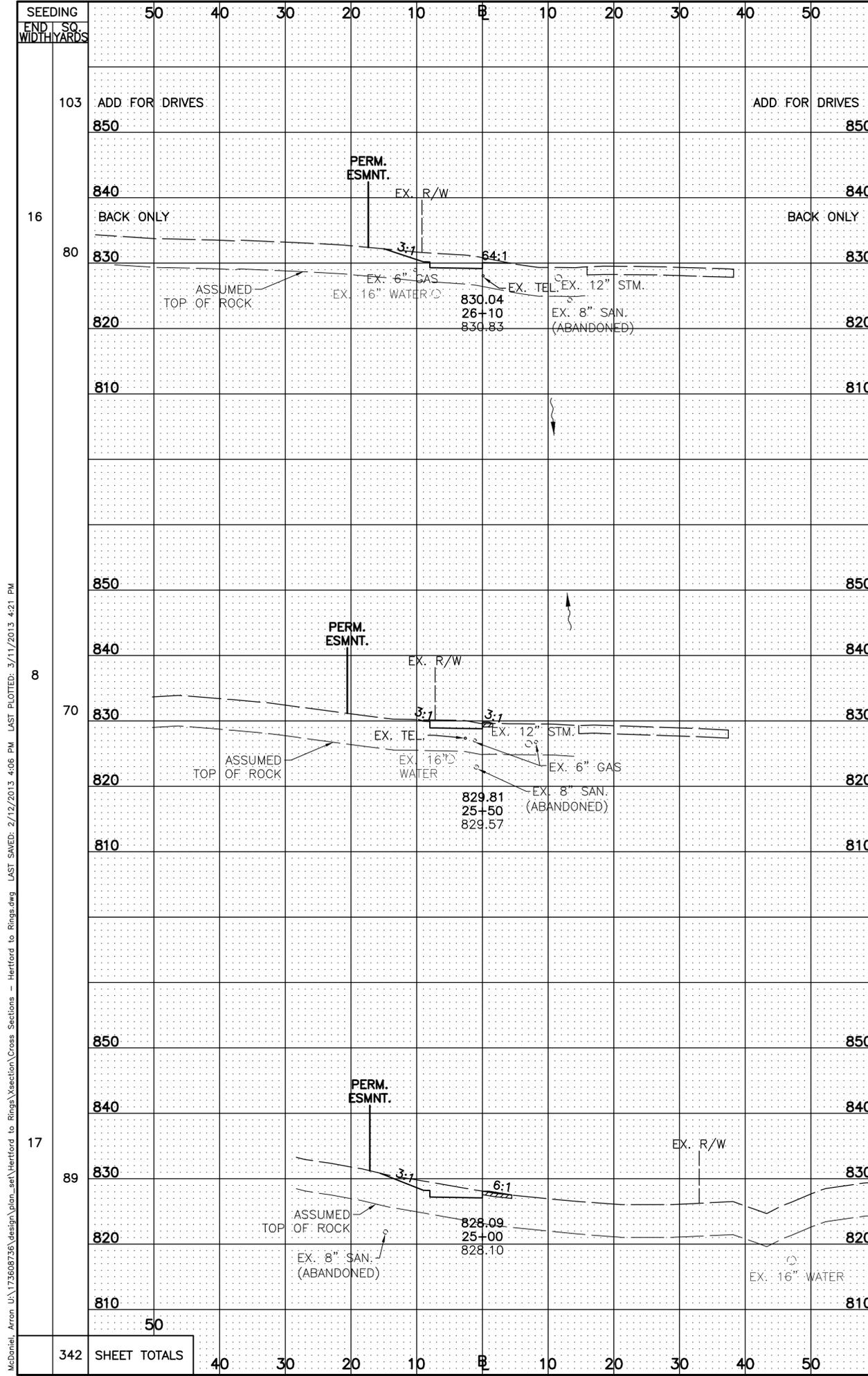
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CROSS SECTIONS - HERTFORD LANE TO RINGS ROAD
 STA. 22+52 TO STA. 24+50

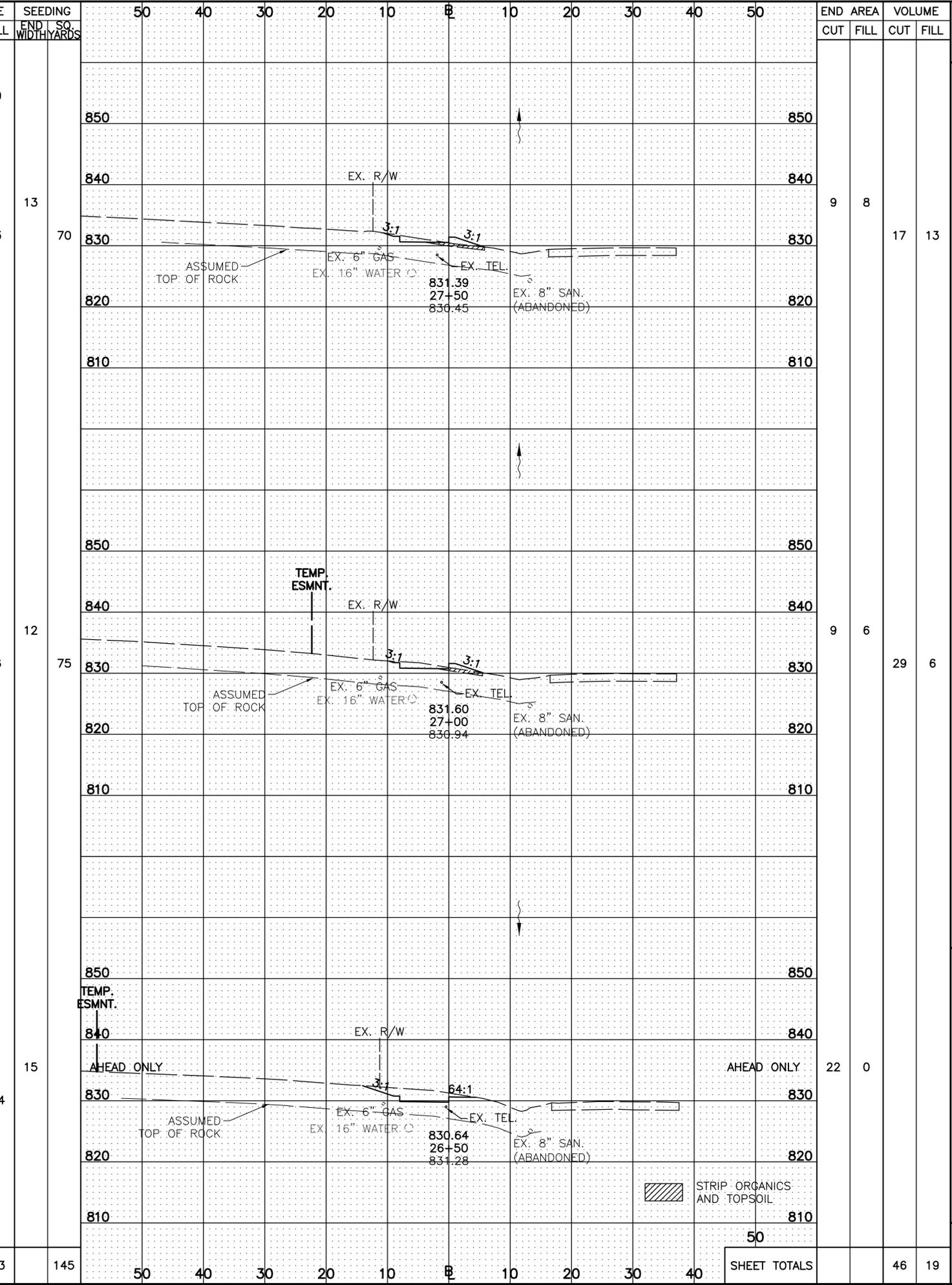
DUBLIN ROAD SOUTH
 MULTI-USE PATH

CALCULATED
 CNK
 CHECKED
 TJS

15
 37



SEEDING END SO. WIDTH YARDS	END AREA		VOLUME		SEEDING END SO. WIDTH YARDS
	CUT	FILL	CUT	FILL	
103			87	0	
16			24	0	13
80			39	3	70
8			11	2	12
70			31	6	75
17			22	4	15
89			29	34	
342			186	43	145



SEEDING END SO. WIDTH YARDS	END AREA		VOLUME		SEEDING END SO. WIDTH YARDS
	CUT	FILL	CUT	FILL	
850					
840					
830					
820					
810					
850					
840					
830					
820					
810					
850					
840					
830					
820					
810					
22			22	0	
342			46	19	16

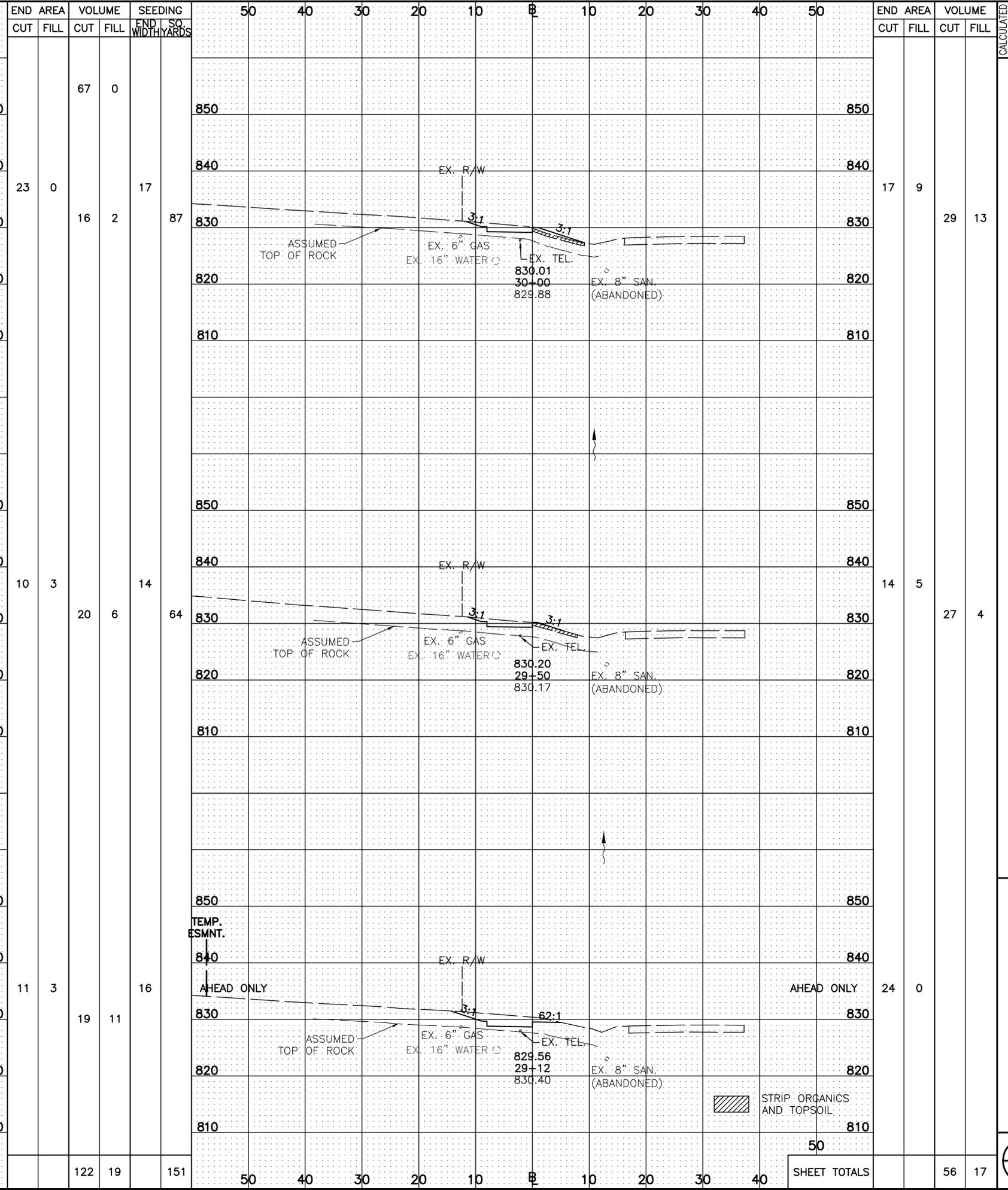
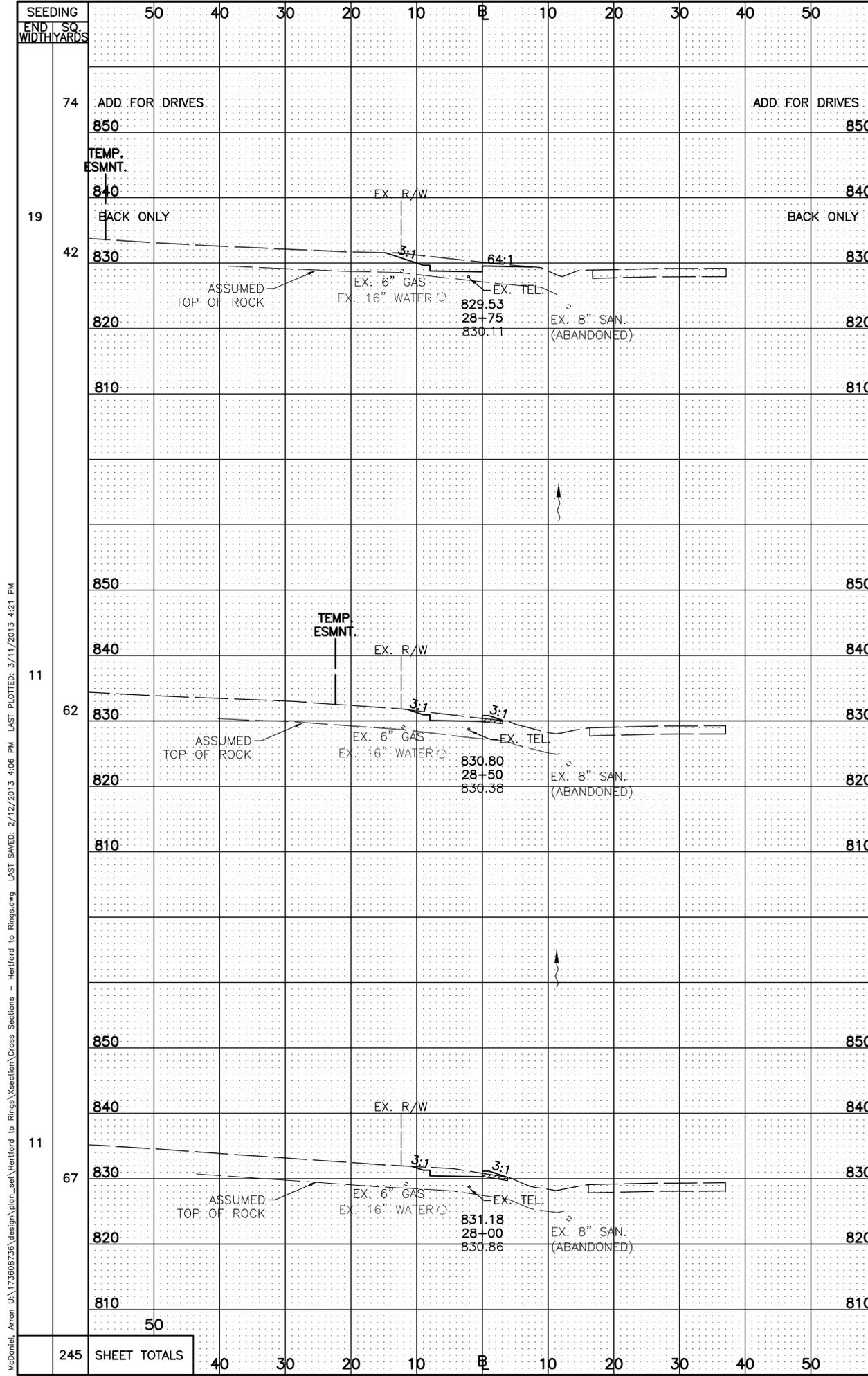
McDonnell, Aron U:\73608736\design\plan_sct\Hertford to Rings.dwg - Hertford to Rings.dwg LAST SAVED: 2/12/2013 4:06 PM LAST PLOTTED: 3/11/2013 4:21 PM

