



Office of the City Manager
5200 Emerald Parkway • Dublin, OH 43017-1090
Phone: 614-410-4400 • Fax: 614-410-4490

Memo

To: Members of Dublin City Council
From: Dana L. McDaniel, City Manager 
Date: February 19, 2015
Initiated By: Steve Langworthy, Planning Director
Jennifer M. Rauch, AICP, Senior Planner
Re: Ordinance 13-15 - Rezoning approximately 3.3 acres ± at the northeast corner of the intersection of Emerald Parkway and Bright Road from R-1, Restricted Suburban Residential District to SO, Suburban Office and Institutional District. (Case 15-006Z/CU)

Summary

Ordinance 13-15 is a request for review and approval of a standard district rezoning from R-1, Restricted Suburban Residential District to SO, Suburban Office and Institutional District for a 3.3-acre site at the northeast corner of the intersection of Emerald Parkway and Bright Road in accordance with the City of Dublin Community Plan.

Background

On February 5, 2015, the Planning and Zoning Commission recommended approval to City Council of this rezoning application. As a separate action, the Commission postponed a conditional use request for a COTA park and ride facility, pending further information on site conditions related to stormwater management and tree preservation.

Community Plan

Future Land Use

The Future Land Use Map designates this site and areas to the east as Neighborhood Office/Institutional. Density limits under this designation are not more than 9,500 building square footage per acre. This would permit, depending on site design, office or institutional uses of up to 46,550 square feet, which, under office development would require between 187 and 233 parking spaces, depending on the type of office use. The Plan describes this land use classification as being used for sites adjacent to residential areas where land use transitions or buffers are necessary. Development character is intended to have low lot coverages, greater setbacks from non-residential uses and extensive landscaping. The equivalent zone district to this classification is SO, Suburban Office and Institutional District.

Bright Road Area Plan

This site is included in the Bright Road Area Plan. The main goal of the Area Plan is to build upon and enhance the existing residential character of Bright Road between Riverside Drive and Emerald Parkway while ensuring the preservation of key natural features and historic sites. The Plan calls for high quality office development to be encouraged along Emerald Parkway that focuses on quality architecture and site design that complements the surrounding natural

environment and residential neighborhoods. The Area Plan also calls for improved traffic circulation, access and movement and the use of capital improvements, such as the completion of Emerald Parkway, as a catalyst for development that provides more transportation options. The Area Plan also identifies the preservation of important archaeological and natural features. A significant number of trees and a portion of the Billingsley Creek are located in the northern portion of the site and will need to be preserved to the extent possible.

Neighborhood/Association Contact

City and COTA representatives held a neighborhood meeting on January 7, 2015 to introduce the proposed COTA park and ride relocation. A summary of the comments and discussion have been included in the packet. Additionally, a number of residents provided written comments or testimony as part of the February 5th Planning and Zoning Commission meeting. Among the comments were concerns that the rezoning to an office district was premature, disagreement with the Community Plan office designation, concerns with COTA's plans for park and ride consolidations and route changes, perceived lack of transportation options within the Bridge Street District, and traffic concerns related to Bright Road and Sawmill Road.

Case Description

Site

This City-owned site is 3.3 acres and currently undeveloped with frontage on both Emerald Parkway and Bright Road. Several single-family homes were demolished on the parcels and used for construction staging for the completion of the last section of Emerald Parkway and the roundabout with Bright Road. Billingsley Creek and the associated floodplain are located along the northern property line including existing vegetation along the creek as well as the eastern property line.

Existing and Adjacent Zoning

To the east and north are properties zoned PUD, Planned Unit Development District – NE Quad, which permit uses within SO, Suburban Office and Institutional District, ambulatory health care services, day care, assisted and skilled care facilities and pharmacy and medical supply sales. To the west and south are properties zoned R-1, Limited Suburban Residential District, which would permit single-family residences, parks, private school, type-b child care and accessory child and adult daycare uses.

Proposed Zoning: SO, Suburban Office and Institutional District

153.026 (A) Uses

Permitted uses in the district are administrative and business offices, professional offices, institutions, organizations and associations, and child and adult daycare centers.

153.021 (B) Conditional Uses

Conditional uses include such uses as auto-oriented commercial facilities or outdoor service facilities, educational and research, restaurants, fitness uses, and park and ride facilities.

153.021 (C) Development Standards

Code does not require a minimum lot size or width. Required side and rear yard setbacks are 15 feet.

Conditional Use Application

The conditional use application was postponed to permit the applicant to address requested modifications to the site plan prior to determine whether the use of permeable pavers or pavement or other alternative stormwater measures could be examined, decrease the size of the retention pond and move it farther from the floodplain and creek, create a riparian corridor planting zone along the north side of the pond adjacent to the creek, and identify areas for additional tree preservation. The information reviewed by the Planning and Zoning Commission has been included for your reference.

Recommendation of the Planning and Zoning Commission

On February 5, 2015 the Planning and Zoning Commission recommended approval to City Council of the standard district rezoning determining the request was in accordance with the Community Plan.

Recommendation

Planning recommends City Council approval of Ordinance 13-15 at the second reading/public hearing on March 9, 2015.

RECORD OF ORDINANCES

Dayton Legal Blank, Inc.

Form No. 30043

Ordinance No. 13-15

Passed _____, 20____

AN ORDINANCE REZONING APPROXIMATELY 3.3 ACRES ± AT THE NORTHEAST CORNER OF THE INTERSECTION OF EMERALD PARKWAY AND BRIGHT ROAD FROM R-1, RESTRICTED SUBURBAN RESIDENTIAL DISTRICT TO SO, SUBURBAN OFFICE AND INSTITUTIONAL DISTRICT. (CASE 15-006Z/CU)

NOW, THEREFORE, BE IT ORDAINED by the Council of the City of Dublin, _____ of its elected members concurring, that:

Section 1. The following described real estate, (see attached legal description, Exhibit A), situated in the City of Dublin, State of Ohio, is hereby rezoned SO, Suburban Office and Institutional District, and shall be subject to regulations and procedures contained in Ordinance No. 21-70 (Chapter 153 of the Codified Ordinances), the City of Dublin Zoning Code and amendments thereto.

Section 2. The application, including the list of contiguous and affected property owners, and the recommendations of the Planning and Zoning Commission, are all incorporated into and made an official part of this Ordinance and said real estate shall be developed and used in accordance there within.

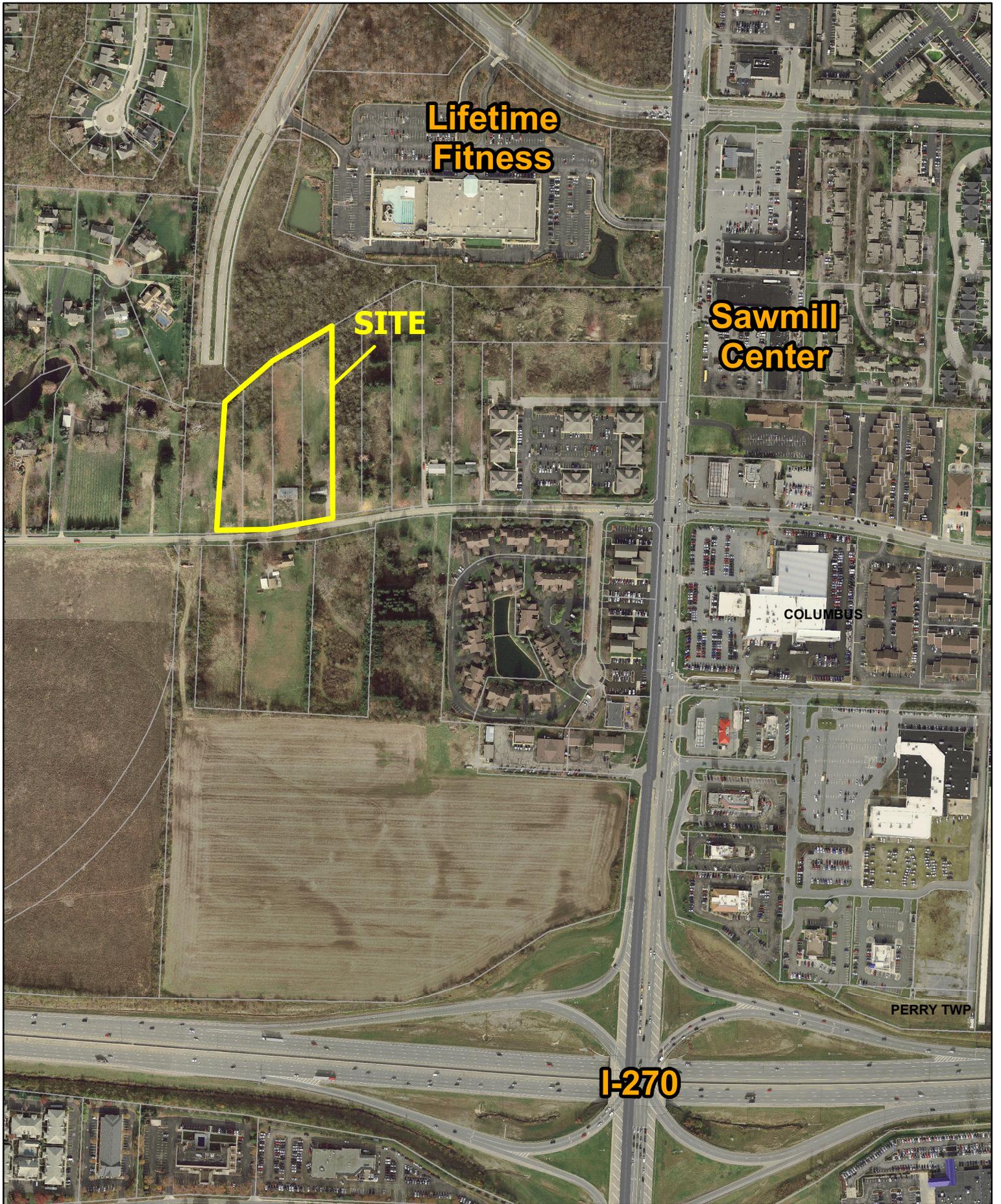
Section 3. This Ordinance shall take effect and be in force from and after the earliest period allowed by law.

Passed this _____ day of _____, 2015.

Mayor - Presiding Officer

ATTEST:

Clerk of Council



City of Dublin

15-006Z/CU
Rezoning / Conditional Use
COTA Park & Ride
4000 Bright Road

0 150 300
Feet





CITY OF DUBLIN.