CROSS SECTIONS - HERTFORD LANE TO RINGS ROAD
STA. 25+00 TO STA. 27+50

DUBLIN ROAD SOUTH
MULTI-USE PATH

CALCULATED
CNK
CHECKED
TJS

STRIP ORGANICS AND TOPSOIL

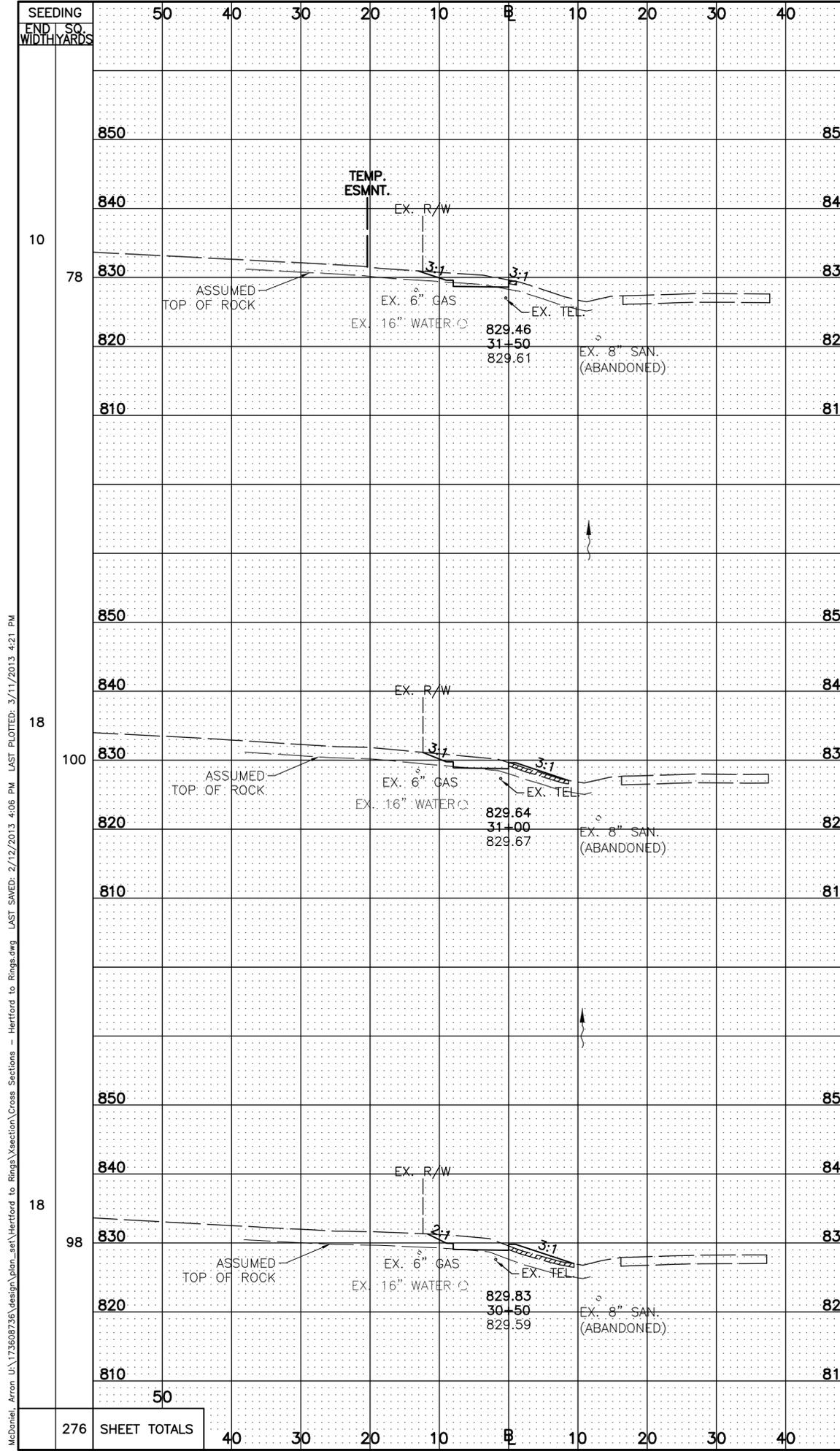


McDaniel, Aron U:\73608736\design\plan_set\Hertford to Rings\Section\Cross Sections - Hertford to Rings.dwg LAST SAVED: 2/12/2013 4:06 PM LAST PLOTTED: 3/11/2013 4:21 PM

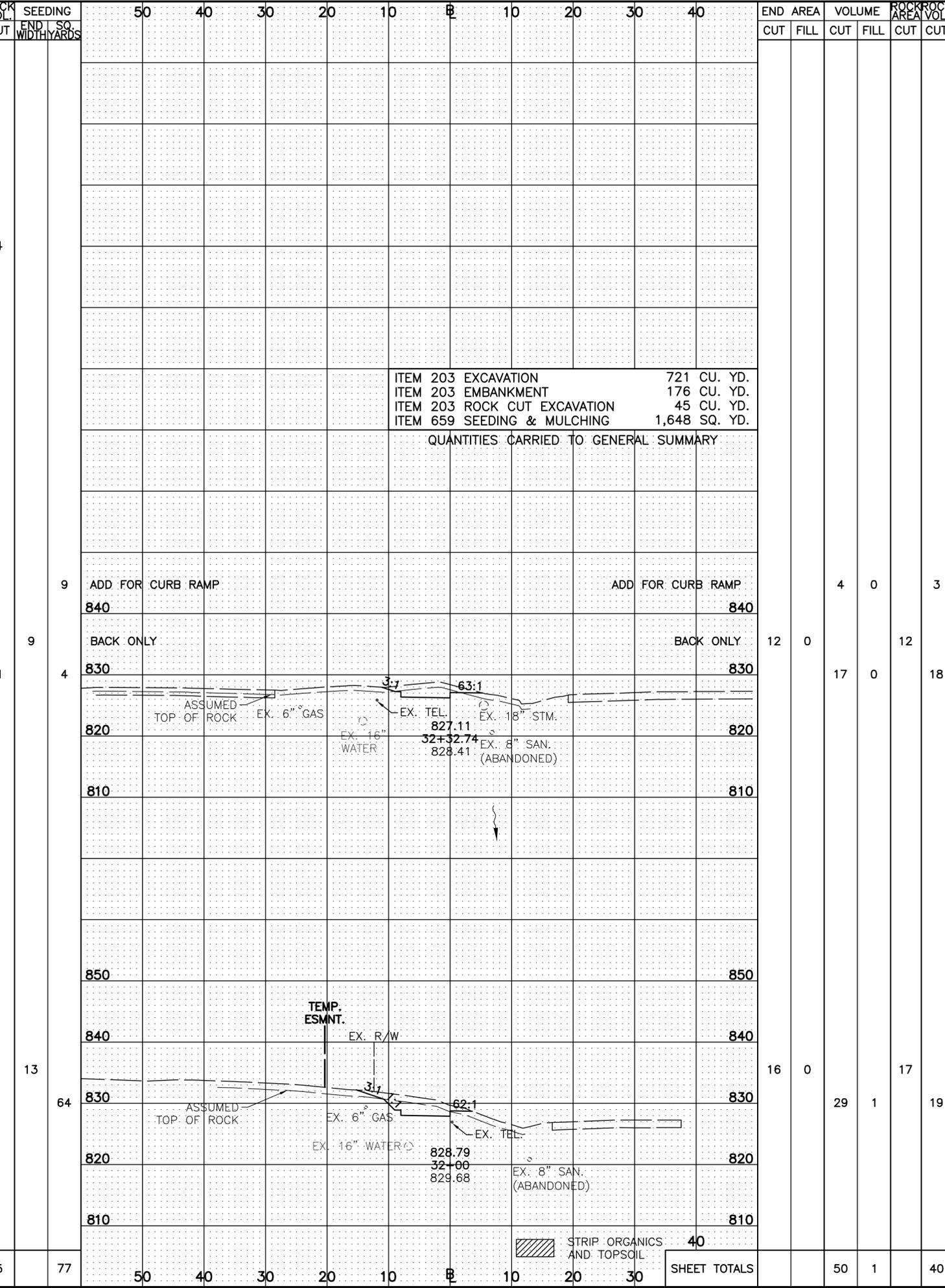
DUBLIN ROAD SOUTH MULTI-USE PATH
CROSS SECTIONS - HERTFORD LANE TO RINGS ROAD
STA. 28+00 TO STA. 30+00

CALCULATED
CNK
CHECKED
TJS

17
37



END STA.	AREA	VOLUME		ROCK AREA	ROCK VOL.	SEEDING END STA.	AREA
		CUT	FILL				
15	1	33	8	3	4		
20	7	39	15	1	9		
22	9	37	17	0	13		
276	SHEET TOTALS	109	40	5	77		



END STA.	AREA	VOLUME		ROCK AREA	ROCK VOL.	SEEDING END STA.	AREA
		CUT	FILL				
9							
12	0						
17	0						
16	0						
18	SHEET TOTALS	50	1	40	40		

ITEM 203 EXCAVATION 721 CU. YD.
 ITEM 203 EMBANKMENT 176 CU. YD.
 ITEM 203 ROCK CUT EXCAVATION 45 CU. YD.
 ITEM 659 SEEDING & MULCHING 1,648 SQ. YD.
 QUANTITIES CARRIED TO GENERAL SUMMARY

ADD FOR CURB RAMP 4 0 3

BACK ONLY 12 0 12

STRIP ORGANICS AND TOPSOIL 40

CROSS SECTIONS - HERTFORD LANE TO RINGS ROAD
 STA. 30+50 TO STA. 32+34.67

DUBLIN ROAD SOUTH
 MULTI-USE PATH

18
37

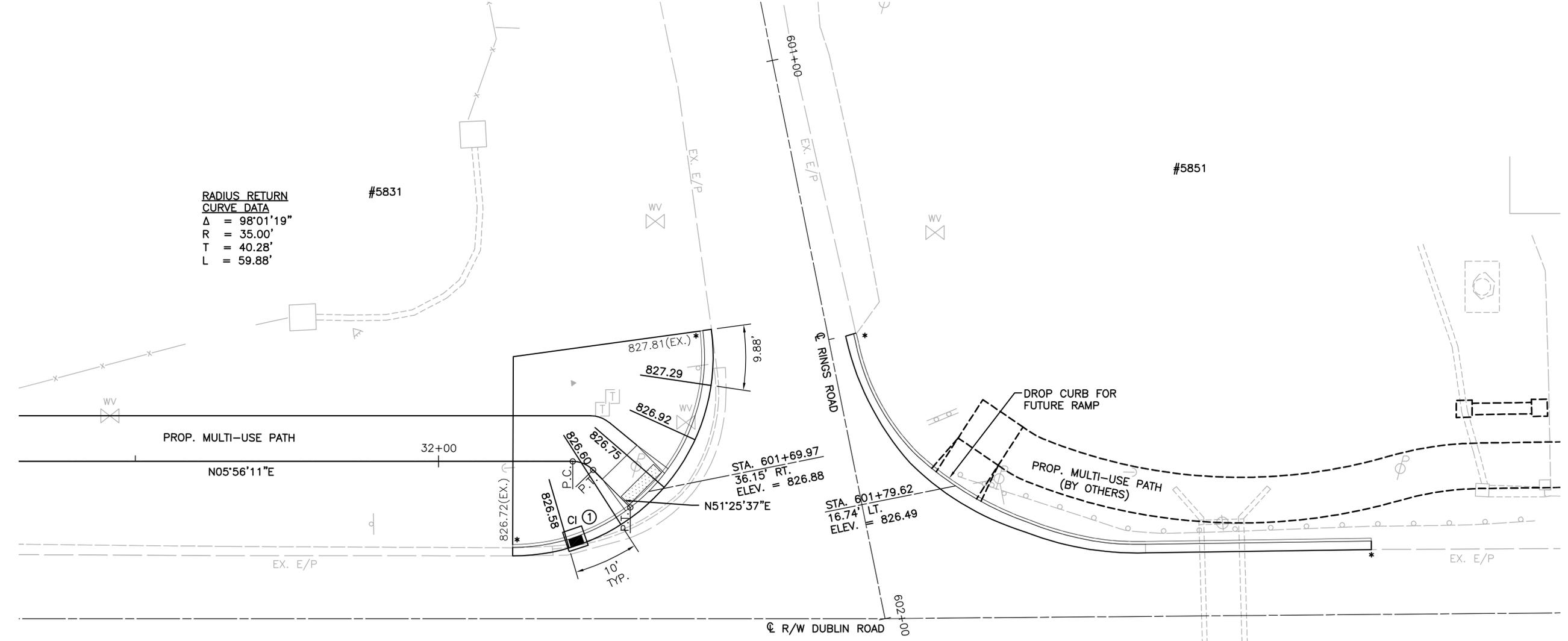
McDonnell, Aron U:\73608736\design\plan_sct\Hertford to Rings.dwg - Hertford to Rings.dwg LAST SAVED: 2/12/2013 4:06 PM LAST PLOTTED: 3/11/2013 4:21 PM



CALCULATED
CNK
CHECKED
TJS

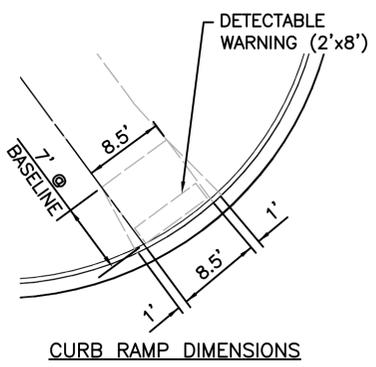
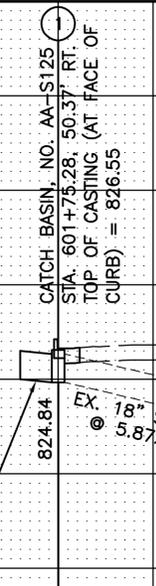
INTERSECTION DETAILS

DUBLIN ROAD SOUTH
MULTI-USE PATH



**RADIUS RETURN
CURVE DATA**
 $\Delta = 98^{\circ}01'19''$
 $R = 35.00'$
 $T = 40.28'$
 $L = 59.88'$

845					845
840					840
835					835
830					830
825					825
820					820
815					815
810					810
805					805

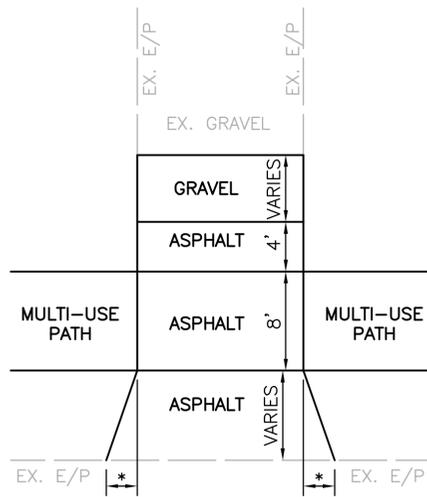


- DETECTABLE WARNINGS

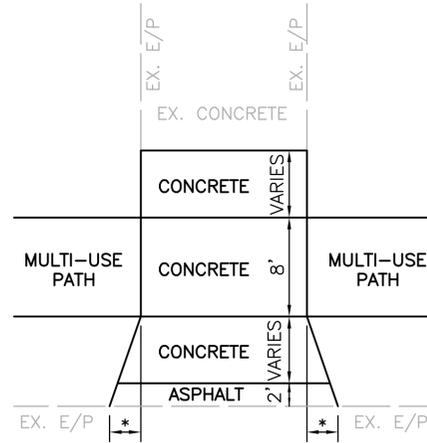
* TRANSITION CURB HEIGHT FROM
0" TO 6" IN 10 FEET

NOTE: THE EDGE OF PAVEMENT SHALL BE
FLUSH WITH THE GUTTER (NO LIP) AT
THE CURB RAMPS.

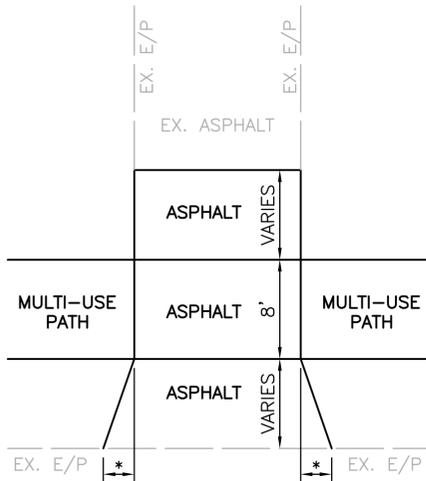
MCDANIEL, ARRON U:\173608736\design\plan_set\Hertford to Rings\Details\173408736cr01.dwg INTERSECTION DETAILS Last Saved: Feb 27, 2013 3:07 PM Plotted: Mar 11, 2013 3:57 PM



DUBLIN ROAD
GRAVEL DRIVE DETAIL



DUBLIN ROAD
CONCRETE DRIVE DETAIL



DUBLIN ROAD
ASPHALT DRIVE DETAIL

* - TAPER WIDTHS VARY

CONCRETE DRIVES

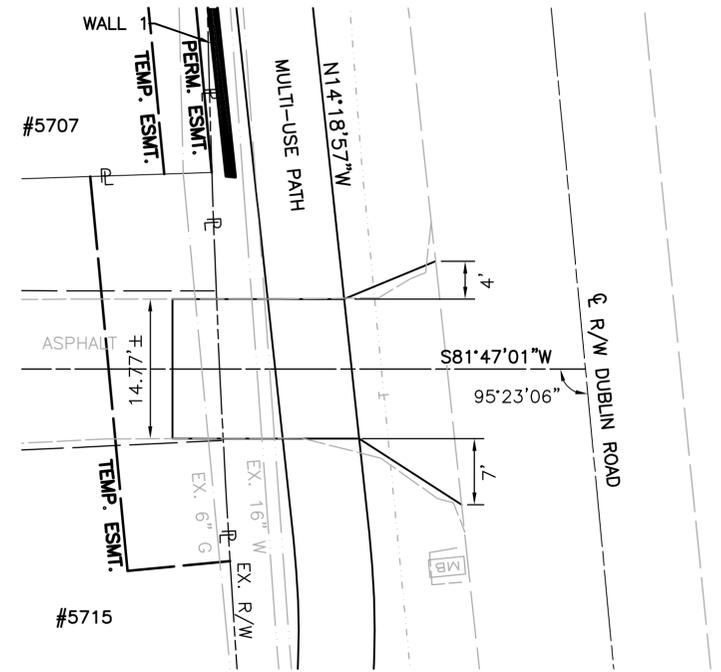
- ITEM 452 - 6" NON-REINFORCED CONCRETE
- ITEM 304 - 6" AGGREGATE BASE
- ITEM 204 - SUBGRADE COMPACTION

GRAVEL DRIVES

- ITEM 304 - 8" AGGREGATE BASE
- ITEM 204 - SUBGRADE COMPACTION

ASPHALT DRIVES

- ITEM 448 - 1-1/2" ASPHALT CONCRETE SURFACE COURSE, TYPE 1, PG64-22, MEDIUM TRAFFIC
- ITEM 301 - 3" BITUMINOUS AGGREGATE BASE COURSE, PG64-22
- ITEM 304 - 6" AGGREGATE BASE
- ITEM 204 - SUBGRADE COMPACTION



STA. 22+42.75, LT.
PLAN

█ - RELOCATED STONE WALL

850									850
840									840
830									830
820									820
810									810

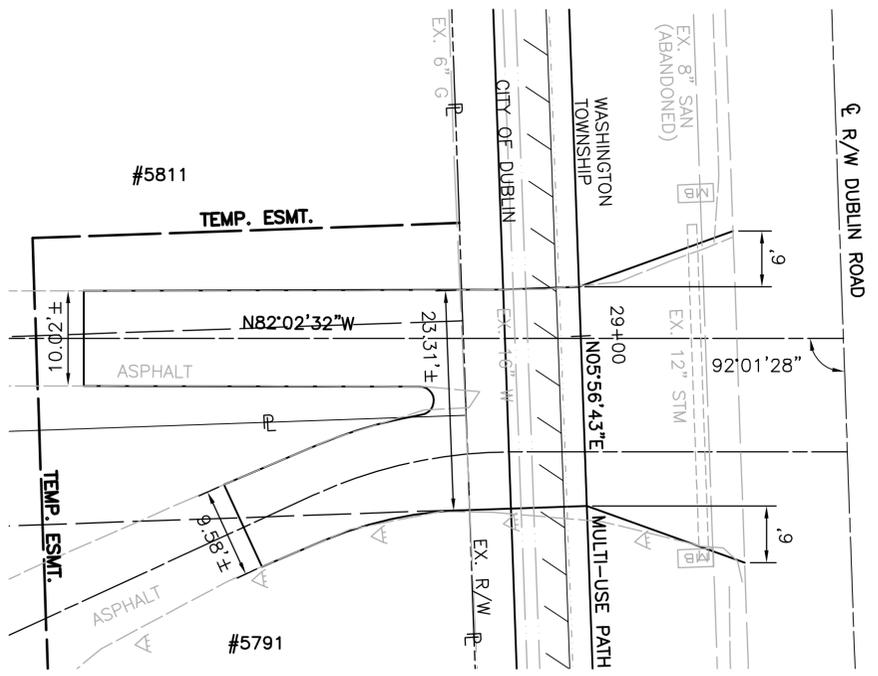
STA. 22+42.75, LT.
PROFILE



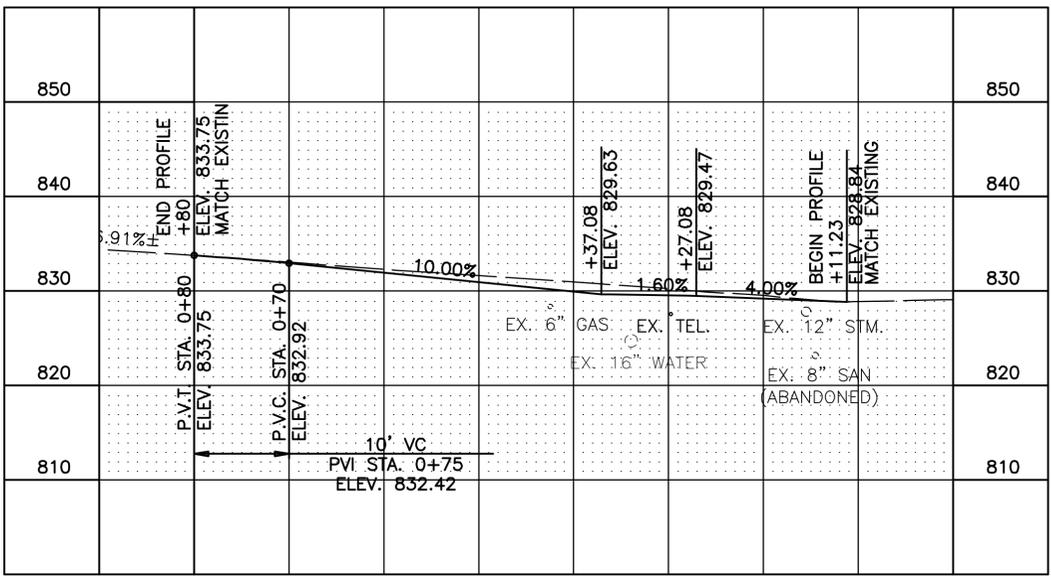
CALCULATED
CNK
CHECKED
TJS

DRIVEWAY DETAILS

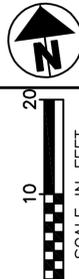
DUBLIN ROAD SOUTH
MULTI-USE PATH



STA. 28+99.75, LT.
PLAN



STA. 28+99.75, LT.
PROFILE

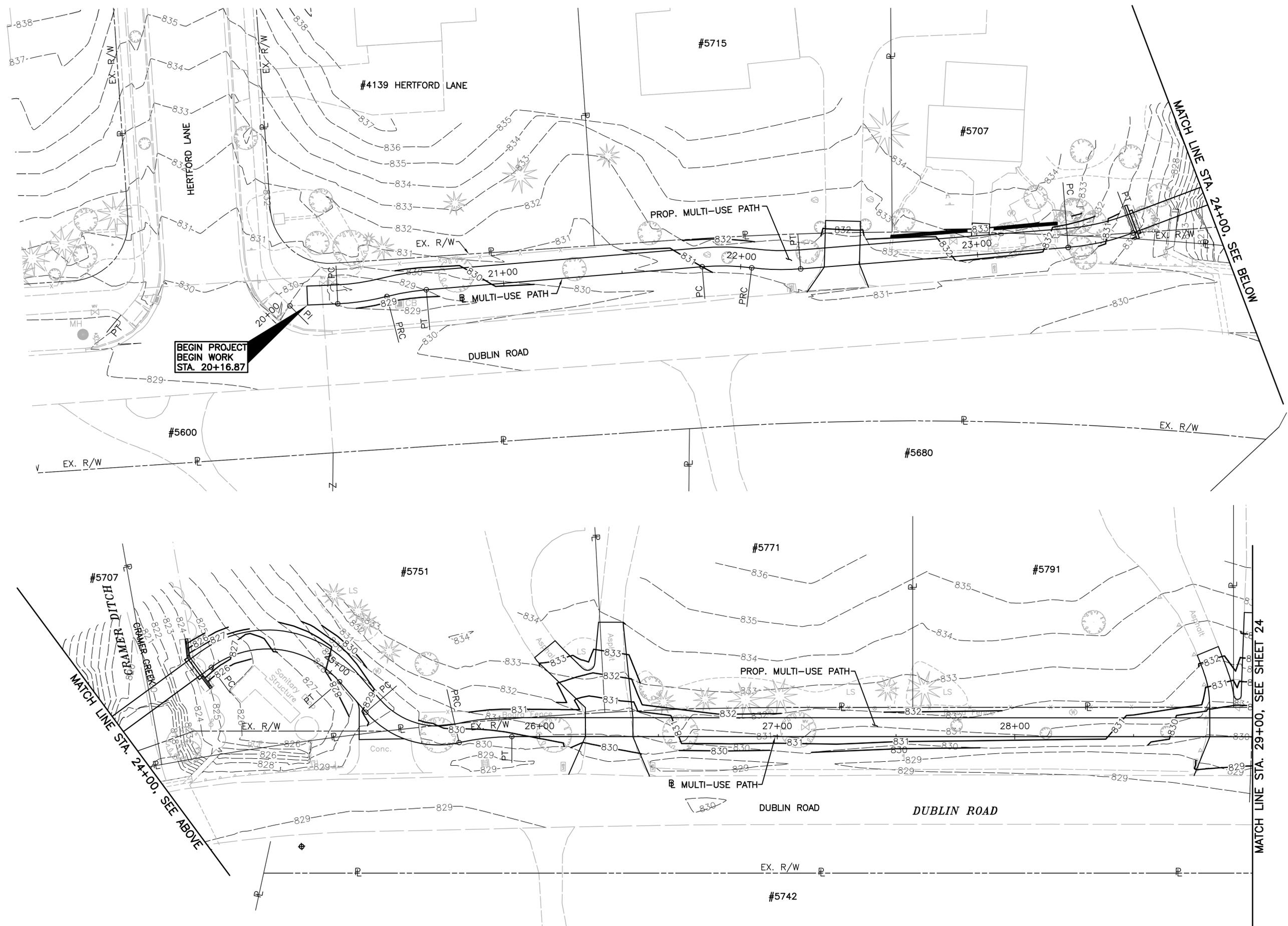


CALCULATED
CNK
CHECKED
TJS

DRIVEWAY DETAILS

DUBLIN ROAD SOUTH
MULTI-USE PATH

MCDANIEL, ARRON U:\173608736\design\plan_set\Hertford to Rings\Grading Plan\173608736gp02.dwg GRADING PLAN Last Saved: Jan 09, 2013 7:49 AM Plotted: Mar 11, 2013 3:57 PM



BEGIN PROJECT
BEGIN WORK
STA. 20+16.87

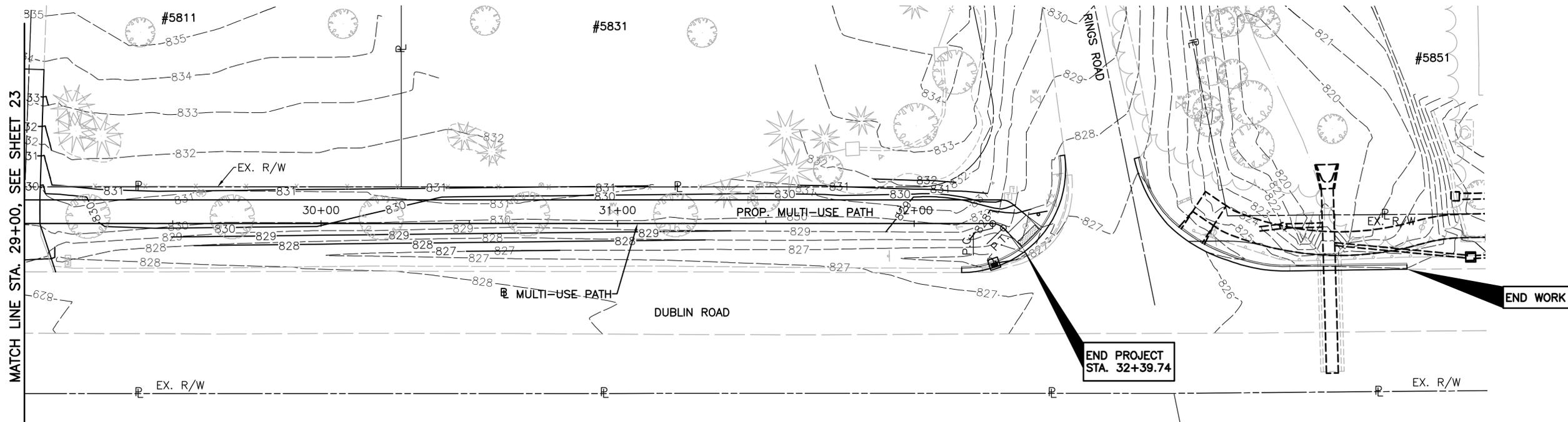


CALCULATED	CNK	TJS
	CHECKED	

GRADING PLAN
STA. 29+00 BACK

DUBLIN ROAD SOUTH
MULTI-USE PATH

MCDANIEL, ARRON U:\173608736\design\plan_set\Hertford to Rings\Grading Plan\173608736gp03.dwg GRADING PLAN Last Saved: Feb 27, 2013 3:06 PM Plotted: Mar 11, 2013 3:57 PM