Land Use and Long Range Planning
5800 Shier-Rings Road
Dublin, Ohio 43016-1236
Phone/TDD: 614-410-4600
Fax: 614-410-4747
Web Site: www.dublin.oh.us

PLANNING AND ZONING COMMISSION APPLICATION

(Code Section 153.232)

I. PLEASE CHECK THE TYPE OF APPLICATION:

<input type="checkbox"/> Informal Review	<input type="checkbox"/> Final Plat (Section 152.085)
<input type="checkbox"/> Concept Plan (Section 153.056(A)(1))	<input checked="" type="checkbox"/> Conditional Use (Section 153.236)
<input type="checkbox"/> Preliminary Development Plan / Rezoning (Section 153.053)	<input type="checkbox"/> Corridor Development District (CDD) (Section 153.115)
<input type="checkbox"/> Final Development Plan (Section 153.053(E))	<input type="checkbox"/> Corridor Development District (CDD) Sign (Section 153.115)
<input type="checkbox"/> Amended Final Development Plan (Section 153.053(E))	<input type="checkbox"/> Minor Subdivision
<input checked="" type="checkbox"/> Standard District Rezoning (Section 153.018)	<input type="checkbox"/> Right-of-Way Encroachment
<input type="checkbox"/> Preliminary Plat (Section 152.015)	<input type="checkbox"/> Other (Please Specify): _____

Please utilize the applicable *Supplemental Application Requirements* sheet for additional submittal requirements that will need to accompany this application form.

II. PROPERTY INFORMATION: This section must be completed.

Property Address(es): 4030, 4000, 3960 Bright Road, Dublin, Ohio 43017	
Tax ID/Parcel Number(s): 273008632 273008633 273008634	Parcel Size(s) (Acres): 1.24 1.39 1.48 <u>4.11</u>
Existing Land Use/Development: Residential	

IF APPLICABLE, PLEASE COMPLETE THE FOLLOWING:

Proposed Land Use/Development: Office Use/COTA Park and Ride
Total acres affected by application: 4.11 acres

III. CURRENT PROPERTY OWNER(S): Please attach additional sheets if needed.

Name (Individual or Organization): Marsha Grigsby, City Manager, City of Dublin	
Mailing Address: 5200 Emerald Parkway Dublin, Ohio 43017 (Street, City, State, Zip Code)	
Daytime Telephone: 614.410.4400	Fax: 614.410.4490
Email or Alternate Contact Information:	

RECEIVED

JAN 20 2015
15-006 CV/Z
CITY OF DUBLIN

IV. APPLICANT(S): This is the person(s) who is submitting the application if different than the property owner(s) listed in part III. Please complete if applicable.

Name:	Applicant is also property owner: yes <input type="checkbox"/> no <input type="checkbox"/>
Organization (Owner, Developer, Contractor, etc.):	
Mailing Address: (Street, City, State, Zip Code)	
Daytime Telephone:	Fax:
Email or Alternate Contact Information:	

V. REPRESENTATIVE(S) OF APPLICANT / PROPERTY OWNER: This is the person(s) who is submitting the application on behalf of the applicant listed in part IV or property owner listed in part III. Please complete if applicable.

Name: Jennifer Rauch, Senior Planner	
Organization (Owner, Developer, Contractor, etc.): City of Dublin	
Mailing Address: (Street, City, State, Zip Code) 5800 Shier Rings Road	
Daytime Telephone: 614.410.4690	Fax: 614.410.4747
Email or Alternate Contact Information: jrauch@dublin.oh.us	

VI. AUTHORIZATION FOR OWNER'S APPLICANT or REPRESENTATIVE(S): If the applicant is not the property owner, this section must be completed and notarized.

<p>I, <u>Marsha Grigsby, City Manager</u>, the owner, hereby authorize <u>Jennifer Rauch</u> to act as my applicant or representative(s) in all matters pertaining to the processing and approval of this application, including modifying the project. I agree to be bound by all representations and agreements made by the designated representative.</p>	
Signature of Current Property Owner: 	Date: 1/13/15

Check this box if the Authorization for Owner's Applicant or Representative(s) is attached as a separate document

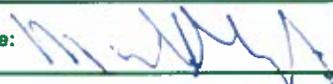
Subscribed and sworn before me this 14 day of January, 2015
 State of Ohio
 County of Franklin

Notary Public Linda L. Glick



Linda L. Glick
 Notary Public, State of Ohio
 My Commission Expires 05-19-2019

VII. AUTHORIZATION TO VISIT THE PROPERTY: Site visits to the property by City representatives are essential to process this application. The Owner/Applicant, as noted below, hereby authorizes City representatives to visit, photograph and post a notice on the property described in this application.

<p>I, <u>Marsha Grigsby, City Manager</u>, the owner or authorized representative, hereby authorize City representatives to visit, photograph and post a notice on the property described in this application.</p>	
Signature of applicant or authorized representative: 	Date: 1/13/15

RECEIVED

JAN 20 2015
 15-006 cu/z
 CITY OF DUBLIN

VIII. UTILITY DISCLAIMER: The Owner/Applicant acknowledges the approval of this request for review by the Dublin Planning and Zoning Commission and/or Dublin City Council does not constitute a guarantee or binding commitment that the City of Dublin will be able to provide essential services such as water and sewer facilities when needed by said Owner/Applicant.

I, <u>Marsha Grigsby, City Manager</u> , the owner or authorized representative, acknowledge that approval of this request does not constitute a guarantee or binding commitment that the City of Dublin will be able to provide essential services such as water and sewer facilities when needed by said Owner/Applicant.	
Signature of applicant or authorized representative: <u>[Signature]</u>	Date: 1/13/15

IX. APPLICANT'S AFFIDAVIT: This section must be completed and notarized.

I, <u>Marsha Grigsby, City Manager</u> , the owner or authorized representative, have read and understand the contents of this application. The information contained in this application, attached exhibits and other information submitted is complete and in all respects true and correct, to the best of my knowledge and belief.	
Signature of applicant or authorized representative: <u>[Signature]</u>	Date: 1/13/15

Subscribed and sworn to before me this 14 day of January, 2015
 State of Ohio
 County of Franklin Notary Public Linda L. Glick



Linda L. Glick
 Notary Public, State of Ohio
 My Commission Expires 05-19-2019

FOR OFFICE USE ONLY			
Amount Received: <u>NA</u>	Application No: <u>15-006cu/z</u>	P&Z Date(s):	P&Z Action:
Receipt No: <u>N/A</u>	Map Zone: <u>C-2/1</u>	Date Received: <u>1/20/15</u>	Received By: <u>JMP</u>
City Council (First Reading):		City Council (Second Reading):	
City Council Action:		Ordinance Number:	
Type of Request: <u>Conditional Use * Standard Rezoning</u>			
<input checked="" type="radio"/> N, S, E, W (Circle) Side of: <u>Bright Rd</u>			
<input type="radio"/> N, S, E, W (Circle) Side of Nearest Intersection: <u>Emerald Pkwy</u>			
Distance from Nearest Intersection: <u>at</u>			
Existing Zoning District: <u>R1</u>		Requested Zoning District: <u>S0</u>	

RECEIVED
 JAN 20 2015
 15-006 CU/Z
 CITY OF DUBLIN
 PLANNING



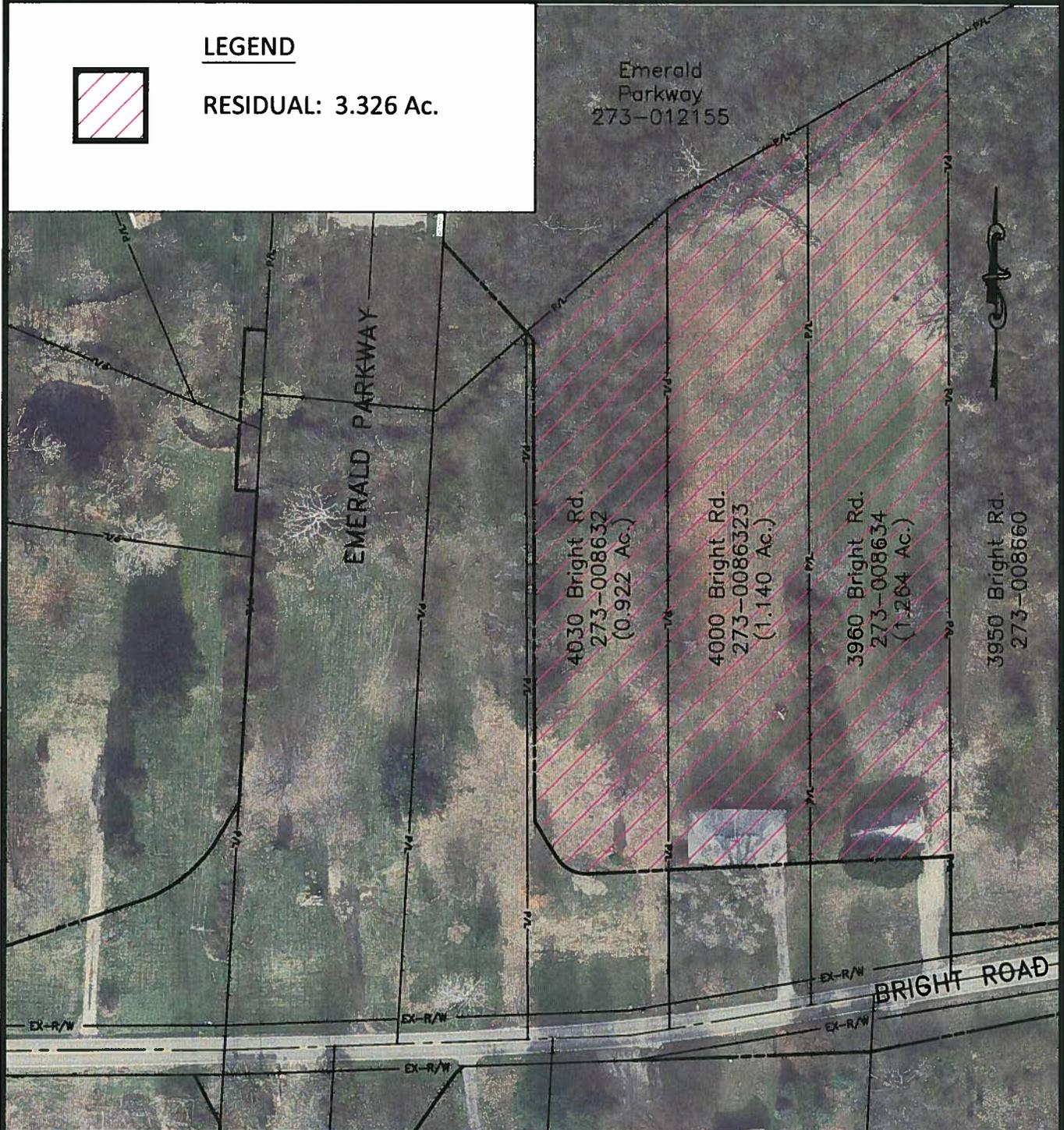
5800 Shier Rings Road • Dublin, Ohio 43016-1236
 Phone (614)410-4800 • Fax (614)410-4899

**EMERALD PARKWAY
 PHASE 8**
 3960, 4000 & 4030
 BRIGHT ROAD
 DUBLIN, OHIO 43017

LEGEND



RESIDUAL: 3.326 Ac.



DRAWN	CHECKED	DATE:	JOB NO.
MSS		08/05/14	07-008



Case #15-006 CU/Z

*City of Dublin
Dana McDaniel, City Manager
5200 Emerald Parkway
Dublin, OH 43017

*Jenny Rauch, Sr. Planner
City of Dublin
5800 Shier Rings Road
Dublin, OH 43016

Jeffrey L. Asman
4158 Bright Road
Dublin, OH 43016

Asman Land Co Ltd
6969 Industrial Parkway
Dublin, OH 43016

Steve & Candice Barnhardt
4143 Macduff
Dublin, OH 43016

Jay Simonds
3570 Jenmar Court
Dublin, OH 43016

Jonathan & Vickie Leffler
4171 Macduff
Dublin, OH 43016

Robert & Marian Schuda
4181 Macduff
Dublin, OH 43016

John & Rosemary Wreathall
4157 Macduff
Dublin, OH 43016

Borrer Realty Co
4900 Tuttle Crossing Blvd
Dublin, OH 43016-0993

Iacovetta Properties Ltd
2525 Fisher Rd
Columbus, OH 43204

Ruth & Stewart Maier
Sharon Secrest
4025 Bright Road
Dublin, OH 43016

Diane Armstrong
6988 Grandee Cliffs Dr
Dublin, OH 43016

McKitrick Properties Inc
6724 Perimeter Lp Rd, ST 300
Dublin, OH 43017

JLP-Bright Road LLC
4300 E Fifth Ave
Columbus, OH 43219

LTF Real Estate Co Inc
3825 Hard Road
Dublin, OH 43016

Amy Kramb
7511 Riverside Drive
Dublin, Ohio 43017

Donald Spangler
3614 Jenmar Court
Dublin, OH 43016

Sandra Taylor
7143 Grandee Cliffs Drive
Dublin, OH 43016

Carla Clifton
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Dublin, OH 43016

Pat Terrell
7243 Inverness Court
Dublin, OH 43016

Julia Felts
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Dublin, OH 43016

Alison Sweeney
6987 Grandee Cliffs
Dublin, Ohio 43016

Sharon Huber
4345 Bright Road
Dublin, Ohio 43016

Asman Land Co Ltd
4150 Bright Road
Dublin, OH 43016

Judy Long
4345 Bright Road
Dublin, OH 43016

Neal Johnson
7172 Grandee Cliffs Dr
Dublin, OH 43016

Randy Roth
6987 Grandee Cliffs Dr
Dublin, OH 43016

Deb Allard
7291 Macbeth Drive
Dublin, OH 43016

Scott Clayton
7239 Sawmill Road
Dublin, OH 43016

Design Recommendations

- Design Detail
- Development Opportunity
- General
- Mobility
- Open Space
- Value



Existing Character

The Bright Road focus area is bordered by Hard Road and I-270, and stretches west from Sawmill Road to Riverside Drive, comprising the northeast portion of the Emerald Corporate District. Defined by the planned extension of Emerald Parkway, the area includes a significant list of planning and land use challenges that were initially addressed in the 1990 Bright Road Land Use Study.

Physical features vary significantly throughout the area, and portions along Riverside Drive are of scenic and historic significance. The area is characterized by the remnants of three geometric earth mounds (known locally as the Holder-Wright Earthworks) constructed between 300 B.C. and 500 A.D. by the Hopewell tribe. The ceremonial mounds consist of a large rectangular enclosure approximately 390 feet by 220 feet in size and two circular bank-and-ditch enclosures located nearby. Archaeologists interpret the earthen enclosures as symbolic forms used as a locus for periodic mortuary or other ritual activity. The area also contains scenic portions of Wright's Run (also known as Billingsley Creek), providing substantial wooded areas, waterfalls and ravines. This western portion of the study area includes terrain that slopes significantly up the River bluff to Grandee Cliffs Drive, while portions south of the earthworks and ravine provide the opportunity to cluster development among significant tree stands along the future extension of Emerald Parkway. The City of Dublin acquired 19 acres of land in 2010, including portions of the Holder-Wright Earthworks and Wright's Run, and has prepared a master plan for a new city park in this location.

Bright Road and surrounding neighborhoods are characterized by modest, low-density residential homes in a rural setting that is typical of older homes constructed within the township prior to annexation. Residential developments within the area include Grandee Cliffs, Glenbrier and Kiplinger Estates. Wright's Run and its surrounding woodlands create a distinctive backdrop for the area, and Kiplinger Pond created by a spillway is located just east of MacBeth Drive. Future completion of Emerald Parkway will unite the area and provide major access for infill development along I-270. Planning efforts focus on the ability to maintain and protect neighborhoods in a balanced manner with future growth along Emerald Parkway.

Planning Challenges and Issues

Protect and buffer existing residential areas

Bright Road incorporates a very distinctive rural residential character that has been established over time by many factors such as housing stock, lot sizes, building setbacks and natural surroundings. All efforts should be made to maintain the quaint character of the area's neighborhoods, and road access for residents should be improved while discouraging through traffic.

Encourage greater open space and pedestrian connections

Due to the era in which development took place along Bright Road, the area lacks sidewalks and pedestrian infrastructure common to today's residential subdivisions. Every effort should be made to improve pedestrian connectivity and movement throughout the area, while sensitively considering the visual character and impacts of pedestrian infrastructure. Incorporation of additional open space near existing neighborhoods should be provided to facilitate pedestrian connections and park opportunities.

Improve traffic circulation, access and movement

The completion of Emerald Parkway from Wright's Run to Riverside Drive will be a significant milestone for Dublin. Providing relief to I-270, the parkway will provide full access from Tuttle Crossing to the south to Sawmill Road on

the north. Completion of this last phase will provide greater transportation network options east of the Scioto River, while facilitating a balance of greater access to area neighborhoods and an expected reduction in through traffic on local roads. Significant multi-jurisdictional efforts should be made to improve traffic and access management along the Sawmill Corridor to the benefit of area businesses and residents. With the completion of Emerald Parkway, Bright Road will be converted to a cul-de-sac at Riverside Drive to improve motorist safety at this problematic intersection. With a significant amount of office development planned along Emerald Parkway, future options for an overpass connection to the Bridge Street District south of I-270 should be explored to connect these important economic development areas while providing an alternative to Sawmill Road.

Preserve important archaeological and natural features

The Bright Road Area contains invaluable natural and man-made features for which every effort should be made to protect. The Holder-Wright Earthworks is an ancient man-made landform that has critical archaeological importance, and the adjacent Wright's Run ravine is a location of importance for its scenic and natural beauty. Nearby historic cemeteries that are poorly surveyed and studied also have importance to the area's heritage.

Establish a high quality, visible gateway into Dublin

Located adjacent to the Sawmill/I-270 interchange, the area is Dublin's major entry point from the northeast. Establishing high quality visible architecture, site planning and landscaping is important to represent Dublin's image and quality of life. Buildings fronting the interchange should be of a larger scale and establish an architectural statement that contrasts them from adjacent suburban retail and big box developments.

Use capital improvements as a catalyst for development

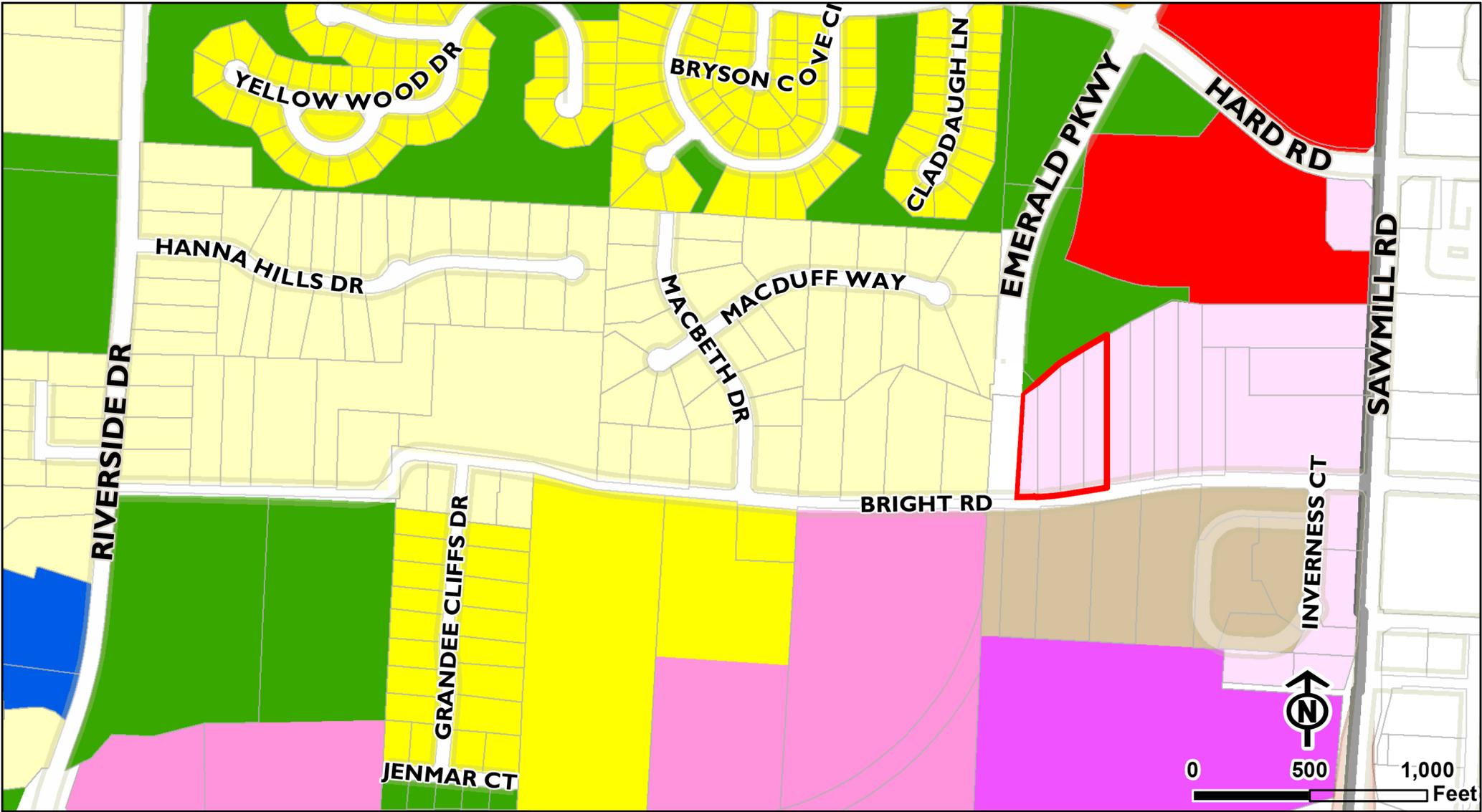
Implementation of major improvements within the area will require initiative on the part of both the public and private sectors. Planned capital improvements, including the final connection of Emerald Parkway and associated infrastructure should be encouraged to provide greater transportation connectivity, access and development potential along this key area of I-270 visibility.

Maintain expectations for appropriate, high quality development

As Dublin's premier business address, locations along the future Emerald Parkway extension should include high quality office development that respects the area's context. Higher profile offices should be preferred in areas where freeway and interchange visibility can be maximized, while appropriate scale and architectural style is provided near residential areas. Throughout the Bright Road Area retail is limited to service uses associated with office development that will reduce arterial trips by employees; integration of such support uses within the ground floors of offices is highly encouraged. Redevelopment proposals between Sawmill Road and Emerald Parkway should also be carefully considered to ensure that residential areas are fully integrated across Emerald Parkway and Bright Road.