CALCULATED	CNK	CHECKED	TJS
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GRADING PLAN
STA. 29+00 AHEAD

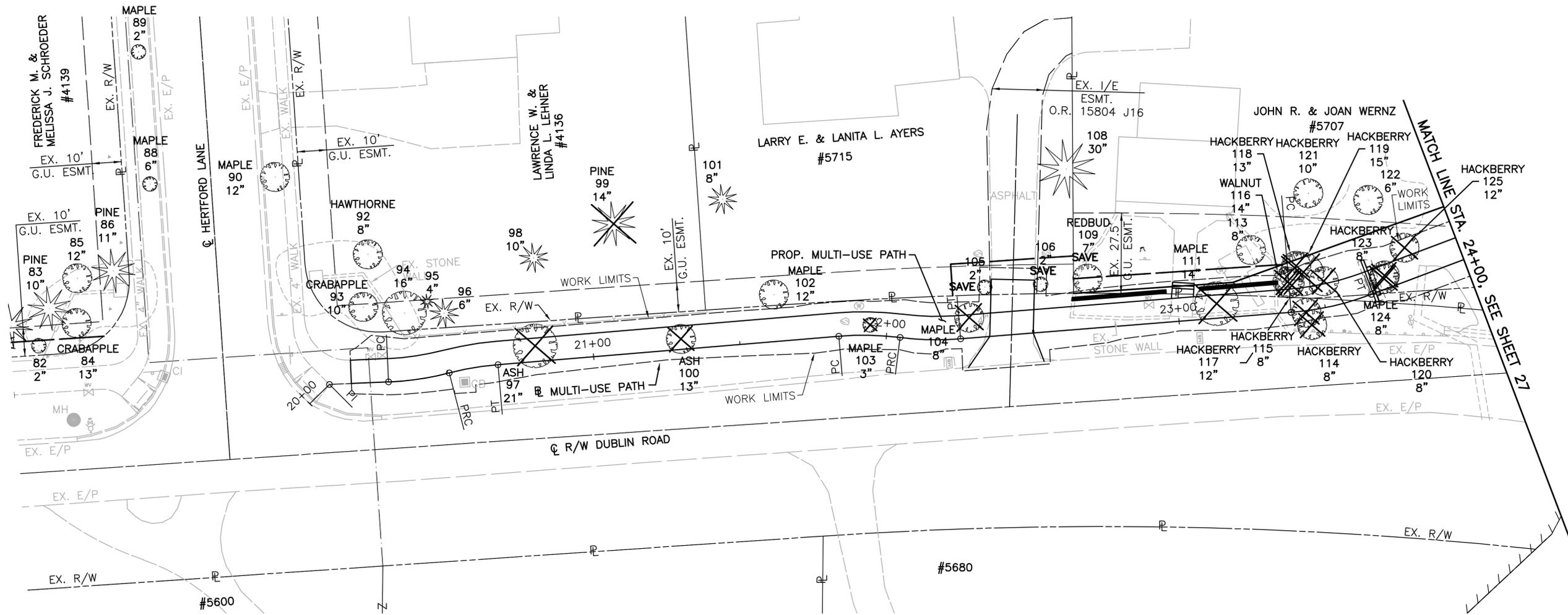
DUBLIN ROAD SOUTH
MULTI-USE PATH

TREE PRESERVATION NOTES:

1. TREES WITH TRUNK DIAMETERS LESS THAN TWELVE INCHES (12") SHALL BE CONSIDERED BRUSH AND THEIR REMOVAL PAID UNDER ITEM 201, CLEARING AND GRUBBING WHERE SPECIFICALLY MARKED FOR REMOVAL.
2. A PROTECTED TREE SHALL BE DEFINED AS ANY TREE NOT DESIGNATED FOR REMOVAL ON THESE PLANS AND HAVING A TRUNK SIX-INCH (6") IN DIAMETER OR LARGER, AS MEASURED FOUR AND ONE HALF FOOT (4 ½') ABOVE GROUND (DIAMETER BREST HEIGHT, DBH).
3. ALL TREES NOT LOCATED WITHIN THE AREAS SPECIFICALLY DESIGNATED FOR REMOVAL SHALL BE PRESERVED, WHETHER SHOWN OR NOT SHOWN ON THE APPROVED CONSTRUCTION DRAWINGS. WHERE SHOWN ON THE DRAWINGS, HIGH-VISIBILITY TREE PROTECTION FENCING SHALL BE PLACED A MINIMUM OF FIFTEEN FEET (15') FROM THE TREE TRUNK, OR AS DIRECTED BY THE DIVISION OF ENGINEERING.
4. THE CONTRACTOR SHALL LIMIT WORK TO AREAS WITHIN PUBLIC RIGHT-OF-WAY, EASEMENTS, OR OTHER SPECIFICALLY DESIGNATED WORK AREAS.
5. TRENCHING WITHIN CRITICAL ROOT ZONES SHALL BE EVALUATED PRIOR TO EXCAVATION. IF EXCAVATION WITHIN CRITICAL ROOT ZONES IS UNAVOIDABLE, TRENCHES SHALL BE BACKFILLED SAME DAY.
6. NO ATTACHMENTS SHALL BE MADE TO ANY PROTECTED TREE (I.E. NO ROPES, NAILS, WIRES, ETC.).
7. THE CONTRACTOR MAY INSTALL PROTECTION FENCING IN PHASES TO MATCH CONSTRUCTION SEQUENCING TO ALLOW REUSE OF FENCING. HOWEVER, PROTECTION FENCING SHALL REMAIN IN PLACE DURING ALL PHASES OF CONSTRUCTION FOR ANY PARTICULAR WORK LOCATION.
8. PROTECTION FENCING SHALL BE LOCATED AROUND THE CRITICAL ROOT ZONE OF AFFECTED TREES. THIS IS THE AREA AROUND A TREE HAVING A RADIUS EQUAL TO ONE FOOT (1') FOR EVERY INCH OF DBH, OR FIFTEEN FEET (15'), WHICHEVER IS GREATER.
9. IN ALL CASES WHERE PROTECTION OF THE ENTIRE CRITICAL ROOT ZONE DOES NOT ALLOW ADEQUATE ROOM FOR CONSTRUCTION, PROTECTION FENCING IS TO BE OFFSET A MAXIMUM OF SEVEN AND ONE-HALF FEET (7 ½') FROM CENTER OF WATER LINE TRENCH, TEN FEET (10') FROM CENTER OF SEWER TRENCH, AND SEVEN AND ONE-HALF FEET (7 ½') FROM LATERAL OR FIRE HYDRANT LEAD PIPES, UNLESS OTHERWISE NOTED IN THE PLANS.
10. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE CONSTRUCTION, ERECTION, AND MAINTENANCE OF ALL PROTECTION FENCING. TREE PROTECTION FENCING IS SHOWN ON THE PLANS FOR BIDDING PURPOSES, AND MAY BE ADJUSTED FOR FIELD CONDITIONS IN ACCORDANCE WITH THE ABOVE REQUIREMENTS.
11. CONTACT THE DIVISION OF ENGINEERING FOR INSPECTION OF PROTECTION FENCING PRIOR TO START OF CONSTRUCTION.
12. THE CITY OF DUBLIN RESERVES THE RIGHT TO ORDER THE REMOVAL OF ADDITIONAL TREES AND/OR STUMPS WITHIN THE CONSTRUCTION LIMITS.
13. QUESTIONS CONCERNING TREE PRESERVATION AND SPECIAL PROTECTIVE MEASURES (I.E. HAND-PRUNING OF SHOOTS, ETC.) SHOULD BE DIRECTED TO THE DIVISION OF ENGINEERING.

TREE REMOVAL NOTES:

1. TOTAL TREE CALIPER TO BE REMOVED: 390".



TREES TO BE REMOVED

I.D.	TREE SPECIES	TRUNK DIA. (IN.)	CONDITION
97	ASH	21	GOOD
99	PINE	14	POOR
100	ASH	13	GOOD
103	MAPLE	3	GOOD
104	MAPLE	8	GOOD
107	NOT USED		
111	MAPLE	14	GOOD
112	NOT USED		
114	HACKBERRY	8	GOOD
115	HACKBERRY	8	GOOD
116	WALNUT	14	GOOD
117	HACKBERRY	12	GOOD
118	HACKBERRY	13	GOOD
119	HACKBERRY	15	GOOD
120	HACKBERRY	8	GOOD
123	HACKBERRY	8	GOOD
124	MAPLE	8	GOOD
125	HACKBERRY	12	GOOD

0 20 40
SCALE IN FEET

CALCULATED
JWS

CHECKED
TJS

TREE PRESERVATION PLAN
STA. 24+00 BACK

DUBLIN ROAD SOUTH
MULTI-USE PATH

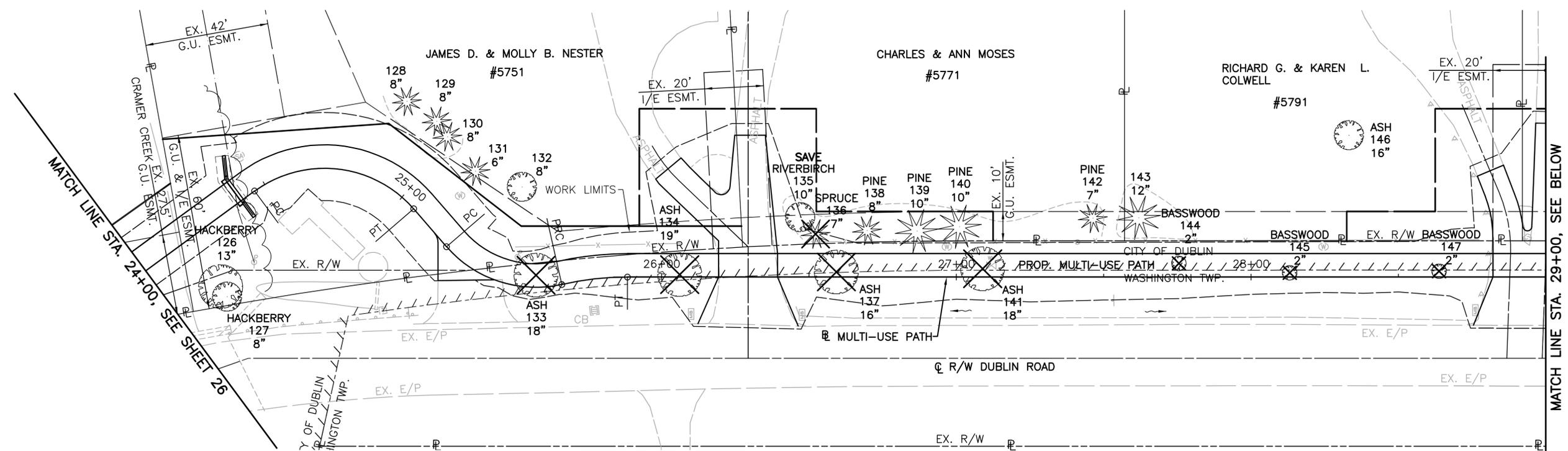
26
37



CALCULATED
JWS
CHECKED
TJS

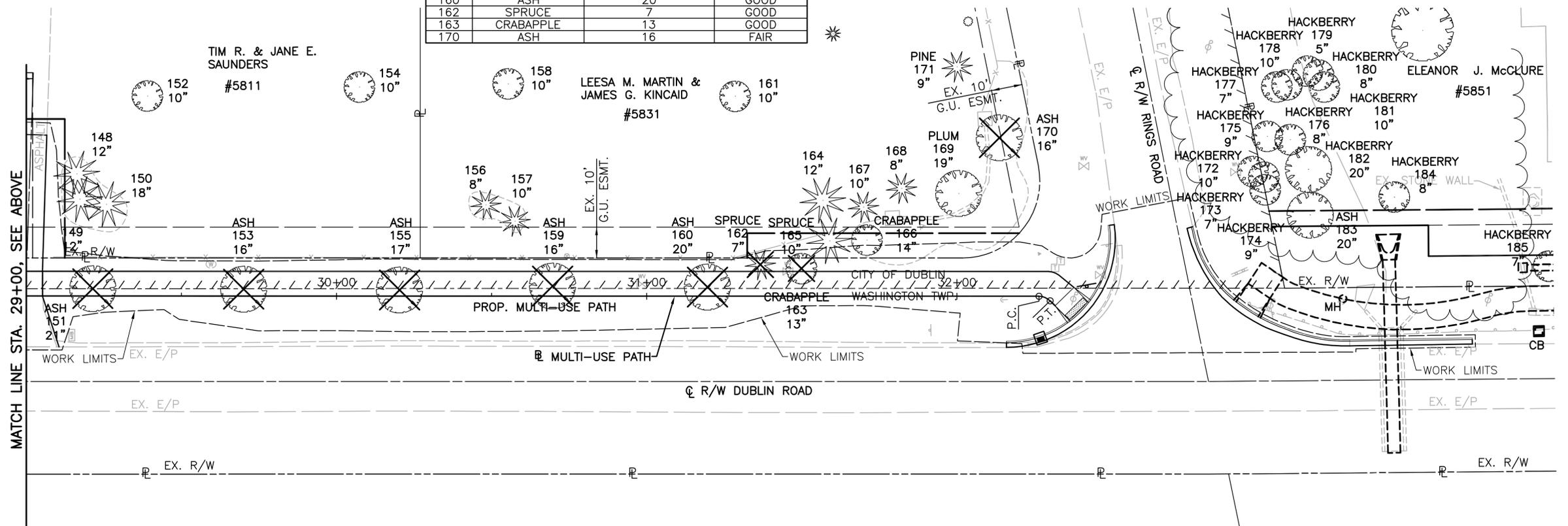
TREE PRESERVATION PLAN
STA. 24+00 AHEAD

DUBLIN ROAD SOUTH
MULTI-USE PATH



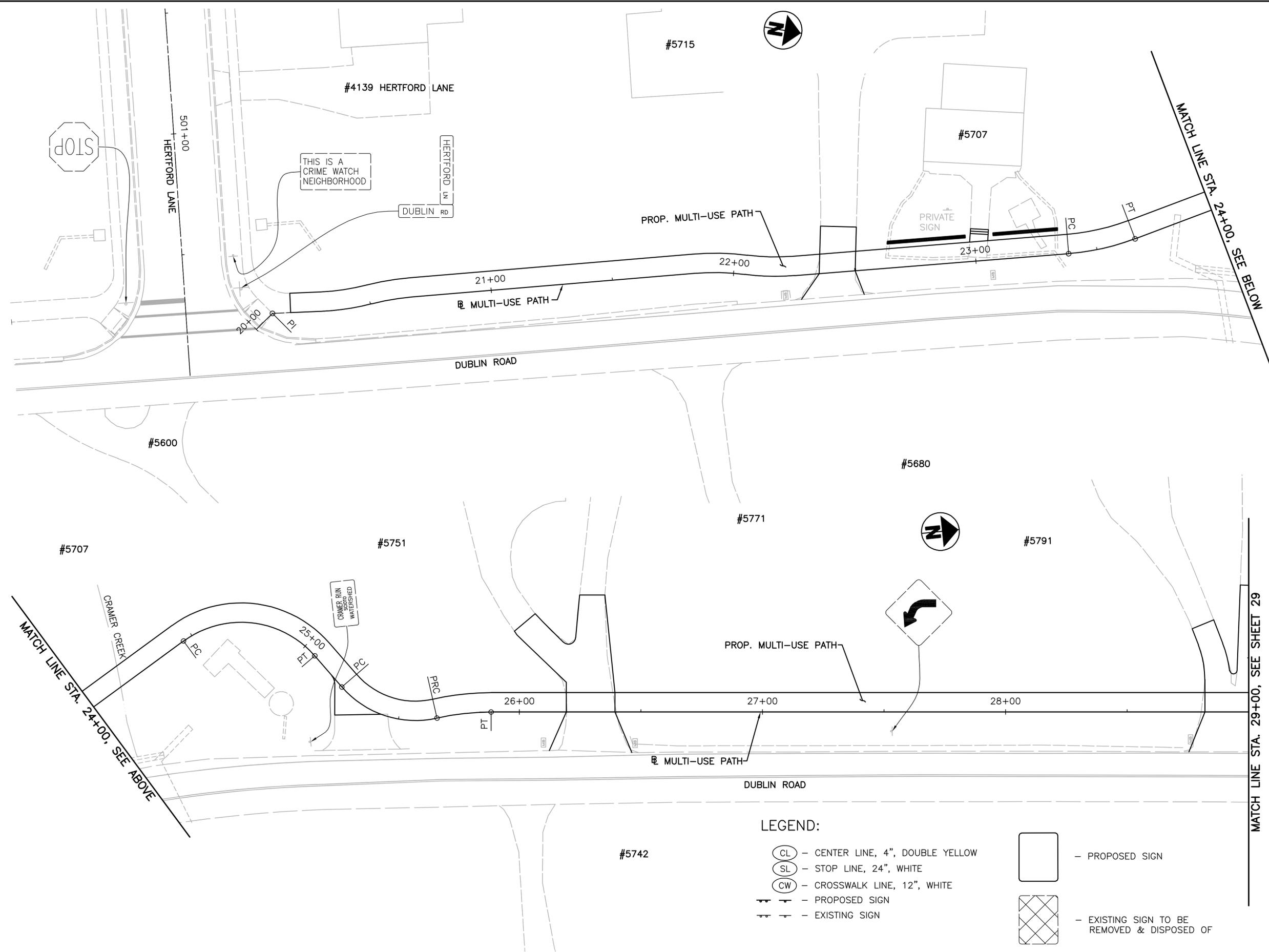
TREES TO BE REMOVED

I.D.	TREE SPECIES	TRUNK DIA. (IN.)	CONDITION
133	ASH	18	FAIR
134	ASH	19	FAIR
136	SPRUCE	7	GOOD
137	ASH	17	FAIR
141	ASH	18	FAIR
144	BASSWOOD	2	GOOD
145	BASSWOOD	2	GOOD
147	BASSWOOD	2	GOOD
151	ASH	21	GOOD
153	ASH	16	GOOD
155	ASH	17	GOOD
159	ASH	16	GOOD
160	ASH	20	GOOD
162	SPRUCE	7	GOOD
163	CRABAPPLE	13	GOOD
170	ASH	16	FAIR