Planning Goals

...To build upon and enhance the existing residential character of Bright Road between Riverside Drive and Emerald Parkway while ensuring the preservation of key natural features and historic sites. High quality office development should be encouraged along Emerald Parkway that focuses on quality architecture and site design that complements the surrounding natural environment and residential neighborhoods.



Legend

Future Land Use

LU_Future

- | | | |
|---|---|---|
|  Suburban/Rural Residential |  Mixed Residential Low Density |  General Commercial |
|  Suburban Residential Low Density |  Mixed Residential Medium Density |  Mixed Use Neighborhood Center |
|  Suburban Residential Medium Density |  Mixed Residential High Density |  Mixed Use Village Center |
|  Mixed Residential Rural Transition |  Neighborhood Office/Institutional |  Mixed Use Urban Core |
|  Flex Office/Research & Development |  Standard Office/Institutional |  Civic/Public Assembly |
| |  Premium Office/Institutional |  Parks/Open Space |
| | | |

Name * Donna Keidel

Email * dkphotographic@gmail.com

Phone Number (614) 793-1244

Subject * East Dublin

Comments *

I've lived on Bright Road for nearly 10 years. I knew of the plan to extend Emerald Parkway but how things are working out is very different from the way I perceived the original plan. The way it is now, I wonder if my home value will ever reach what I paid for my home. You're putting in more offices (and more traffic). I'm not okay with that. And now, a Park and Ride? That was NEVER in any of the plans. I DO NOT want a COTA bus stop for a neighbor. A parking lot will have a negative impact on my home's value.

I'm so disappointed in my city. In the last year, I've seen that Dublin cares more about money and development than it cares about its residents and their home values, at least on this side of the river. Stop being greedy. Treat our area as if you lived here. Would you want a COTA stop by your home? Would you want a huge office complex by your home? Would you want LOTS of apartments by your home? Please... Care about your residents, their happiness and their home values...

I would like my message to be sent to the following Council Members. *

- All City Council Members

transportation system centered on the Bridge Street development clear in its remarks on the city website (<http://dublinohiousa.gov/bridge-street-district-questions/qa-traffic/>): "The existing Park and Ride is in the area shown as part of the potential 'Bridge Park' mixed use development and realignment of Dale Drive, and any relocation of this facility should minimize service disruptions and should remain in close proximity of the existing Park and Ride."

2. These two Park-and-Rides could serve as hubs for the circulator bus system envisioned in the Dublin Community Plan. Dublin would run small circulator buses to transport workers to and from our business parks along Emerald Parkway and in Metro Center from the two Park-and-Rides and from our dense multifamily developments.

3. A COTA Park-and-Ride at Emerald and Bright would not serve Dublin or the residents of northwest Franklin and southern Delaware counties effectively. City Council defeated a similar COTA proposal years ago that would have drawn all commuter traffic from the northwest to a centralized facility on Sawmill Rd. We opposed that proposal unanimously because we believed COTA should develop a decentralized system that would pick up Columbus commuters on Smoky Row and Delaware County commuters in Powell, rather than force those commuters to cross or drive down Sawmill Road. Adding all those cars to one of the worst choke points in the county's transportation system—the Sawmill-Bright-Sawbury area—makes no sense whatsoever. And COTA plans to make the situation worse by closing its current facility on Smoky Row. The proposed Park-and-Ride certainly makes things easy for COTA—only one pick-up and drop-off area—but it will create problems for everyone else. < br />

4. As the Dublin Community Plan shows, Sawmill Road cannot be widened beyond 7 lanes. And at build-out, 7 lanes will not be wide enough to carry the traffic loads projected for Sawmill. Every intersection will fail badly. And the cities of Dublin and Columbus are now developing projects along Sawmill Road at far higher densities than those anticipated by the community plan traffic study. We need to think boldly about how to keep cars off of Sawmill Road and its collectors (Hard, Emerald, Summit View, etc.) if our business parks and

multifamily developments are to be successful.

5. The proposed COTA Park-and-Ride will make it virtually impossible for the City of Dublin to carry out the Bright Road area plan and fulfill its commitment to rebuilding the Bright Road neighborhood. The neighborhood has already lost a number of homes to Emerald Parkway. The Bright Road plan calls for a high-quality, owner-occupied condominium development at R-5 along Billingsley Creek north of Bright Road and east of Emerald Parkway. That development would maintain the residential character of the neighborhood, reverse at least some of the traffic load at rush hours, have the least impact on the creek and its adjacent landowners, and prevent the isolation of the Village at Inverness. The landowners in the area agreed to the plan because it was the highest use possible for that difficult piece of land and would preserve the more expensive land south of Bright Road for office development. The major landowner to the south of Bright Road was adamant that he did not want to build condos. The staff's plan to move the condo development to the south side of Bright Road west of the Village at Inverness could only happen if the city were to subsidize that development—something we doubt future councils would be willing to do. The proposal has also stranded two single family homes east of the Park-and-Ride on acreages too small to redevelop satisfactorily.

For these reasons, we believe it would be a mistake for the City of Dublin to proceed with its land swap agreement with COTA. The plan is bad for Dublin and will undermine our efforts to execute our Community Plan.

Thank you for your consideration.

Sincerely,

The Officers and Trustees of the EDCA

Randy Roth
Bob McKnight
Don Spangler
Joanne Crockett
Merlin Marshall
Scott Haring
Dave Underwood
Amy Kramb

1/29/2015

Jenny M. Rauch - Outlook Web App

John Leffler
Gerald Kosicki
Jennifer Brenneman

February 4, 2015

To the Members of the City of Dublin Planning and Zoning Commission

Re: COTA Park and Ride Relocation, 15-006Z/CU

My Name is Sharon Huber and my partner and I are 30 year residents of Dublin, specifically Bright Road. I am writing because I am unable to attend the meeting regarding this rezoning issue on Thursday due to a work schedule conflict, but I definitely want to share my concerns about this project.

I attended the informational meeting about this project a few weeks ago and was shocked to learn of the plan to put the park and ride in the midst of our neighborhood. Our neighborhood plan called for multi-family buildings that would mirror the Village of Inverness, giving some continuity to that end of the street, and complementing the few single residences that still remain in that area. By putting this parking lot at this location – that plan is destroyed, isolating the 2 single family homes on the North side of the street and causing even more traffic and congestion for the family who lives on the southeast quadrant of the roundabout.

My other concern is the additional traffic that this will create for an already busy intersection at Bright and Sawmill. This intersection backs up tremendously at rush hour now, and if this project is approved, the majority of the patrons of this lot will be coming and going at this busy time. We know that more development will occur along the Emerald Parkway corridor, and will add *even more* traffic at rush hour. Eventually the west end of Bright road will be changed in some way, either dead-ended or making it mandatory to turn north on Riverside Drive. The reality of the situation when this occurs, is that anyone west of the roundabout will find it next to impossible to get out and go south during rush hour without taking a long circuitous route to get there, or be forced to sit through numerous light changes to access Sawmill Road.

I don't fully understand what the benefit to the City of Dublin will be to put a Park and Ride in the middle of a residential neighborhood. There is nothing accessible by foot and no real retail opportunities in close proximity.

My hope is that the City and COTA will consider some other location for this Park and Ride, that could benefit both the City and its businesses, and not create traffic and congestion in a quiet residential neighborhood.

Thank you for your consideration of this matter.

Sincerely,

Sharon Huber

4345 Bright Rd

Dublin, OH

Comments on COTA and Bridge Street Development for the Record

Linda <micetro@wowway.com>

Thu 2/5/2015 7:25 AM

Inbox

To: Jenny M. Rauch <jrauch@dublin.oh.us>; Greg S. Peterson <gpeterson@dublin.oh.us>; Amy Salay <ASalay@dublin.oh.us>;

Cc: 'Scott' <sdharing@columbus.rr.com>; 'Steve Masonbrink' <smasonbrink@wowway.com>;

Planning and Zoning and Dublin City Council:

My husband and I live on the Lilly Mar Ct. We are some of the neighbors who will be affected by moving the COTA stop and by the Bridge St. corridor development.

I can't for the life of me understand why more retail is proposed when so much has failed on the east side of the river. I also can't understand where these young apartment dwellers will come from and why they would want to live there. Since we don't know what kind of offices will be built in the Bridge Street corridor that would attract young professionals, we can't be sure they will flock to live in this community. There will be no incorporated public transportation to get them to their jobs outside their community, so they will have to depend on their vehicles to get out of the complex (no easy feat). That defeats the purpose of this being a walkable urban space where people work and play.

This does not blend in with the rest of Dublin and does not blend into the riverscape which is the heart of Dublin. Frank Lloyd Wright would be appalled at the wasted opportunity to blend a community with the natural surroundings at appropriate density. It will be a wart in a very visible state route that calls attention to Dublin's big mistake. It will be known as the "Mistake on the Scioto."

We should not be an Easton wannabe. It will destroy our unique character. That is why people visit and live here. Why are we transforming this community into something the community does not want? Why is Dublin City Council trouncing on the plan the community put together? Why is council not representing us? What is the motivation? Why is there a double standard that scrutinizes resident's plans for minor changes to their property, and the design of signs, when council quickly approves waivers to a plan the community prepared. Is it because money talks and citizens take a back seat? And why, right after residents eloquently voiced their opposition to this development at the council special meeting, did council start talking about building materials? This was immaterial to the current issue before council and also reflects their lack of understanding or consideration of the very valid concerns raised that evening.

If council thinks we should "build it and they will come", then they should look at River Ridge and Dublin Village Center. That was not the case there. And how are we going to hide this wart once people and businesses do not come? Unlike a wart, we cannot treat this. It is not convertible to other types of space. It is permanent.

Moving the COTA bus stop will be an inconvenience for me and others in the community, including the planned community. Those already in our community depend on the Dale Rd. bus to get downtown where the jobs are. Planning a 170 space parking lot seems excessive for the current ridership. Are the extra spaces planned for the future apartment dwellers who are supposed to stay in their urban walkable community? And why doesn't the city

use property they already own for a bus stop, like where Wendy's used to be on Riverside Dr.? Or how about converting some of the empty retail space at River Ridge or Dublin Village Center to a COTA park and ride? Auto dealers are already using vast parking lots for storing their cars at Dublin Village Center.

I am also very concerned about the safety of pedestrians crossing a state route close to a roundabout which people don't know how to navigate. How many people will be run over and how much traffic will back up while people dodge pedestrians and bicyclists crossing from the Bridge Street development? Too much traffic and confusion is not what Dublin should seek. And what about the liability of Dublin for people run over crossing to the pedestrian bridge?

Storm water is also a critical consideration when locating such a vast amount of hardscape close to the river. Water quality and the rate of storm water return needs to be closely considered. I am not convinced this has been done.

Also, my husband and I are concerned about the backup of traffic on the south side of the roundabout at Martin Rd. This will make it impossible to get out of Martin Rd. into the stream of traffic on Riverside Dr. Will the city put in a traffic light on Martin Rd. to make this a meaningful exit point?

If council will not represent its citizens, we will remember this next time at the polls. that we want people on council who take its citizens concerns seriously.

Linda Masonbrink
3168 Lilly Mar Ct.
Dublin , OH 43017

PLANNING AND ZONING COMMISSION

RECORD OF ACTION

FEBRUARY 5, 2015

The Planning and Zoning Commission took the following action at this meeting:

6. COTA Park and Ride Relocation 15-006Z/CU Emerald Parkway & Bright Road Standard District Rezoning-Conditional Use

Proposal: A rezoning from R-1, Restricted Suburban Residential District to SO, Suburban Office and Institutional District in accordance with the Community Plan. This is also a proposal for the use of the site as a public Park and Ride. The site is at the northeast corner of the intersection of Emerald Parkway and Bright Road.

Request: Review and recommendation of approval to City Council of a standard district rezoning under the provisions of Zoning Code Section 153.234 and approval of a conditional use under the provisions of Zoning Code Section 153.236.

Applicant: City of Dublin
Planning Contact: Jennifer M. Rauch, AICP, Sr. Planner
Contact Information: (614) 410-4690, jrauch@dublin.oh.us

MOTION #1: Mr. Zimmerman moved, Ms. Salay seconded, to recommend approval to City Council of this rezoning from R-1, Restricted Suburban Residential District to SO, Suburban Office and Institutional District because it complies with the Community Plan.

VOTE: 6 - 0

RESULT: This Rezoning application will be forwarded to City Council with a recommendation of approval.

RECORDED VOTES:

Victoria Newell	Yes
Amy Salay	Yes
Chris Brown	Absent
Cathy De Rosa	Yes
Bob Miller	Yes
Deborah Mitchell	Yes
Todd Zimmerman	Yes



City of Dublin

Land Use and Long
Range Planning

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PLANNING AND ZONING COMMISSION

RECORD OF ACTION

FEBRUARY 5, 2015

**6. COTA Park and Ride Relocation
15-006Z/CU**

**Emerald Parkway & Bright Road
Standard District Rezoning-Conditional Use**

MOTION#2: Ms. Salay moved, Mr. Zimmerman seconded the motion, to table this conditional use.

VOTE: 6 – 0.

RESULT: This conditional use application was tabled.

RECORDED VOTES:

Victoria Newell	Yes
Amy Salay	Yes
Chris Brown	Absent
Cathy De Rosa	Yes
Bob Miller	Yes
Deborah Mitchell	Yes
Todd Zimmerman	Yes

DRAFT

STAFF CERTIFICATION

Jennifer M. Rauch, AICP, Senior Planner

PLANNING AND ZONING COMMISSION

MEETING MINUTES

FEBRUARY 5, 2015

6. COTA Park and Ride Relocation 15-006Z/CU

Emerald Parkway & Bright Road Standard District Rezoning - Conditional Use

Chair Newell said the following application is a request for a rezoning from R-1, Restricted Suburban Residential District to SO, Suburban Office and Institutional District in accordance with the Community Plan. She said this is also a proposal for the use of the site as a Park and Ride, which requires the review and approval of a conditional use. She noted the site is at the northeast corner of the intersection at Emerald Parkway and Bright Road. She said the Commission will forward their recommendation to City Council for the Rezoning and the Commission is the final authority on the conditional use.

Chair Newell swore in all those intending to speak on this application.

Jennifer Rauch introduced this application for relocation of the COTA Park and Ride with two parts of the application with the standard district rezoning, which is the request to change from R-1 Restricted Suburban Residential District to SO, Suburban Office and Institutional District. She said the review and analysis is based on the Community Plan and the designations called out as part of the plan. She said the second application is a conditional use application, which is required within the proposed SO District for park and ride facilities and the Code outlines specific requirements related to the shelter details and review criteria. Ms. Rauch said the applications will be reviewed separately and two separate motions will be required.

Ms. Rauch said the site is located at the northeast corner of the intersection of Bright Road and Emerald Parkway. She said the current zoning for this site and the areas to the south and west are R-1, and to the north and east are zoned PUD as part of the NE Quad Rezoning.

Ms. Rauch stated there was a public meeting held in January with COTA and City representatives, and the neighbors within the area regarding the proposal and the feedback provided from that meeting is in the packet.

Ms. Rauch said City Council is reviewing a separate action related to real estate and a development agreement. She stated that as part of those discussions concerns were raised related to the Community Plan and original of the Bright Road Area Plan. She said in 1997, the Community Plan Future Land Use designated this site as existing residential. She said when the City undertook the Community Plan update in 2005, they looked at all the future land use designations and area plans. She said through numerous joint work sessions and meetings with the neighbors the various area plans were developed including the Bright Road area, which originally had shown this site as a multiple-family designation. She said as part of City Council's final review of the Bright Road Area Plan in 2007, Council made a recommendation and voted to change the site to Neighborhood Office. She said those minutes were also included in the packet. She said this designation was retained in the most recent updates to the Community Plan in 2013.

Ms. Rauch said the Future Land Use Map designation is Neighborhood Office, which calls for density not to exceed 9,500-square-feet per acre. She said area plan recommends development with low lot coverages, increased setbacks, and the provision of a transition between the residential and the future office developments. She said the area to the east of the site is zoned for office and the area to west is

residential, making this site the corner piece to provide the transition between the uses. She said the most consistent zoning district with the Neighborhood Office designation is SO, Suburban Office District, which is the recommendation of this site.

Ms. Rauch said the Bright Road Area Plan recommends the preservation of the natural features, which would include substantial trees on the site as well as along the creek on the northern boundary. She said future development need to ensure that those features within the area are accounted for. She indicated the area plan calls for opportunities for improving traffic circulation. She said the completion of the final phase of Emerald Parkway, has helped open up and provide better access and traffic movement within the area.

Ms. Rauch said based on the standards of the standard zoning district review, Planning has determined the proposal meets the criteria based on the future land use designation and the specific recommendations of the area plan. She said the proposed zoning district is the most compatible district and provides for office and institutional uses in line with the Community Plan. She stated the recommendation for this site is a recommendation of approval to City Council.

Ms. Rauch said the second portion of this application is the conditional use review. She said under the Suburban Office standards conditional use approval is required for park and ride facilities.

Ms. Rauch noted the proposed site shows two access points; one is off Bright Road and the second is off Emerald Parkway. She said there is a bus lane for the buses to circulate on the site that is separate from the parking area. She said the bus circulation action and route traveling is handled on-site, which is different from the current location on Dale Drive where it is done on the street. She said the setback on this site is based on the width of the right-of-way, which in this area, has been increased significantly from the Thoroughfare Plan and the Community Plan.

Ms. Rauch indicated the proposal meets the parking setback lines but the building setback lines are encroached by the proposed shelter, which is one of the deviations requested as part of the proposal. She said based on the significant setback from the roadway and the proposed landscaping and mounding, Planning recommends the location for the shelter be permitted. She said the Code specifies the shelter be limited to 50-square-feet and the architecture of the shelter coordinates and is harmonious with the architecture of the surrounding area. She said Planning's analysis finds these two requirements to be met.

Ms. Rauch said the specific perimeter landscaping and interior landscaping meet required Code. She said there is a pond at the northern end of the site for stormwater retention and the creek runs along the northern boundary. She said the proposed pond and setback will not disturb the 100-year flood plain.

Ms. Rauch said the applicant is proposing a sign at the property line at the corner of the site. She said Code requires signs to be setback 8 feet from the right-of-way; however, due to utilities within the area that they are trying to avoid Planning recommends the sign be permitted within the proposed location. She indicated there will be lighting proposed on-site, which will meet the lighting requirements within the Code.

Mike Bradley, Vice President of Planning and Service Development, 5941 Hadler Drive, Dublin, Ohio, said a park and ride facility is preferred next to a main arterial and located north of I-270 with good access and visibility. He said COTA is looking to consolidate the park and ride facilities with the goal of increasing the number of trips at each park and ride for greater success. He said their consultant has recommended consolidating the park and rides and having more trips making it more convenient for the people by providing direct service to downtown destinations and operate on the freeway network. He said there is

an express fee, which is higher than local circulation, which comes with it an expectation that the trip is to be express.

Mr. Bradley said they surveyed the customers that currently use the Dale Drive Park and Ride and determined most of the riders are north of the current location. He said a park and ride is designed for commute trips and is weekday-service only. He said there will be 170 parking spaces on site with a passenger shelter and a separate bus lane. He said COTA will start off with six trips in the am that generally operate between approximately 6:00 am to 8:00 am. He said COTA will run three trips down Riverside Drive to Griggs Dam and three trips on I-270 to SR315. He noted operation for pm would be approximately between 3:45 pm – 5:15 pm.

Mr. Bradley said COTA has 29 Park and Rides and not a single incident has been reported record. He said the majority of the Park and Rides have security cameras and the noise is reasonable. He said lighting is directed down and light/shadowing does not go outside of their property. He said COTA has no trash problems to note. He reported this is COTA's second highest Park and Ride.

Ms. Rauch said based on this information and the analysis completed, approval is recommended for conditional use as the criteria has been met with the two deviations related to the location of the proposed sign and shelter.

The Chair invited public comment.

Gerry Kosicki, 4313 Wyandotte Woods Blvd., said he understands building a city is complicated and if Dublin is going to be successful in the long run the Bridge Street District needs to be about inventing a city and anticipating all the needs of a dense urban area including transit, safety services, environmental, and economic sustainability. He said the COTA relocation project provides an opportunity to rethink the future of transit needs and options. He said if BSD is going to be based on dense, urban walkability, then it should have priority to future transit needs and space should be set aside for this; the city needs can be addressed systematically. He said Dublin cannot rely on COTA to anticipate future transit needs as BSD is built out over the coming years. He indicated COTA has no credible plans for light rail and what they have proposed in the past has been inadequate. He said the area suffers from the lack of such plans. He said mass transit guides future development and infrastructure has a way of channeling density into areas that can be meaningfully served by mass transit. He indicated the Park and Ride relocation plans on Bright Road seem to be business as usual for COTA by replacing one Park and Ride with another to haul some people downtown and back. He said this will not meet the future transit needs of BSD and the City as a whole. He requested a vision for how a new location for the Park and Ride can best facilitate future development of transit options within BSD and between BSD and other parts of the City as well as the surrounding areas. He urged the PZC to carefully consider both the merits and design of this site as well as how this fits into the larger issue of future transit.

Amy Krumb, 7511 Riverside Drive, said she was representing the East Dublin Civic Association. She reported she attended both of COTA's meetings in January. She said this proposal would be a win for COTA at this location. She said the Smokey Row neighbors are extremely upset about this location because they would lose a bus route. She indicated this site was not the best for the City of Dublin. She pointed out that the future land designation and the area plan state this should be office. She said one day it may be acceptable to rezone this parcel as Suburban Office but premature to rezone it tonight based on this application. She said Emerald Parkway is lined with beautiful corporate headquarters. She questioned why the City is asking to place a parking lot on this prominent intersection on this new signature roadway that recently just opened as a gateway from Columbus to the City of Dublin and the first parcel being developed along Emerald Phase 8. She said this is suburban office. She said in a work session in 2007, one of the former city staff members, Mr. Combs said that this plan is intended to preserve the key natural features and to maintain the residential character along Bright Road. She said

the plan also continues the high quality design in corporation of offices along Emerald Parkway. She read where Mr. Combs said the concepts give the general expectation for future development with buildings closer to the street, internal parking lots, appropriate landscaping and buffer zones. She said that vision that Staff said was going to be in this area is in nothing like what is being presented tonight. She reiterated that this parcel should not be rezoned tonight.