MCDANIEL, ARRON U:\173608736\design\pbn_sct\herford to Rings\Tree Preservation Plan\173608736tp03.dwg TREE PRESERVATION PLAN Last Saved: Feb 13, 2013 10:14 AM Plotted: Mar 11, 2013 3:58 PM

MCDANIEL, ARRON U:\173608736\design\plan_set\Hertford to Rings\Traffic Control\173608736tc02.dwg - TRAFFIC CONTROL PLAN Last Saved: Jan 09, 2013 7:59 AM Plotted: Mar 11, 2013 3:58 PM

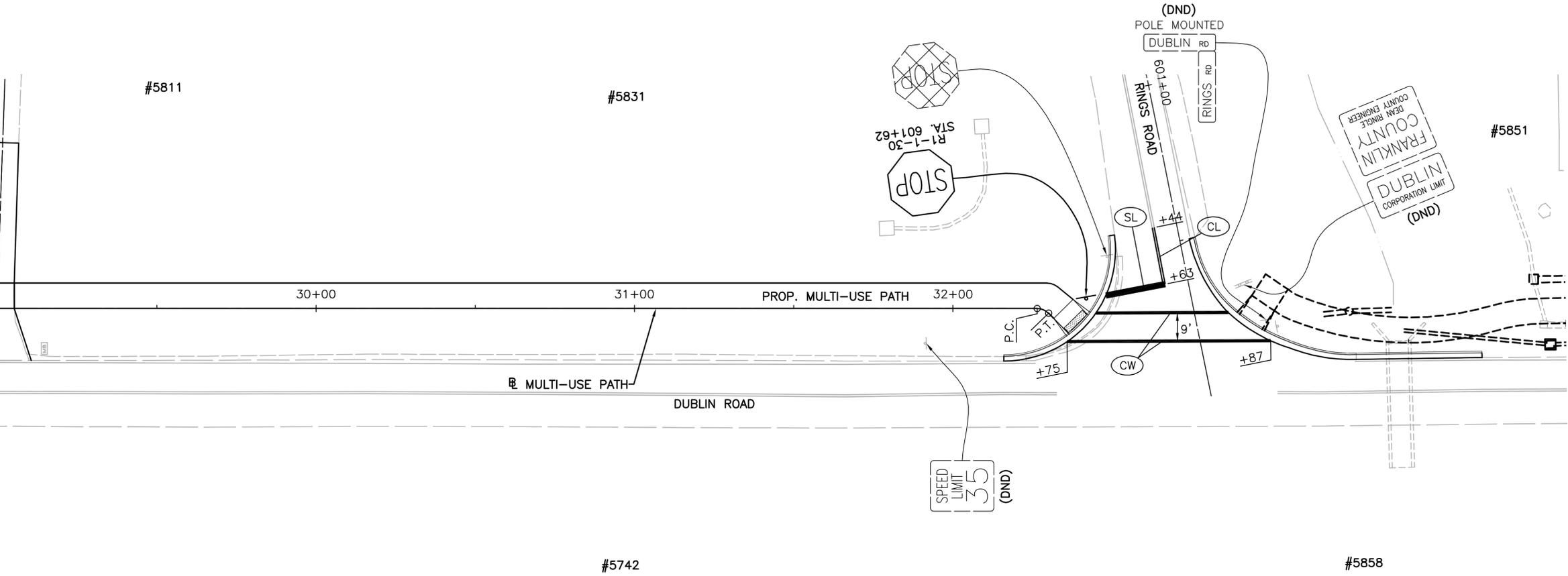


LEGEND:

- CENTER LINE, 4", DOUBLE YELLOW
- STOP LINE, 24", WHITE
- CROSSWALK LINE, 12", WHITE
- PROPOSED SIGN
- EXISTING SIGN TO BE REMOVED & DISPOSED OF
- EXISTING SIGN TO REMAIN
- PROPOSED SIGN
- EXISTING SIGN TO BE REMOVED & DISPOSED OF
- EXISTING SIGN TO REMAIN

CALCULATED	CNK	CHECKED	TJS
<p>SCALE IN FEET</p>			
<p>TRAFFIC CONTROL PLAN STA. 29+00 BACK</p>			
<p>DUBLIN ROAD SOUTH MULTI-USE PATH</p>			
<p>28 37</p>			

MATCH LINE STA. 29+00, SEE SHEET 28



LEGEND:

- (CL) - CENTER LINE, 4", DOUBLE YELLOW
- (SL) - STOP LINE, 24", WHITE
- (CW) - CROSSWALK LINE, 12", WHITE
- - PROPOSED SIGN
- - EXISTING SIGN
- [] - PROPOSED SIGN
- [X] - EXISTING SIGN TO BE REMOVED & DISPOSED OF
- [] - EXISTING SIGN TO REMAIN

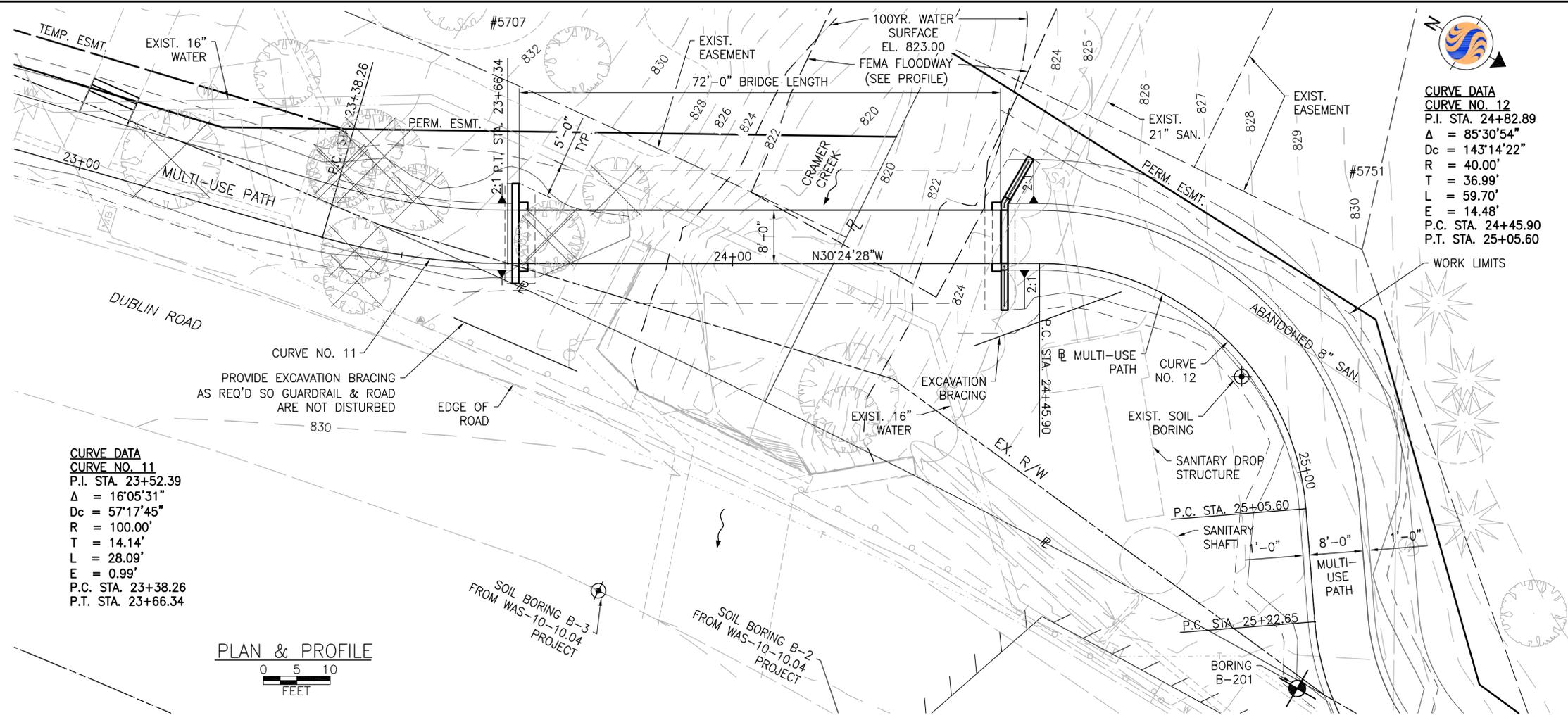


CALCULATED
CNK
CHECKED
TJS

TRAFFIC CONTROL PLAN
STA. 29+00 AHEAD

DUBLIN ROAD SOUTH
MULTI-USE PATH

U:\173608736\design\plan_set\Herford to Rings\Bridges\173608736_sp02.dwg SAVEd: Wednesday, February 13, 2013 8:31 AM PLOTTED: Monday, March 11, 2013 3:58 PM



CURVE DATA
CURVE NO. 12
 P.I. STA. 24+82.89
 $\Delta = 85^{\circ}30'54''$
 $D_c = 143'14'22''$
 $R = 40.00'$
 $T = 36.99'$
 $L = 59.70'$
 $E = 14.48'$
 P.C. STA. 24+45.90
 P.T. STA. 25+05.60

CURVE DATA
CURVE NO. 11
 P.I. STA. 23+52.39
 $\Delta = 16^{\circ}05'31''$
 $D_c = 57'17'45''$
 $R = 100.00'$
 $T = 14.14'$
 $L = 28.09'$
 $E = 0.99'$
 P.C. STA. 23+38.26
 P.T. STA. 23+66.34

PLAN & PROFILE
 0 5 10
 FEET

LEGEND:
 B.O.F. - BOTTOM OF FOOTING V.C. - VERTICAL CURVE
 CL. - CLEARANCE
 DND - DO NOT DISTURB
 E.F. - EACH FACE
 EL. - ELEVATION
 EXP. - EXPANSION
 F/F - FACE TO FACE
 STA. - STATION
 T&B - TOP & BOTTOM

B.M. "D"
 CHISELED SQUARE ON THE NORTHEAST WINGWALL OF THE FRANKLIN COUNTY ENGINEER CULVERT "WAS 10-10.04" FOR CRAMER DITCH, BEING 19 FEET RIGHT OF THE CENTERLINE OF DUBLIN ROAD RIGHT-OF-WAY.
 N: 759645.15
 E: 1796376.49
 ELEV= 829.40 (88)

PEAK FLOW (cfs) (FROM CITY OF DUBLIN)

	2 YEAR	5 YEAR	100 YEAR
CRAMER CREEK:	253	338	864

- NOTES:**
- 1.) DENOTES SOIL BORING LOCATION.
 - 2.) DENOTES BENCH MARK LOCATION. SEE ROADWAY SCHEMATIC PLAN
 - 3.) DENOTES SOIL BORING LOCATION, FROM PREVIOUS CONSTRUCTION PROJECTS.
 - 4.) EARTHWORK LIMITS SHOWN ARE APPROXIMATE, ACTUAL SLOPES SHALL CONFORM TO ROADWAY PLAN CROSS SECTIONS.
 - 5.) ELEVATIONS BASED ON 1988 NORTH AMERICAN VERTICAL DATUM.
 - 6.) FOR REFERENCE POINTS, SEE SHT. 5
37

EXISTING DUBLIN ROAD STRUCTURE:
 TYPE: SINGLE SPAN CONCRETE SLAB
 SPAN: 14' FACE/FACE ABUTMENT (16' C/C BEARING)
 ROADWAY:
 SKEW: 16' RT FWD
 LOADING: H15
 DECK: REINFORCED CONCRETE
 WEARING SURFACE: ASPHALT
 APPROACH SLABS: NONE
 ALIGNMENT: 13'32'
 SUPERELEVATION: NONE
 DATE BUILT: UNKNOW (REHABILITATED 1999)
 SFN: 2531356

PROPOSED MULTI-USE PATH STRUCTURE:
 TYPE: PRE ENGINEERED TRUSS BRIDGE
 SPAN: 72'-0" BRIDGE LENGTH
 ROADWAY: 8'-0" F/F TRUSSES
 SKEW: NONE
 LOADING: 85 PSF LIVE LOAD AND 8,000 LB VEHICLE LOAD
 DECK: WOOD DECK
 WEARING SURFACE: WOOD DECK
 APPROACH SLABS: NONE
 ALIGNMENT: TANGENT
 CROSS SLOPE: LEVEL

PROPOSED GRADE	832.10	831.18	830.18	829.18	828.18	827.18	827.04	826.09	PROPOSED GRADE
860									860
850									850
840									840
830									830
820									820
810									810
800									800
EXISTING GRADE	831.7	831.2	829.3	817.7	820.6	825.2	827.0	828.1	EXISTING GRADE
	+25	+50	+75	24+00	+25	+50	+75	25+00	

PROFILE ALONG BASE LINE

ESTIMATED QUANTITIES			
ITEM NO.	QUANTITY	UNIT	DESCRIPTION
503	LUMP	LUMP	COFFERDAMS, CRIBS AND SHEETING
503	LUMP	LUMP	UNCLASSIFIED EXCAVATION
511	34	CY	CLASS C CONCRETE, ABUTMENT INCLUDING FOOTING, AS PER PLAN
518	3	CY	POROUS BACKFILL WITH FILTER FABRIC
564	10	SY	SEALING OF CONCRETE SURFACES (NON EPOXY)
SPECIAL	LUMP	LUMP	STRUCTURE MISC.: 72' PRE-ENGINEERED TRUSS PEDESTRIAN BRIDGE
SPECIAL	14	LF	RAILING (42" HIGH METAL RAILING)

DESIGN SPECIFICATIONS:

THIS STRUCTURE CONFORMS TO THE "LRFD GUIDE SPECIFICATIONS FOR DESIGN OF PEDESTRIAN BRIDGES, 2ND EDITION" ADOPTED BY THE AMERICAN ASSOCIATION OF STATE HIGHWAY AND TRANSPORTATION OFFICIALS AND THE ODOT BRIDGE DESIGN MANUAL EXCEPT AS NOTED.

DESIGN DATA:

DESIGN LOADING: 85 PSF AND 8,000 LB. VEHICLE

CONCRETE CLASS C - COMPRESSIVE STRENGTH 4,000 P.S.I.
(SUBSTRUCTURE)

REINFORCING STEEL - MINIMUM YIELD STRENGTH 60,000 P.S.I. (NON-EPOXY)

PREFABRICATED BRIDGE:

SUPERSTRUCTURE SHALL BE DESIGNED BY PRE-APPROVED PREFABRICATED BRIDGE MANUFACTURER. STANTEC IS RESPONSIBLE FOR THE DESIGN OF THE SUBSTRUCTURE. SEE PREFABRICATED BRIDGE SPECIFICATIONS.