Ms. Krumb said this is the wrong location for a Park and Ride with regard to the conditional use request. She agreed with the prior speaker-resident that BSD was a much better location. She reported 2013 Census data that showed the City has 21,338 Dublin residents over the age of 16 working in the City. She reported 8,248 of these residents drive alone to work. She said only 74 reported riding transit to work. She referred to COTA's point of origin survey that showed where people come from to ride their services. She said there were 43 riders by adding up the little dots on the survey originating in that area, which extended up to Union County, Powell, Delaware, over to Smokey Row and Columbus. She said there were just 23 dots in the City of Dublin and only 5 of those dots were on the east side of the river. She said if we are looking at this proposal from the City of Dublin's perspective, and their residents, excluding COTA's demographics, we are looking at building a parking lot on land that was \$1.2 million. She said there are more than 25 people present tonight that are opposed to this Park and Ride going to the proposed location. She said the City is in a hurry to acquire this land because it is needed for the Bridge Park District. She said there are other mechanisms for the City to acquire the land. She said the City relocated Spa at River Ridge and they can do that with COTA. She summarized this does not have to happen now and does not have to happen at this location.

Ms. Krumb said vehicular circulation will interfere with the existing circulation around there. She said Planning said it is not going to interfere at all. She said she contests that because Bright Road is not sufficient to handle those trips or those buses at Bright and Sawmill Road. She said that is a horrible intersection at rush hour, which is the exact time these buses will be going through there. She explained that intersection backs up past Inverness every morning and every night as it is and now buses are proposed to be added to the congestion. She said Engineering has repeatedly said Bright Road would be widened to alleviate traffic at this intersection and that when Emerald Parkway went in, there would be less traffic on Bright Road. She said the City is now proposing to allow additional traffic onto Bright Road when the City said they were going to take it off by using Emerald Parkway. She said we will get additional traffic from Smokey Row when their route has been closed down.

Ms. Krumb said this application impedes the development of the area and is harmful economically. She said there will be two residences stuck there between the existing offices to the east if a parking lot is constructed on that site.

Ms. Krumb indicated we should take pride in this corner of the intersection and build something worthwhile on this valuable parcel at this corner. She said the Planning Report states this Park and Ride is going to be an amenity but it is just an amenity for COTA, not for the City of Dublin.

Ms. Krumb said even if people are drawn from Delaware, Powell, and Columbus, there is nothing to keep the riders here. She said they will come, add congestion to our roads, and then will leave the area. She said if the Park and Ride was down in the BSD and riders were dropped off the bus after work, they might grab some dinner at the new restaurant, have a drink at the new bar with a happy hour, hit the gym, or use any number of amenities they could walk to before heading home, spending money in our City.

Ms. Krumb concluded she hopes the PZC votes no to the rezoning and conditional use tonight. She said if the conditional use is approved, there should be a condition added, which is to require COTA to restrict all buses from using Bright Road. She said it would be appropriate for the buses to enter on the south entrance off of Bright Road but always exit north on Emerald Parkway, using the Emerald Parkway and

Hard Road intersection. She said COTA is getting everything they want with this application and the residents are not getting anything.

Randy Roth, 6987 Grandee Cliffs Drive, said he is president of East Dublin Civic Association.

Mr. Roth said he just learned about this application at the end of December. He said whether Council is going to vote against this or not, the residents are being heard and taken very seriously. He said he is concerned about stream buffer locations and the natural habitat. He indicated the landscaping trees appear to grow right at the edge of the creek. He said 20 feet at the top of the bank should be natural to retain the habitat and the key is the top of the bank. He said we like our coyotes and had them shifted over from Brandon. He said not only should the traffic be diverted from the Sawmill/Bright intersection but consider a way to make it natural.

Mr. Roth said he serves on the Community Plan Steering Committee and served on the Transportation Task Force. He suggested there should be two centers of Park and Rides; one in the BSD and one on Perimeter in the commercial area. He said we could have our own circulator system of buses and suggested working with COTA. He said once you come here, you are far from our Metro Center and the hospital where the jobs are. He read from the website that states "The existing Park and Ride on Dale Drive is in the area that shows potential BSD mixed development and realignment with Dale Drive and any relocation of this facility should minimize service disruptions and should remain in close proximity to the existing Park and Ride." He reported persons with disabilities live between the interstate and along SR161; persons 65 years old and older are in that same parcel. He reported persons in households without a vehicle are in the same area. He said he spoke with some COTA riders and they do not own cars. He said all of this new demographic data really fits our original vision but that is where COTA needs to be to help us. He suggested we take time to consider options and plan this out for an ultimate transportation solution.

Mr. Roth said the City of Dublin voted down a request from COTA to locate near the interchange on the north side. He said every intersection on Sawmill Road by 2030 will have seven lanes. He said we need a decentralized system to pick up Columbus people in Columbus, Powell people in Powell and try to keep them off of Sawmill Road. He said with this plan, COTA will forget about the people of Powell, close the Park and Ride on Smokey Row, and draw all traffic to the jump point. He said we already know all these intersections are going to fail. He said there will not be a Park and Ride between Sawmill Road and US23.

Mr. Roth referred to the Community Plan for Bright Road. He said if this plan is defeated he wants to flip back to the plan they all support. He said this land should be used for multi-family and put the office on the more barren land to the south.

Don Spangler, 3614 Jenmar Court, said there does not seem to be a lot of riders to justify the need. He said the long-time residents of Dublin did not expect to see a parking lot as the first thing constructed on the new section of Emerald Parkway. He said they are very disappointed. He believes there probably is not anyone on City Council that desires to have a Park and Ride in their neighborhood. He said if this is an amenity as described, sitting in a residential area, why it was not an amenity sitting in the BSD where there were a lot more people to use it. He said if the bus would stop where there were restrooms, activities, entertainment, or shops revenue could be made. He said the Park and Ride appears to be a loser as it does not generate revenue and it takes up space. He suggested that if the Park and Ride were located by Chase Bank by Kroger Marketplace on Sawmill Road there is open space and shopping areas besides the grocer and bank. He reiterated at Bright Road and Emerald Parkway, there is nothing. He said people will drive in, get on the bus, and when they return they will get back into their cars and Dublin will never make any money off of them. He said if this is an amenity, we need to rethink how we look at amenities. He concluded this only seems to be an amenity for approximately 50 people and does

not see how this Park and Ride fits the criteria for businesses, entertainment, opportunities, parks and recreational facilities that benefit and protect the majority of Dublin residents.

Scott Haring, 3280 Lilly-Mar Court, said he understands the City owns this parcel and the City's purchase of this parcel was to facilitate a little bit of the frontage and west edge to make this new roundabout. He said he read where this parcel was referred to as over three acres of access land. He asked to clarify that the PZC was being asked to rezone the parcel from R-1 (one house per acre) to Suburban Office and then once that is in hand for the parcel to be used as a parking facility as a conditional use. He stated he did not believe this was the right place.

Mr. Haring said he heard the applicant say they wanted good visibility but he also heard there would be mounding around this so it would be hidden from the street. He added being a block back, west of Sawmill Road, does not sound visible. He said other speakers have noted more recognizable commercial areas where this Park and Ride could be located. He said this proposal reminds him of another facility that is west of Sawmill Road with mounding, which is Dublin Village Center. He recalls hearing years ago that mounding and lack of signage killed Dublin Village Center so he is surprised to hear that these are some of the goals here tonight.

Mr. Haring said he attended the recent City Council meeting that precipitates all this for a new road that is going to bisect the current Park and Ride facility. He said he still does not understand the mechanics that the City could buy the right-of-way on that parcel but it sounds like the preference is to purchase the entire parcel. He said then the City will go back to having two small slivers of excess land. He said it is not clear what happens to that excess land if Dublin does this. He indicated we are a heck of a city to say to COTA you have a Park facility, we would like a sliver of your land for a new road, let us build you a new facility for \$1 million. He said he understands there is supposed to be some land trading and some value but as he had mentioned to City Council 10 days ago, there is another parcel near a roundabout in the City where a little portion of that will be for the future SR161/Riverside Drive Roundabout. He said he understands the City also owns the former Wendy's restaurant lot. He suggested that would be a great place; ±two acres will be taken for the roundabout but it would be a much more 'like for like' and it would be closer to BSD. He said earlier it was stated that the previous goal was to keep it near the BSD and Wendy's lot would meet that requirement. He said there is a line on the map showing a bus route down Riverside Drive and this piece is right next to Riverside Drive. He said he had heard repeatedly from PZC over the years a phrase "the highest and best use for property". He said he went to the party at Emerald Parkway for its opening of the final phase. He was told there were a few more parcels and hopefully big office to come and this parking lot does not seem to fit in the whole spirit of it.

Mr. Haring concluded by stating he hoped the PZC would table this application and consider other ideas or say no; this is not good use.

Robert Cudd, 4281 McDuff Place, said the creek that runs alongside this parking lot, actually runs along the residential area in his back yard. He said he often pulls debris out of that creek, like whenever there is a storm; the stream runs pretty quickly. He said if this lot is fully utilized it will have approximately 44,000 cars parking in it during the year. He said he is concerned about radiator needs, litter, and all the other things that blow into the stream, which feeds right into the Scioto River. He asked the PZC to consider the elements that could go into the stream including the sealants that will be applied to the parking lot. He indicated this is bad for wildlife such as deer, rabbits, and squirrels that are there. He summarized this is a bad idea of putting a parking lot with that kind of capacity right on a stream that feeds into the Scioto River.

The Chair asked if there were any further public comments to be made. [Hearing none.] She closed off the public comment portion of the meeting and invited questions or comments from the Commissioners.

Amy Salay remarked on the phrase “highest and best use”. She said that is a development term and it has to be used very carefully because a lot of times a developer looks at a piece of land very differently than we do in Dublin in terms of maximizing what you can get out of a piece of ground. She said she was unsure that they ever wish for “highest and best use” in Dublin as that is a dangerous term.

Ms. Salay asked Staff about stormwater. She asked if pervious paving was considered for the parking lot so there would not be runoff. She admitted she did not know the price comparison from one to the other. She asked if maybe the part that is not going to be used all the time could be pervious. She asked if that question could be answered before this proposal goes to Council.

Ms. Salay said she had a couple of questions for Mr. Bradley of COTA. She said she had spoken to a few people from Smokey Row that attended the COTA meeting and they did not know that they would necessarily lose their park and ride but the bus service might be decreased. She asked him if he could answer that question.

Mr. Bradley said COTA was proposing that but it was not final yet to combine Route 30 with this proposed location. He said the consultants for the transit system review first recommended eliminating it completely. He said the reason COTA left it in was because it was a little bit further from Dale Drive. He said COTA had made a statement if a park and ride is established in the Sawmill corridor they would consider combining the routes. He said they do see the people from Smokey Row using the Dale Drive Park and Ride. He said the watershed for the Park and Ride is pretty large. He said in short, we will not make that decision until the end of May. He said during the transit system review, they considered a lot of changes redesigning the network.

Ms. Salay said what the Smokey Row residents heard, or maybe it was wishful thinking, was that there may be a bus or two removed but that there would still be a facility. Mr. Bradley said that was the residents’ suggestion, not COTA’s.

Ms. Salay said we have heard a lot of suggestions about keeping a park and ride facility in the BSD. She asked Mr. Bradley how he sees the COTA service within the Bridge Street District working in tandem with park and ride facilities. She said she knows he wants one somewhere on the west side of Dublin in the Perimeter/Avery area. She asked how he sees COTA serving Dublin in the future or would it be something that Dublin would invent themselves.

Mr. Bradley said the long range transit plan was done around 2011. He said even with the Dale Drive location and without the proposed Bridge Street District, COTA was considering a park and ride in Sawmill Corridor as those are the growing corridors. He said in the early 1990s, Dublin was not as extensive and dense to the north and west. He explained the key to a park and ride is capturing people before they get to the highway. He said if they go beyond the freeway they do not want to back up for the most part. He said we have to change with the community. He said COTA is proposing local service on SR161 coming from Sawmill Road over to the Metro Place by 2017. He said the denser an area, the more people will use their service. He said he does not expect the large numbers from the BSD. He said it takes a larger watershed in order to be effective on a Park and Ride.

Victoria Newell asked Engineering about the circulation with the buses. She thought the buses were going to function at the intersection at Bright Road.

Tina Wawszkiewicz said the site layout shows the Emerald Parkway access as a right in/right out only because there is a median. She said the applicant is proposing to include a left turn lane on Bright Road to get into the site and the length calculated for that left turn lane is only a 50-foot stacking lane. She said Engineering has been working with them to increase that to 125 feet. She said from a traffic perspective a park and ride is good for the transportation system by consolidating trips. She said Engineering wants to see how things go with Emerald Parkway as traffic patterns have not fully been

established there. She said they still believe that the completion of Emerald Parkway will take some burden off of Bright Road as those patterns develop. She said Bright Road will continue to be evaluated, but Engineering is aware there is congestion.

Ms. Newell asked if there was a formal traffic study completed for this project. Ms. Wawzkiewicz said a traffic study was submitted and reviewed by Engineering. She said the details are being finalized and will be completed during the site planning process.

Ms. Newell said that was not included in the packets. Ms. Rauch said the planning report included an overview of the traffic study.

Ms. Salay asked Ms. Wawzkiewicz about a timeframe for improvements on Bright Road. She stated the Community Plan discusses the widening to Bright Road between Emerald Parkway and Sawmill Road to four lanes.

Ms. Wawzkiewicz said it is not programmed at this time and reiterated Engineering wants to understand the traffic patterns of Emerald Parkway before any improvements are made.

Paul Hammersmith agreed with Ms. Wawzkiewicz that traffic patterns have to be established with the opening of Emerald Parkway. He recalled what was said during the update of the Community Plan that they were very uncertain as to what Bright Road needed to be when it grew up and what would happen to the network. He said Engineering would start taking counts later this year to understand these patterns. He reported the City of Columbus is considering a southbound lane addition to Sawmill Road, which will include the Sawmill/Bright intersection. He said working from a systemic standpoint we need to work together with Columbus not only to improve Bright Road but also the intersection of Bright/Sawmill. He said Bright Road could be widened to eight lanes wide but if the capacity does not exist at the intersection it does not matter how wide Bright Road is between Emerald and Sawmill. He explained the controlling factors are always going to be the intersection and again that is the City of Columbus' jurisdiction.

Ms. Salay asked about the timing of the cul-de-sac at Bright Road and Riverside Drive. Mr. Hammersmith said Engineering has not determined that yet. He said it will be discussed during the next CIP update. He said there will be some land acquisition required.

Cathy De Rosa asked about the traffic flow. She said the traffic study is completed and Engineering is evaluating what will happen now that the intersection is open. She asked what the anticipated change is in that demand. Ms. Wawzkiewicz said Engineering's expectation would be for people to gravitate towards Emerald Parkway. She said there is no question that there will still be a delay on Bright Road at Sawmill Road.

Ms. De Rosa asked if Engineering was starting to see that happen or if it was too early to tell. Ms. Wawzkiewicz said there have not been any formal counts as it would not help during the change in the traffic pattern.

Deborah Mitchell asked for clarification about the results of the traffic study. Ms. Wawzkiewicz said the study provided for this site is directly related to the two access points that are proposed and the impacts on the roadways.

Ms. Mitchell confirmed Engineering has completed the review of the traffic study, but it was not included in the packet for this meeting. Claudia Husak said Engineering has conducted the analysis of the traffic study and the numbers were provided in the Planning Report. She said detailed traffic studies are not provided to the Commission for review, because those are under the purview of Engineering.

Ms. Mitchell confirmed the conclusion drawn by Engineering an extreme traffic problem is not anticipated. Ms. Wawzkiewicz said the use outlined in Community Plan as an office would be more intense use and generate more trips than the proposed park and ride.

Ms. De Rosa asked if any additional properties were forecast to be rezoned in the near future to align with the Community Plan designation. Ms. Rauch said no additional properties were being considered at this point.

Todd Zimmerman asked if any other locations were considered for the park and ride or if this was the primary targeted area. Ms. Rauch said this is the site we were presented to consider for this particular use.

Ms. Salay said the City needs to consider COTA's request to be located north of I-270. She indicated with the Bridge Street District becoming a reality the City needed to relocate some businesses, which includes the park and ride. She said Council's goal was to determine how to make that happen with COTA as a partner with the City. She said the City owns this land and it was considered to be an option for the relocation COTA. She indicated the use works from a traffic standpoint and that is how the proposal turned in an application.

Mr. Langworthy said the Commission needs to evaluate this site and this use on this site and not focus on where it might be better located. He said ultimately, the site location is up to COTA to determine where they think the best location is and the Commission's task is to evaluate this proposal on this particular site.

Mr. Zimmerman said Dublin will give ownership over to COTA. Ms. Salay confirmed that is what is envisioned.

Mr. Zimmerman said COTA will be responsible for the maintenance of the facility. Ms. Rauch agreed.

Mr. Miller asked if there were an options to keep the buses off Bright Road and move the buses across Emerald Parkway to Hard Road.

Mr. Bradley said it would add operational costs for every day they serve this site and there are no restrictions at this time. He said the routes are done very efficiently and not being able to get through on Bright Road would cause a run around every day at 16 times at \$70.00 per hour. He said it adds up and the cost to deliver this service to Dublin is passed on to the passengers, who only pay about 20 percent of the total costs.

Ms. Salay asked if COTA was talking about four trips down Bright Road and two trips down Emerald Parkway. Mr. Bradley said COTA is not sure at this time. He said to provide the best service would be to travel on Sawmill Rod to I-270 and travel the freeway downtown.

Ms. Wawzkiewicz said from Engineering's perspective, if this were an office use as it was envisioned in the Community Plan, those trips would not be restricted to any particular route. She said rerouting this particular use, even if those trips went up to Hard Road and came south on Sawmill, they are still using the same intersection, which would be the same level of delay.

Ms. Newell said the retention basin is 11 feet deep and not a very attractive shape as a triangle and extremely close to the creek. She said the suggestion about pervious pavers or underground storage could contribute to reducing the size of the pond. She said this would add a benefit to the site. She referred back to the tree survey noting a good grouping of trees pretty close along the property line. She

said if the retention pond is reduced through underground storage there may be an opportunity to reduce a row of parking and extended the green space to the north. She indicated COTA might be able to hold the front parking a little bit farther off of Emerald Parkway and save a few more of those trees that are in that area. She said the plan can be improved and is still bothered with the access along Bright Road. She expressed concerns for the residences across the street and the traffic being too great.

Ms. De Rosa said she had driven around and found the intersection at Bright and Sawmill to be really hard to navigate. She said she was not sure if rerouting solves all the problems because congestion still ends up back on Sawmill Road. She asked if there was any opportunity as far as timing here to think about some ways to advance what could be done at Sawmill and Bright Road. She asked if that was totally out of our hands and if it was a broader conversation with the City. She said waiting until 2018 or 2019 to solve that problem seems impractical.

Mr. Hammersmith said it is going to be a long study process; there are no cheap solutions and again it has to be a systemic approach. He said not only at Bright and Sawmill Roads but they are looking at Billingsley. He said the study will look at the entire corridor and not just one location, and it is not going to be an immediate solution. He said there will need to be funding sources identified. He said in the end, this is going to be a project between \$10 million – \$15 million to implement a correction. He explained this is being driven by the City of Columbus. He said he would report back to City Council as alternatives come forward but it is not going to be something that this project is going to solve.

Ms. Salay thought a decision was made but it looked like prior to that there was a lot of discussion about the Bright Road plan. She said we decided on the alignment of Emerald Parkway, 20 some years ago. She said the properties that are adjacent to the park and ride as you go eastbound toward Sawmill Road, are all in single ownership and being sold for redevelopment. She suggested the neighbors sit down with Staff, PZC, and Council to discuss the Community Plan and possible land uses west of Emerald Parkway.

Ms. Salay agreed with Ms. Newell about holding stormwater underground.

Ms. Newell said Suburban Office is the appropriate rezoning for this site. She said she takes exception to the conditional use.

Ms. Salay addressed stream protection and invited Mr. Roth to speak.

Mr. Roth said it would be nice to have natural woodland for about 20 feet; whole preservation would require more than that.

Ms. Newell said it can be two working together and does not have to be one or the other. She said the design of the retention basin on this plan is poorly functional and has no aesthetic redeeming qualities whatsoever. She said by doing a portion of piping underground and splitting the depth the site design would be improved. She said Engineering can speak to how to best balance the retention. She indicated there is a better aesthetic solution than what we were presented with this evening.

Ms. Newell said she was not in favor of the current plan conditional use. She said it fails to be harmonious to the existing intended character of the vicinity. She said she is comfortable with the rezoning of Suburban Office as it meets the Community Plan. She said there is an option to table this case and return with a revised plans that addresses the Commission's concerns or the Commission can vote on the application as presented.

Ms. Rauch said the City is the applicant for this project. She suggested if the Commission was inclined to vote on the rezoning tonight that portion of the application could be forwarded onto Council. She said Planning could work through the details and comments with regards to the conditional use and come back with a revised plan.