PRE-APPROVED BRIDGE MANUFACTURERS

- CONTINENTAL BRIDGE (CONTECH ENGINEERED SOLUTIONS)
9025 CENTRE POINT DRIVE, SUITE 400
WESTCHESTER, OH 45069
- STEADFAST BRIDGES (CONTECH ENGINEERED SOLUTIONS)
9025 CENTRE POINT DRIVE, SUITE 400
WESTCHESTER, OH 45069

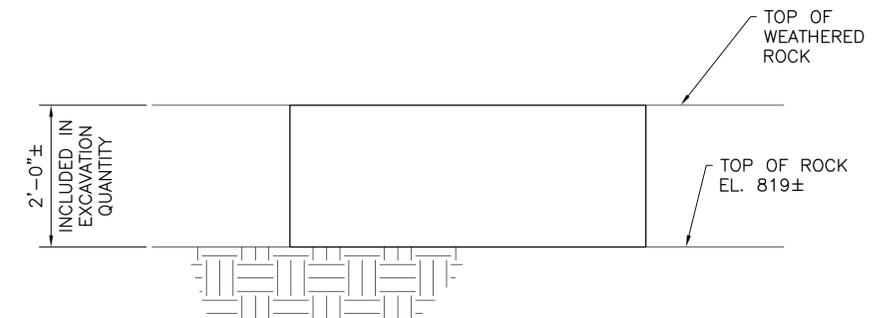
FOUNDATION BEARING RESISTANCE:

ABUTMENT FOOTINGS, AS DESIGNED, PRODUCE A MAXIMUM SERVICE LOAD PRESSURE OF 3.0 KIPS PER SQUARE FOOT. THE ALLOWABLE SERVICE LOAD BEARING CAPACITY IS 20 KIPS PER SQUARE FOOT.

SOILS ENCOUNTERED AT THE BEARING ELEVATION DURING CONSTRUCTION MUST MEET OR EXCEED THE REQUIRED BEARING CAPACITY. IF THEY DON'T, OVEREXCAVATION IS TO BE PERFORMED UNTIL THIS REQUIREMENT IS MET.

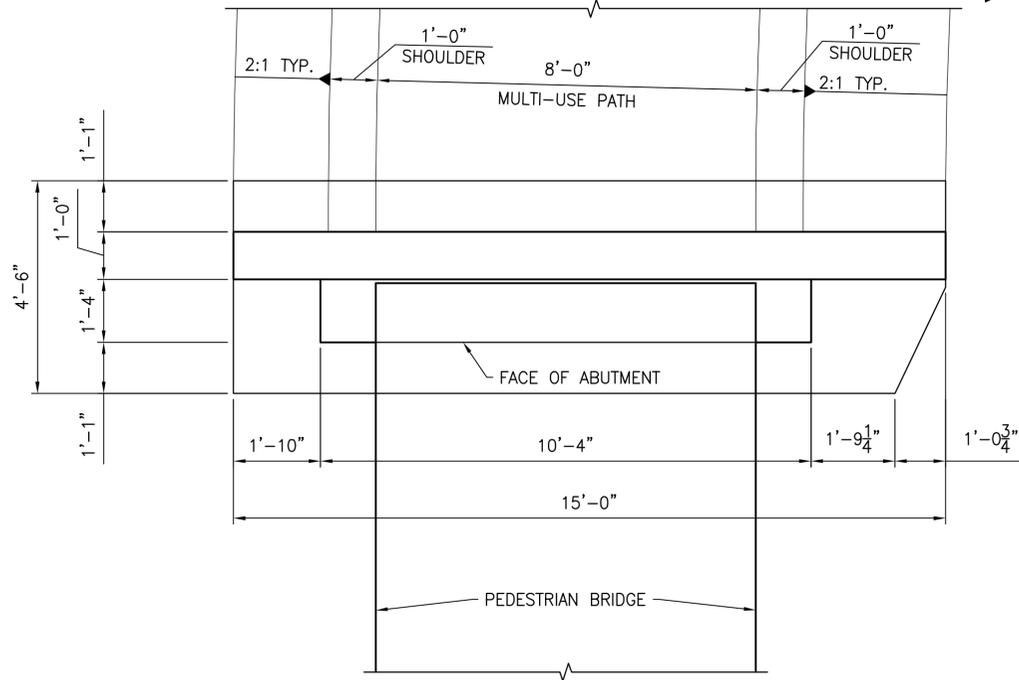
511 CLASS C CONCRETE, ABUTMENT INCLUDING FOOTING, AS PER PLAN

THE PAYMENT FOR THE COST OF ALL MATERIALS AND LABOR ASSOCIATED WITH THE DETAILING, FABRICATION AND ERECTION OF THE NON-EPOXY COATED REINFORCING STEEL IS INCLUDED IN ITEM 511 CLASS C CONCRETE, ABUTMENT INCLUDING FOOTING, AS PER PLAN.

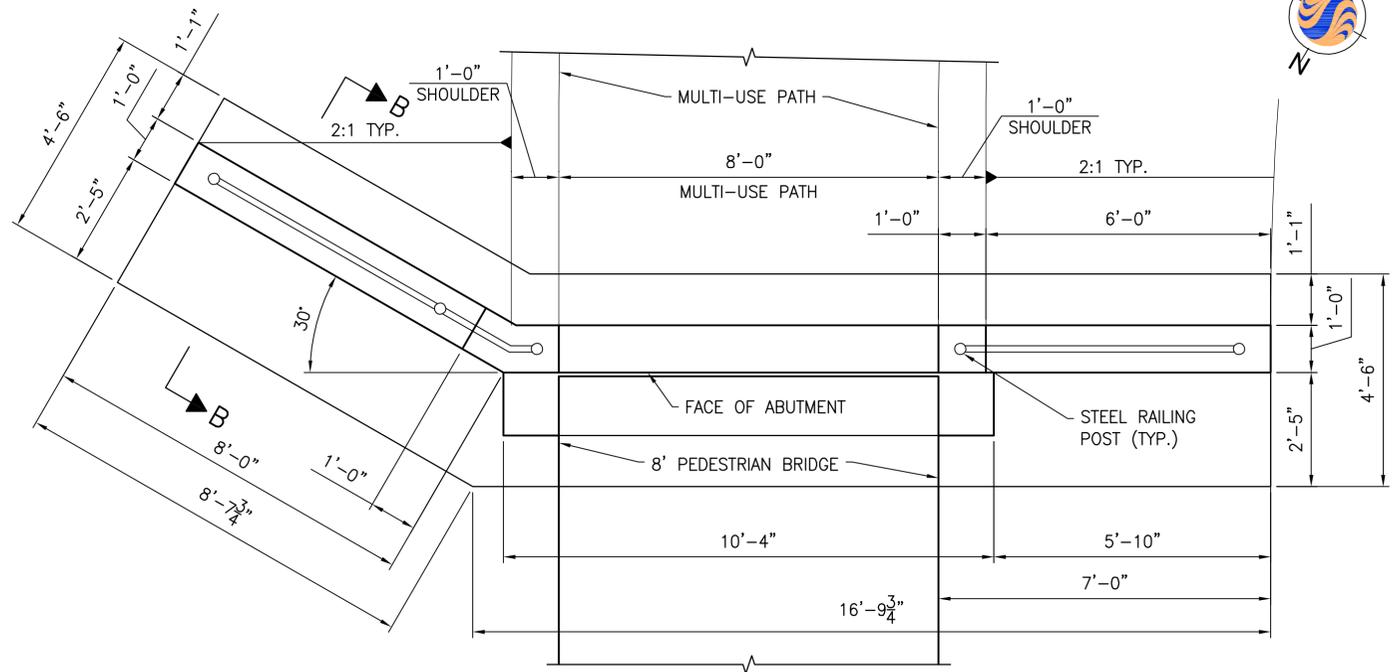


TYP. EXCAVATION OF WEATHERED ROCK

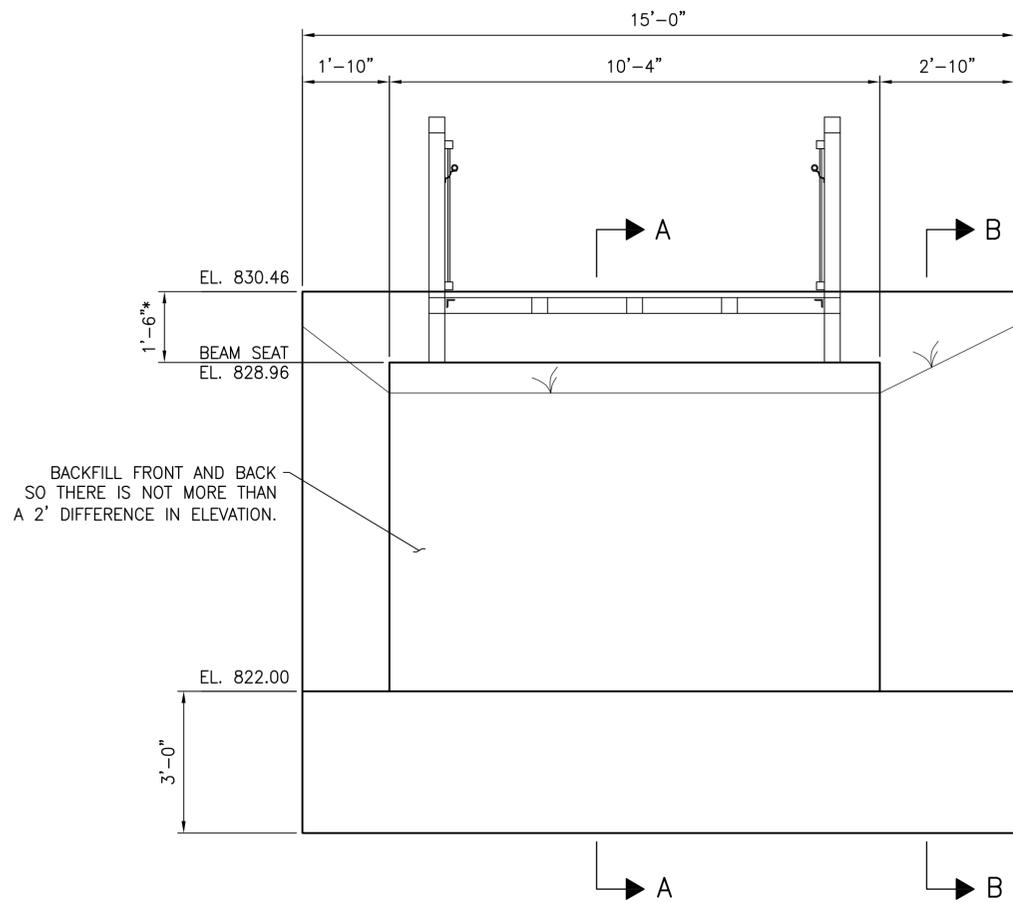
U:\173608736\design\plan_set\Herford to Rings\Bridges\173608736_ab01.dwg SAVED: Wednesday, February 13, 2013 8:34 AM PLOTTED: Monday, March 11, 2013 3:59 PM



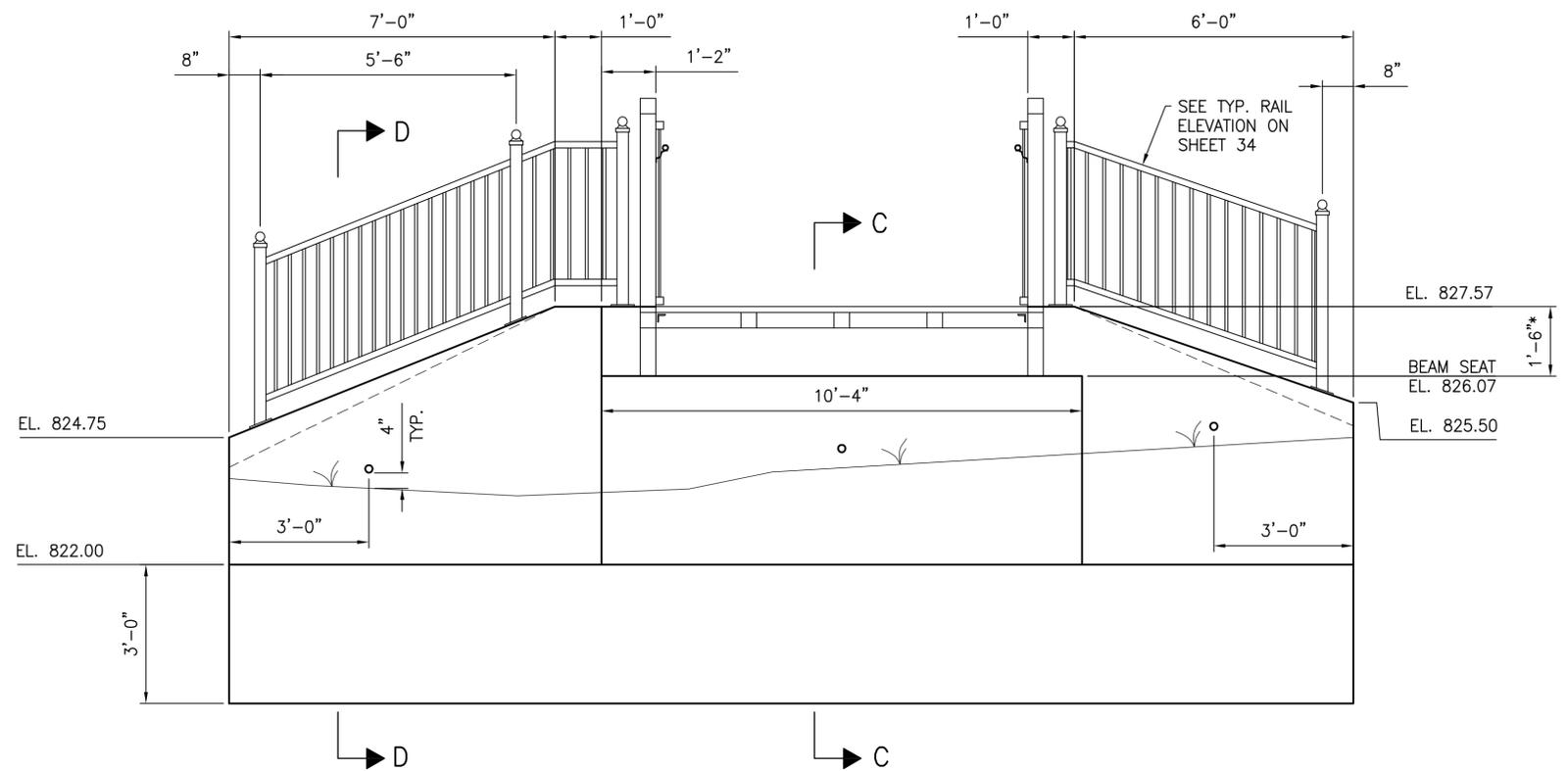
REAR ABUTMENT PLAN



FORWARD ABUTMENT PLAN



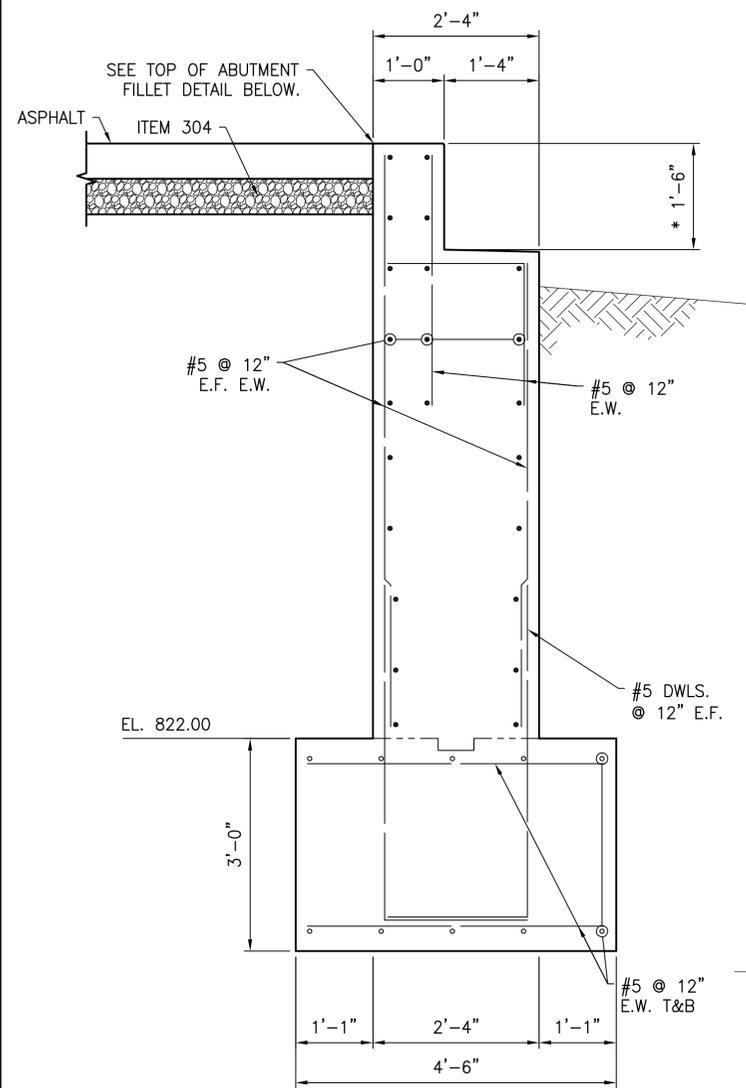
REAR ABUTMENT ELEVATION



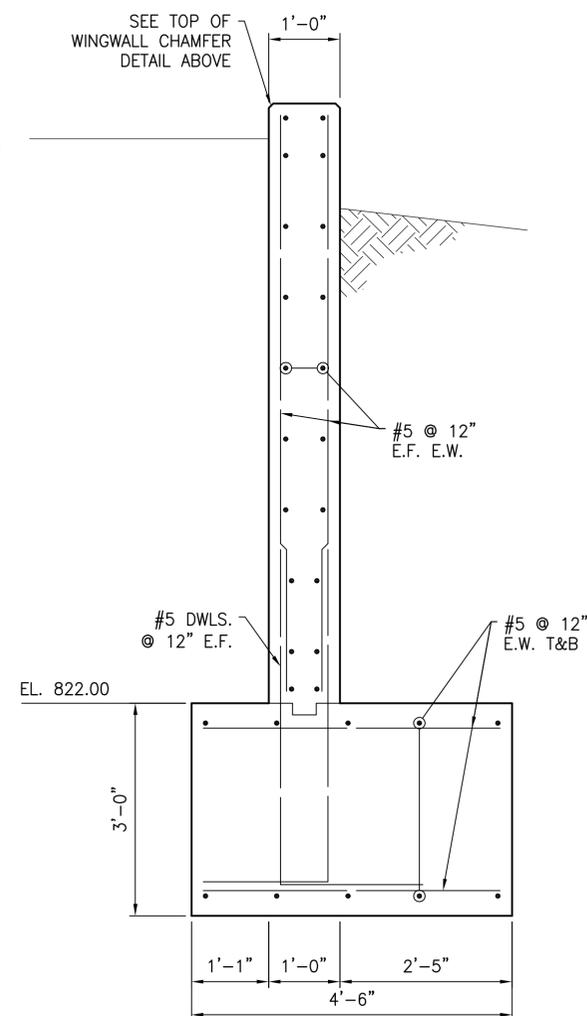
FORWARD ABUTMENT ELEVATION

NOTE:
*VERIFY W/MFR SHOP DRAWINGS

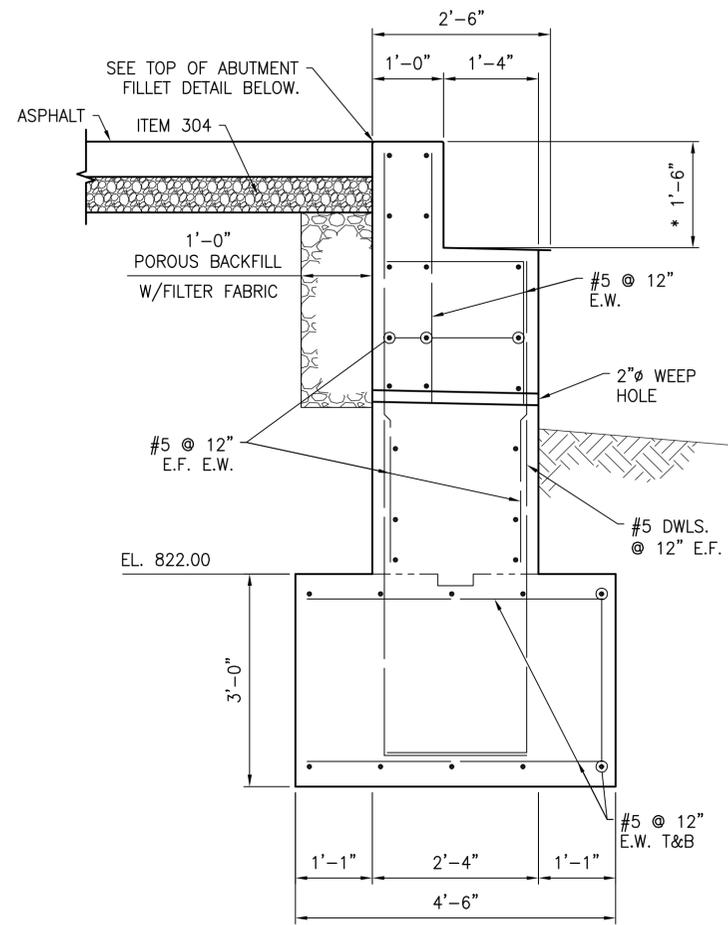
DESIGNED	DATE
BSM	05/12
CHECKED	GT
JWE	
DRAWN	REVIEWED
TWB	GT
REVISED	



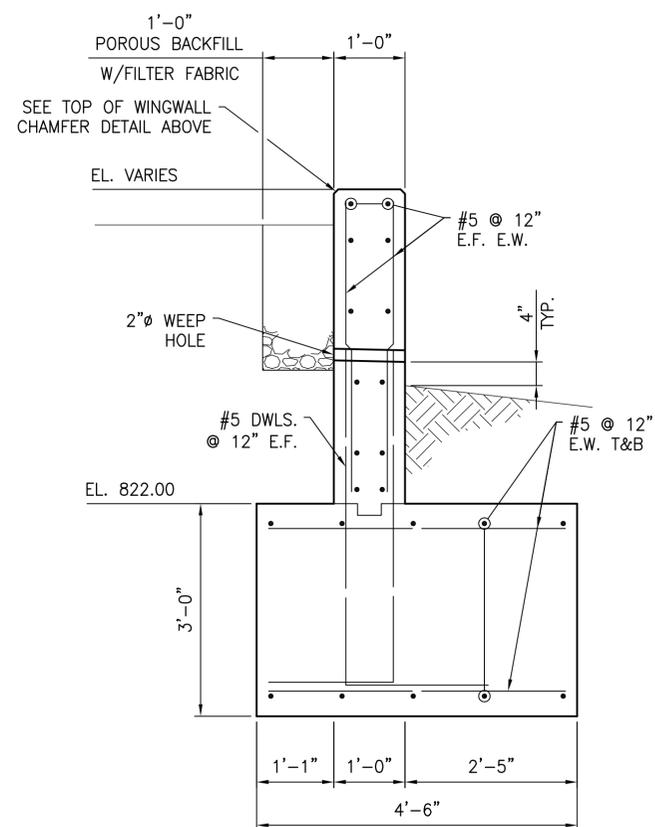
SECTION A-A
* - VERIFY WITH MANUFACTURER



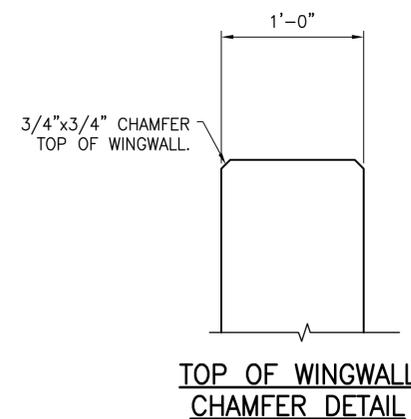
SECTION B-B



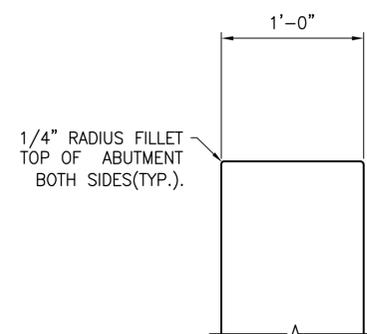
SECTION C-C
* - VERIFY WITH MANUFACTURER



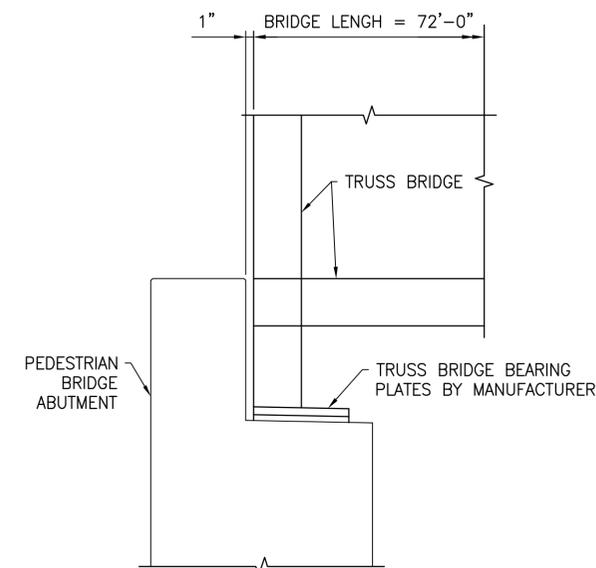
SECTION D-D



TOP OF WINGWALL CHAMFER DETAIL



TOP OF ABUTMENT FILLET DETAIL

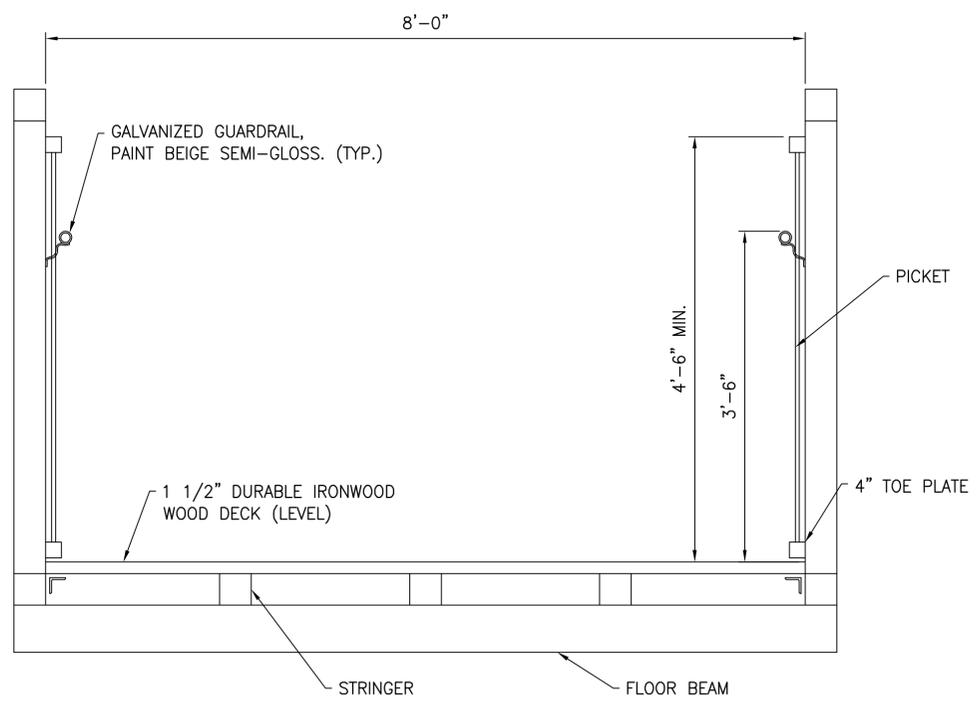


TRUSS BRIDGE BEARING DETAIL

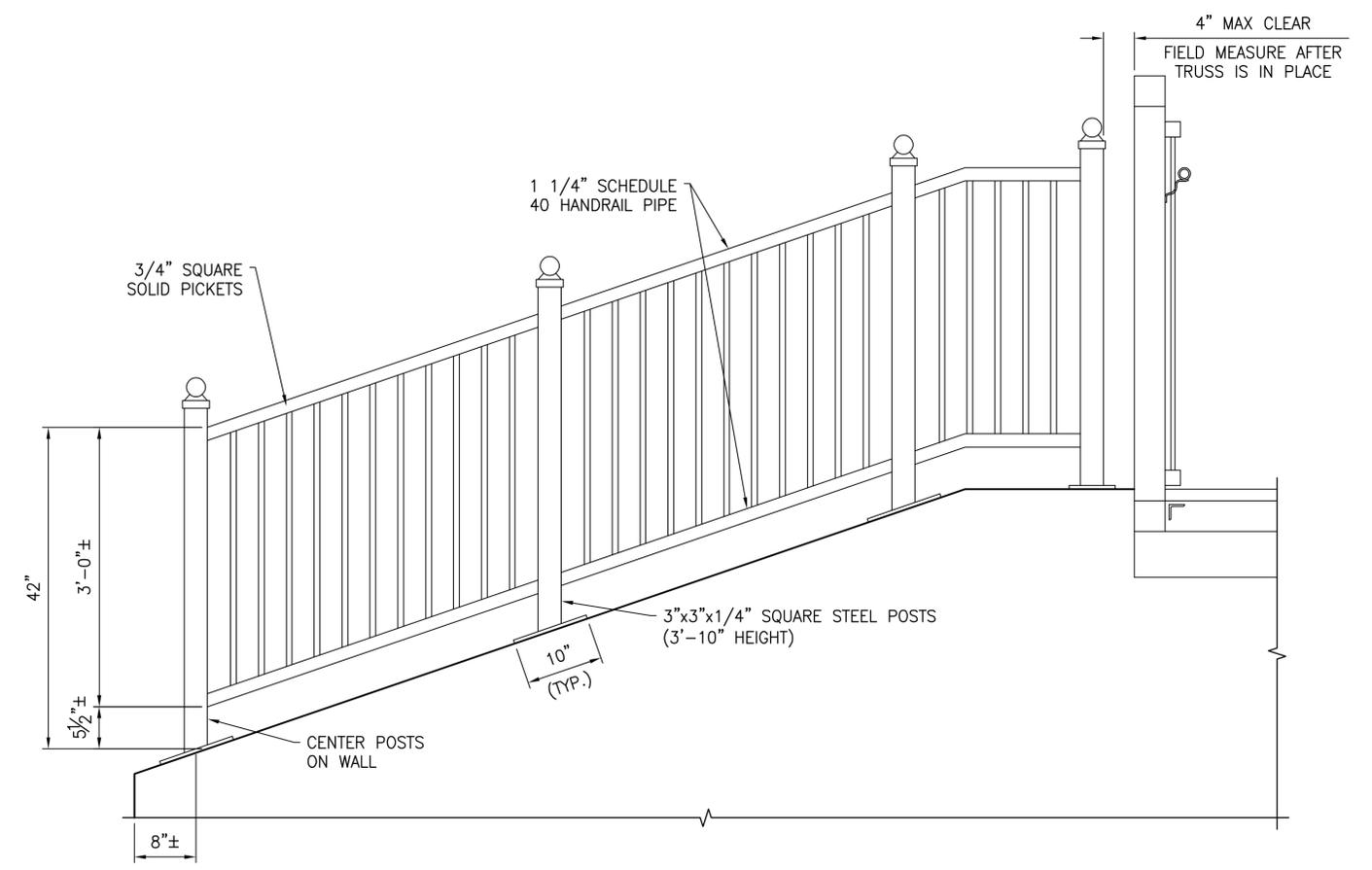
DESIGNED	DRAWN	REVIEWED	DATE
BSM	TWB	GT	05/12
CHECKED	REVISED		
JWE			

TRANSVERSE SECTION

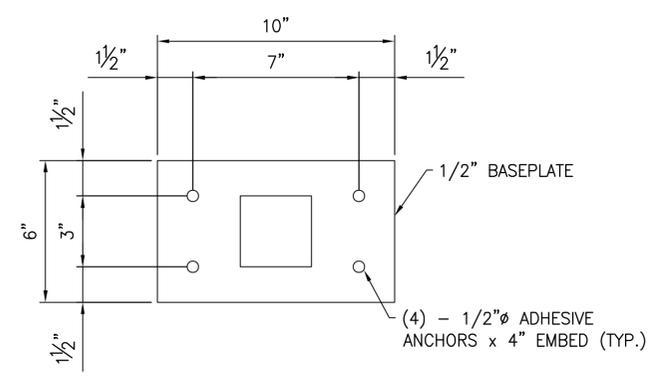
DUBLIN ROAD SOUTH
MULTI-USE PATH



TYPICAL SECTION - PRE-ENGINEERED BRIDGE



TYPICAL RAILING ELEVATION

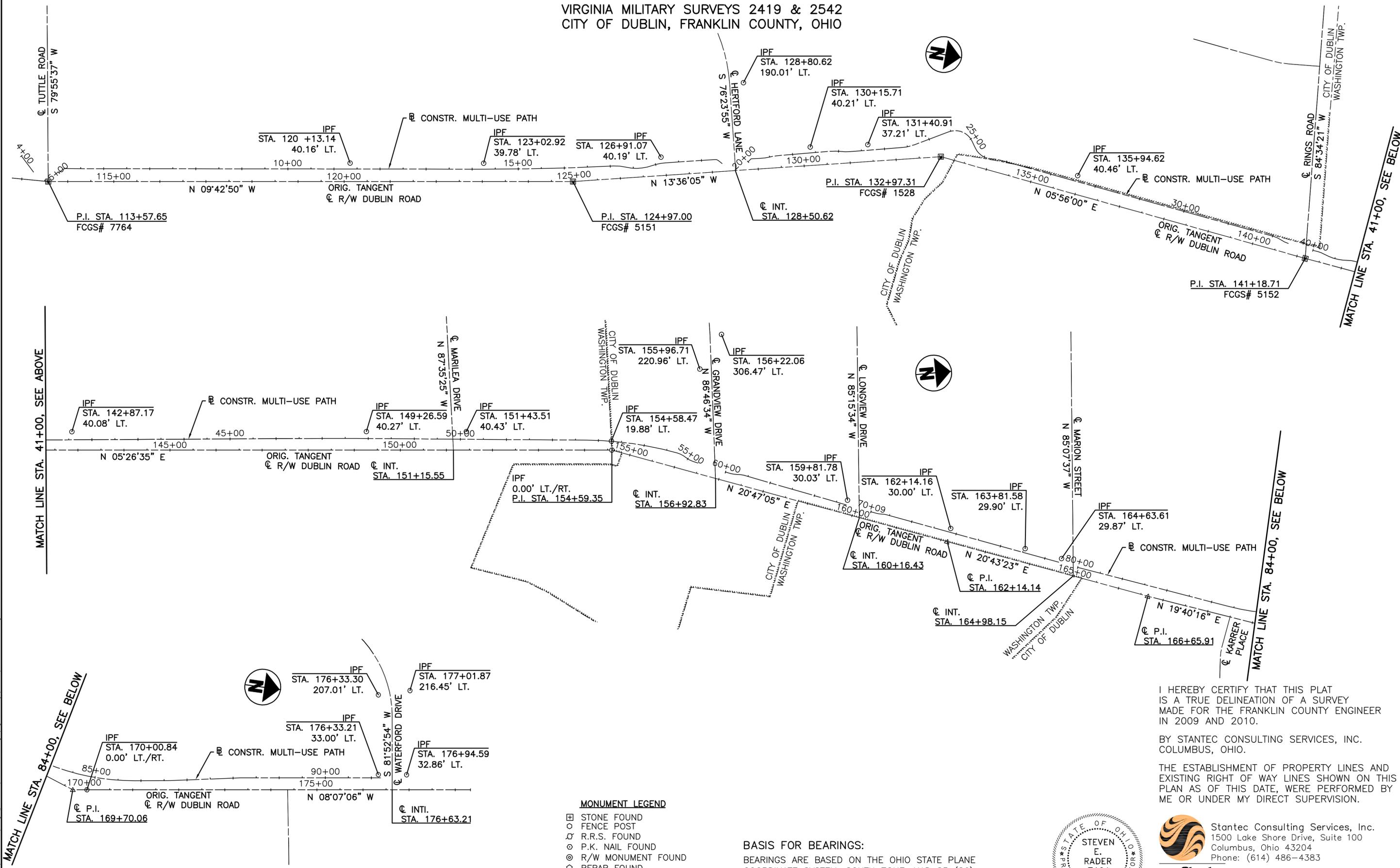


BASEPLATE DETAIL

RAILING
FABRICATE AND INSTALL APPROXIMATELY 14' OF 42" HIGH TUNNEL GUARD RAIL CONSTRUCTED OF 1 1/4" SCHEDULE 40 HANDRAIL PIPE FOR HORIZONTALS. 3/4" SQUARE SOLID PICKETS, NO MORE THAN 4 3/4" ON CENTER. 3"x3" SUPPORT POSTS WITH BALL CAPS AND 6"x10"x3/8" BASE PLATES. POST SPACING NOT TO EXCEED 8' ON CENTER. ALL MATERIAL HOT DIP GALVANIZED AND POWDER COATED SEMI-GLOSS BLACK. ALL RAILINGS MOUNTED WITH 1/2"x4" WEDGE ANCHORS. STAINLESS STEEL HARDWARE.

DUBLIN ROAD SOUTH MULTI-USE PATH

VIRGINIA MILITARY SURVEYS 2419 & 2542
CITY OF DUBLIN, FRANKLIN COUNTY, OHIO



- MONUMENT LEGEND**
- ☐ STONE FOUND
 - FENCE POST
 - ⊕ R.R.S. FOUND
 - ⊙ P.K. NAIL FOUND
 - ⊙ R/W MONUMENT FOUND
 - ⊙ REBAR FOUND
 - IRON PIN FOUND
 - ⊙ I.P.F. WITH I.D. CAP
 - 3/4" IRON PIN SET
 - ☒ MONUMENT BOX

BASIS FOR BEARINGS:
 BEARINGS ARE BASED ON THE OHIO STATE PLANE COORDINATE SYSTEM, SOUTH ZONE, NAD 83 (86), AS ESTABLISHED FROM A GPS SURVEY IN 2009, OCCUPYING FRANKLIN COUNTY GEODETIC CONTROL MONUMENTS "FCGS 8820", "FCGS 9980 RESET", AND "FRANK 26".



SURVEYOR'S SEAL REGISTERED SURVEYOR No. 7191 DATE

Stantec
 Stantec Consulting Services, Inc.
 1500 Lake Shore Drive, Suite 100
 Columbus, Ohio 43204
 Phone: (614) 486-4383

I HEREBY CERTIFY THAT THIS PLAT IS A TRUE DELINEATION OF A SURVEY MADE FOR THE FRANKLIN COUNTY ENGINEER IN 2009 AND 2010.

BY STANTEC CONSULTING SERVICES, INC. COLUMBUS, OHIO.

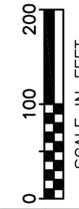
THE ESTABLISHMENT OF PROPERTY LINES AND EXISTING RIGHT OF WAY LINES SHOWN ON THIS PLAN AS OF THIS DATE, WERE PERFORMED BY ME OR UNDER MY DIRECT SUPERVISION.

CALCULATED
 TB
 CHECKED
 SER

CENTERLINE PLAT

DUBLIN ROAD SOUTH
MULTI-USE PATH

MCDANIEL, ARRON U:\173608736\design\plan_seri\Herford to Rings\Right of Way\Details\173608736cd1.dwg CENTERLINE PLAT Last Saved: Jan 09, 2013 8:38 AM Plotted: Mar 11, 2013 3:59 PM



DUBLIN ROAD SOUTH MULTI-USE PATH

VIRGINIA MILITARY SURVEYS 2419 & 2542

CITY OF DUBLIN, FRANKLIN COUNTY, OHIO

BEGIN WORK STA. 4+00

END WORK STA. 91+17.25



MATCH LINE STA. 142+00, SEE ABOVE

MATCH LINE STA. 172+00, SEE ABOVE

MATCH LINE STA. 142+00, SEE BELOW

MATCH LINE STA. 172+00, SEE BELOW

- | | | |
|--|---|---|
| 1 MINOO S. AMINIAN & ARTHUR NEAL OWENS, TRUSTEES | 15 JAMES D. & MOLLY B. NESTER | 27 KENT L. & LINDA D. STEPHENS |
| 2 KAREN M. BROCKWAY, TRUSTEE | 16 CHARLES & ANN MOSES | 28 PAUL E. NULL, JR. |
| 3 HAROLD E. JENKINS & BARBARA BIDDLE JENKINS | 17 RICHARD G. & KAREN L. COLWELL | 29 ISAAC D. & KAY W. PENSON |
| 4 JUDY M. & GEORGE ARDOLINO | 18 TIM R. & JANE E. SAUNDERS | 30 ROSLYN JANE TURNER & LAWRENCE K. WATKINS |
| 5 CHRIS & LAURIE L. FANNIN | 19 LEESA M. MARTIN & JAMES G. KINCAID | 31 CYNTHIA S. TAWNEY |
| 6 BOGI V. GUDBRANDSSON & JENNIFER S. RIDENOUR | 20 ADAM P. JOHNSON | 32 ELIZABETH MILLER |
| 7 LINDA F. FORSYTHE | 21 KEVIN M. & KAREN L. KLOSTERMAN | 33 DARRELL L. & VICTORIA L. CRAMBLIT |
| 8 CAROLYN S. ALLEN-ENDERLE | 22 CHRISTOPHER & BETTY HEAD | 34 JAMES A. & CATHERINE T. HARDEN |
| 9 WILLIAM S. NEWCOMB, JR. & NANNETTE L. NEWCOMB | 23 THOMAS A. McDOWELL & ALLEN E. McDOWELL, TRUSTEES | 37 FREDERICK M. COGGIN & JUDITH W. HARVEY |
| 10 WILLIAM S. NEWCOMB, JR. & NANNETTE L. NEWCOMB | 24 JACK M. & SUE E. MCKITRICK | 38 BRUCE C. & SHARON C. MILLER |
| 11 FREDERICK M. & MELISSA J. SCHROEDER | 25 THOMAS A. McDOWELL & ALLEN E. McDOWELL, TRUSTEES | 39 JANE E. FOX |
| 12 LAWRENCE W. & LINDA L. LEHNER | 26 GARY G. & COLLEEN D. GREEN | 40 MICHAEL F. & LORI L. HARRIS |
| | | 41 CITY OF DUBLIN |

REV.	DATE	DESCRIPTION
DATE OF COMPLETION:		08-13-2012

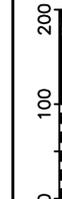
PROPERTY MAP

DUBLIN ROAD SOUTH MULTI-USE PATH

36
37

MCDANIEL, ARRON U:\173608736\design\plan_set\Herford to Rings\plan_set\Herford to Rings.dwg PROPERTY MAP Last Saved: Jan 09, 2013 2:43 PM Plotted: Mar 11, 2013 3:59 PM

CALCULATED
SER
CHECKED
TB



LLEWELLYN FARMS
SEC. 3, PT. 2
P.B. 66, PG. 94

LLEWELLYN FARMS
SEC. 3, PT. 1
P.B. 66, PG. 64

LLEWELLYN FARMS
SEC. 2, PT. 1
P.B. 64, PG. 9

LLEWELLYN FARMS
SEC. 1
P.B. 60, PG. 31

LEPPERT SUBDIVISION
P.B. 27, PG. 13

LEPPERT SUBDIVISION
P.B. 27, PG. 13

GOULD ADDITION NO. 2
P.B. 27, PG. 73

GOULD ADDITION NO. 2
P.B. 27, PG. 36

LONGVIEW ADDITION
P.B. 23, PG. 1

SOUTH CIVIC ADDITION
P.B. 22, PG. 71

V.M.S. 2419
V.M.S. 2542

= FRANKLIN COUNTY RECORDER'S
OFFICE INSTRUMENT NUMBER

NET RESIDUE = RECORD AREA - TOTAL PRO-NET TAKE

* DENOTES RIGHT-OF-WAY ENCROACHMENT

GRANTEE:
ALL RIGHT OF WAY ACQUIRED IN THE
NAME OF CITY OF DUBLIN, OHIO
UNLESS OTHERWISE SHOWN.

PARCEL NO.	OWNER	SHEET NO.	OWNERS RECORD		AUDITORS PARCEL	RECORD AREA	TOTAL P.R.O.	GROSS TAKE	P.R.O. IN TAKE	NET TAKE	STRUC-TURE	NET RESIDUE		TYPE FUND	REMARKS AND PERSONALTY	AS ACQUIRED INSTR. NO.
			BOOK	PAGE								LEFT	RIGHT			
12	LAWRENCE W. & LINDA L. LEHNER		O.R. 7368	D13	273-003108										LOT 50 OF LLEWELLYN FARMS S-2, P-1 NO R/W REQUIRED	
13-T	LARRY E. & LANITA L. AYERS		O.R. 16202	H04	273-003107		0.011	0	0.011	NO				CITY	LOT 49 OF LLEWELLYN FARMS S-2, P-1 RECONSTRUCT DRIVE	
14-P	JOHN R. & JOAN WERNZ		O.R. 3612	B01	273-002026		0.069	0	0.069	NO				CITY	LOT 48 OF LLEWELLYN FARMS S-1 UTILITY, GRADING, AND MULTI-USE PATH	
15-P	JAMES D. & MOLLY B. NESTER		O.R. 34321	A08	273-002025		0.130	0	0.130	NO				CITY	LOT 47 OF LLEWELLYN FARMS S-1 UTILITY, GRADING, AND MULTI-USE PATH	
15-T							0.031	0	0.031	NO				CITY	RECONSTRUCT DRIVE	
16-T	CHARLES & ANN MOSES		O.R. 30866	D19	273-002024		0.042	0	0.042	NO				CITY	LOT 46 OF LLEWELLYN FARMS S-1 GRADING, RECONSTRUCT DRIVE	
17-T	RICHARD G. & KAREN L. COLWELL		O.R. 8423	D15	273-002023		0.038	0	0.038	NO				CITY	LOT 45 OF LLEWELLYN FARMS S-1 GRADING, RECONSTRUCT DRIVE	
18-T	TIM R. & JANE E. SAUNDERS		#19981124	0303107	273-002022		0.021	0	0.021	NO				CITY	LOT 44 OF LLEWELLYN FARMS S-1 GRADING, RECONSTRUCT DRIVE	
19-T	LEESA M. MARTIN & JAMES G. KINCAID		#19990721	0185222	273-002021		0.015	0	0.015	NO				CITY	LOT 43 OF LLEWELLYN FARMS S-1 GRADING	
20-P	ADAM P. JOHNSON		#20120403	0045766	270-000304	0.957	0.045	0	0.045	NO				CITY	UTILITY, GRADING, AND MULTI-USE PATH	

ALL AREAS ARE IN ACRES FROM THE COUNTY
AUDITOR'S RECORDS, UNLESS OTHERWISE SHOWN.

NOTE: (C) = CALCULATED AREA

NOTE: UNDER NO CIRCUMSTANCES ARE TEMPORARY EASEMENTS
TO BE USED FOR STORAGE OF MATERIAL OR EQUIPMENT
BY THE CONTRACTOR, UNLESS NOTED OTHERWISE.

NOTE: ALL TEMPORARY PARCELS TO
BE OF 12 MONTHS DURATION.

PARCEL LEGEND

- P = PERMANENT EASEMENT
- T = TEMPORARY EASEMENT
- S = SEWER EASEMENT
- SL = SLOPE EASEMENT
- CH = DRAINAGE EASEMENT

REV. BY	DATE	DESCRIPTION
FIELD REVIEW BY	DATE:	
OWNERSHIP VERIFIED BY	JWS	DATE: 04-24-12
DATE COMPLETED		08-13-12

C.I.P. NUMBER 08-009.2
 RW DESIGNER TB RW REVIEWER SER
 SUMMARY OF ADDITIONAL RIGHT-OF-WAY
 DUBLIN ROAD SOUTH MULTI-USE PATH
 37
 37

MCDANIEL, ARRON U:\173608736\design\plan_set\Herford to Rings\Summaries\173608736rs02.dwg SUMMARY OF ADDITIONAL RIGHT-OF-WAY Last Saved: Jan 09, 2013 2:42 PM Plotted: Mar 11, 2013 3:59 PM