Motion and Vote

Mr. Zimmerman moved, Ms. Salay seconded, to recommend approval to City Council of this rezoning from R-1, Restricted Suburban Residential District to SO, Suburban Office and Institutional District because it complies with the Community Plan. The vote was as follows: Ms. De Rosa, yes; Ms. Mitchell, yes; Ms. Newell, yes; Mr. Miller, yes; Ms. Salay, yes; and Mr. Zimmerman, yes. (Approved 6 – 0)

Motion and Vote

Ms. Salay moved, Mr. Zimmerman seconded to table this conditional use. The vote was as follows: Ms. Mitchell, yes; Mr. Miller, yes; Ms. Newell, yes; Ms. De Rosa, yes; Mr. Zimmerman, yes; and Ms. Salay, yes. (Approved 6 – 0)



City of Dublin Planning and Zoning Commission

Planning Report

Thursday, February 5, 2015

COTA Park and Ride Relocation Emerald Parkway and Bright Road

Case Summary

Agenda Item	6
Case Number	15-006Z/CU
Site Location	4030, 4000, 3960 Bright Road Northeast corner of the intersection Emerald Parkway and Bright Road.
Proposal	A <u>rezoning</u> from R-1, Restricted Suburban Residential District to SO, Suburban Office and Institutional District in accordance with the Community Plan. A <u>conditional use</u> to permit the use of the site as a public park and ride.
Request	Review and recommendation of approval to City Council of a rezoning and review and approval of a conditional use under the provisions of Zoning Code Sections 153.234 and 153.236.
Owner	City of Dublin. Dana McDaniel, City Manager
Case Manager	Jennifer M. Rauch, AICP, Senior Planner (614) 410-4690 jrauch@dublin.oh.us
Planning Recommendation	<u>Recommendation of Approval of Rezoning</u> Based on Planning's analysis, the proposed modification to the Zoning Map to rezone from R-1, Restricted Suburban Residential District to SO, Suburban Office and Institutional District meets the Community Plan. A recommendation of approval to City Council is recommended. <u>Approval of Conditional Use</u> Based on Planning's analysis, this proposal complies with all applicable review criteria. Approval is recommended with no conditions.



 <p>City of Dublin</p>	<p>15-006Z/CU Rezoning / Conditional Use COTA Park & Ride 4000 Bright Road</p>	<p>0 150 300 Feet</p> 
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Facts	
Site Area	4.9 acres, 3.3 acres affected
Zoning	R-1, Restricted Suburban Residential District
Surrounding Zoning	West and South: R-1, Restricted Suburban Residential District East and North: PUD, Planned Unit Development - NE Quad
Site Features	The site is currently undeveloped and has frontage on both Emerald Parkway and Bright Road. Several single-family homes were demolished on the parcels, as the sites were used for construction staging for the completion of the last section of Emerald Parkway and the new roundabout with Bright Road. Billingsley Creek and the associated floodplain are located along the northern property line. Existing vegetation is located in the northern portion of the site along the creek and along the eastern property line.
Neighborhood Contact	City and COTA representatives held a neighborhood meeting on January 7, 2015 to introduce the proposed COTA park and ride relocation. A summary of the comments and discussion have been included in the packet.
Ordinance 04-15	<p>City Council introduced Ordinance 04-15 on January 5, 2015, regarding the proposed development agreement and real estate exchange for the relocation of the COTA park and ride. The ordinance was tabled at the second reading on January 26, 2015 to await the outcome and review of the Planning and Zoning Commission for the rezoning and conditional use proposals. Council stated the importance of allowing the Planning and Zoning Commission review process to take place prior to making a final decision on the development agreement and real estate exchange.</p> <p>During the hearing, a number of residents outlined their concerns regarding the proposed park and ride at the Bright Road location. These concerns included the approval of the existing Bright Road Area Plan, impacts to the surrounding area, timing of the project, and neighborhood involvement.</p> <p>City Council requested additional information be provided, which include the Bright Road Area Plan history, proposed COTA bus routes and times, and COTA rider location information. This information has been provided in the packet for your reference. Additional information from COTA may be provided prior to the meeting.</p>

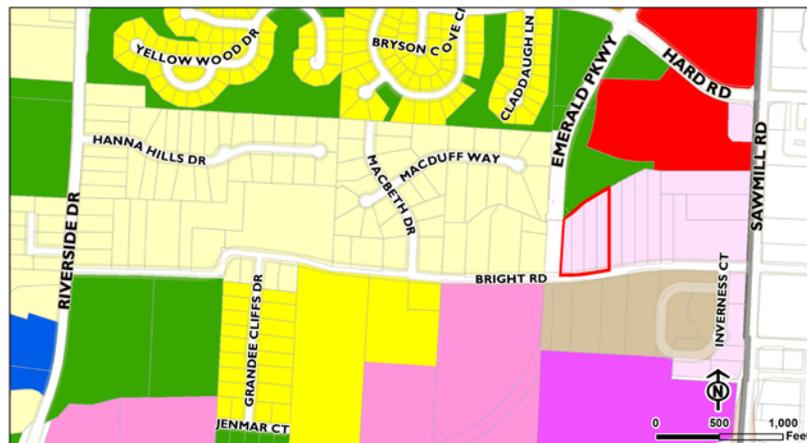
Details	Standard District Rezoning
Process	Code Section 153.232(B)(9) provides the Planning and Zoning Commission with "other powers and duties" which includes making recommendations to City Council for amendments to the Zoning Map, which is the case in any rezoning. The Commission should review the proposal, provide input, and vote on the zoning change. The proposed amendment will be forwarded to City Council for its consideration. The following sections summarize the major components of the proposed Zoning District.

Details **Standard District Rezoning**

Plan Overview	The proposal is for a rezoning from R-1, Restricted Suburban Residential District to SO, Suburban Office and Institutional District. The development standards of the district are outlined below. The conditional use review for the proposed park and ride is outlined and reviewed separately.
153.026 (A) Permitted Uses	Permitted uses in the district are administrative and business offices, professional offices, institutions, organizations and associations, and child and adult daycare centers.
153.026 (B) Conditional Uses	Conditional uses include such uses as auto-oriented commercial facilities or outdoor service facilities, educational and research, restaurants, exceptional uses, fitness uses, and public park and rides.
153.026 (C) Development Standards	Code does not require a minimum lot size or width. Required side and rear yard setbacks are 15 feet.

Community Plan
Future Land Use

The Future Land Use Map designates this site as Neighborhood Office/Institutional (less than 9,500 SF per acre). This would permit, for example and depending on site design, up to 46,550 square feet of office development for the site, which would require between 187 and 233 parking spaces, depending on the type of office use. The Plan describes this land use classification for sites located adjacent to residential areas where land use transitions or buffers are necessary. Development intensity is limited with low lot coverages, greater setbacks from non-residential uses and extensive landscaping. The equivalent zone district to this classification is SO, Suburban Office and Institutional District.



Legend

Future Land Use	Mixed Residential Low Density	General Commercial
LU_Future	Mixed Residential Medium Density	Mixed Use Neighborhood Center
Suburban/Rural Residential	Mixed Residential High Density	Mixed Use Village Center
Suburban Residential Low Density	Neighborhood Office/Institutional	Mixed Use Urban Core
Suburban Residential Medium Density	Standard Office/Institutional	Civic/Public Assembly
Mixed Residential Rural Transition	Premium Office/Institutional	Parks/Open Space
	Flex Office/Research & Development	

Community Plan
Bright Rd. Area Plan

This site is included in the Bright Road Area Plan. The main goal of the Area Plan is to build upon and enhance the existing residential character of Bright Road between Riverside Drive and Emerald Parkway while ensuring the preservation of key natural features and historic sites. High quality office development should be

Details	Standard District Rezoning
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	<p>encouraged along Emerald Parkway that focuses on quality architecture and site design that complements the surrounding natural environment and residential neighborhoods. The Area Plan also calls for improved traffic circulation, access and movement and the use of capital improvements as a catalyst for development. The completion of Emerald Parkway provides greater transportation network options to the area. The Area Plan also identifies the preservation of important archaeological and natural features. A significant number of trees and a portion of the Billingsley Creek are located in the northern portion of the site and will need to be preserved to the extent possible.</p>
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Analysis	Standard District Rezoning
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Process	The Zoning Code requires the Planning and Zoning Commission to determine whether the proposed rezoning will generally conform to the Dublin Community Plan and other applicable area plans, integrates in an appropriate and compatible manner with surrounding land uses, and generally adheres to other accepted planning policies and practices. After recommendation by the Planning and Zoning Commission, the rezoning application will be forwarded to City Council for public hearing and final vote. This analysis is separate from any consideration of a specific use.
1) Evaluation based on the Future Land Use designation	Future Land Use met: The proposed rezoning meets the definition of the Future Land Use map designation of Neighborhood Office/Institutional. The SO - Suburban Office and Institutional District is the most compatible zoning district and provides for office and institutional uses.
2) Evaluation based on Area Plan	Area Plan met: The Plan graphic indicates the incorporation of neighborhood office uses on the proposed site, which is compatible with the proposed SO zoning district.

Recommendation	Approval
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Approval	The proposed modification to the Zoning Map to rezone from R-1, Restricted Suburban Residential District to SO, Suburban Office and Institutional District meets the Community Plan. Planning recommends the Planning and Zoning Commission recommend approval of this rezoning to City Council.
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Details	Conditional Use
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Plan Overview	The proposal includes a COTA park and ride with 169 parking spaces, a 48-square-foot bus shelter, and associated site improvements.
Site Layout	The proposed COTA park and ride relocation is located at the northeast corner of the Emerald Parkway and Bright Road roundabout. The site includes two access points on Emerald Parkway and Bright Road, respectively. The internal site circulation provides a separated bus lane for passenger drop-off and pick-up that runs parallel to the Emerald Parkway and Bright Road frontages. The 169 parking spaces are located to the north and east of the bus lane. A proposed stormwater management pond is located at the northeastern corner of the site, adjacent to the creek.

Details	Conditional Use												
Setbacks	<p>The front yard setbacks for buildings are based on the width of the rights-of-way and measured from the center line of the road. The required building setback along Bright Road is 133 feet and along Emerald Parkway is 201 feet. The proposed plans show the bus shelter located within the building setback along Emerald Parkway. The conditional use process (Section 153.236 (C)(2)) permits the Commission to approve this encroachment with the conditional use.</p> <p>Code permits parking to encroach into the required building setback by 40%, which is shown at 66 feet along Bright Road and 80 feet along Emerald Parkway, which while labeled incorrectly, meet the requirements. Code required side (east) and rear (north) yard setbacks are 15 feet, which are met.</p>												
Shelter	<p>Code permits one passenger shelter not to exceed 50 square feet, designed to be harmonious with the architectural character of the surrounding area. The proposed shelter is shown at 48 square feet and is a detailed black metal and glass shelter located on the west side of the bus lane along Emerald Parkway.</p>												
Landscaping and Stormwater	<p>The site complies with Code required landscape provisions (Section 153.130). Landscaping includes the required perimeter buffering and interior landscaping for vehicular use areas. Mounding (42 inches tall) and landscaping (1 tree per 30 feet) are provided along the Emerald Parkway and Bright Road frontages to screen the parking area and bus drive aisle. Evergreen shrubs (3-foot tall) and trees (1 tree per 30 feet) are shown along the northern and eastern edges of the parking area to meet Code. Code required landscape areas (3,865 square feet) and trees (16) are provided within the parking lot landscape islands. There will be 76 trees (1,195 inches) removed and the applicant has worked with staff to maximize the replacement on-site. A retention pond is located at the northeastern portion of the site. Stormwater management requirements have been met for the site.</p>												
Sign	<p>Code requires ground signs to be set back 8 feet from the right-of-way. The plans indicate one proposed ground sign at the right-of-way line in southwest portion of the site adjacent to the roundabout. Given the significant depth of the rights-of-way for this site and the existing utilities with the vicinity of the proposed sign, Planning recommends the sign be permitted in the proposed location and reviewed as part of the conditional use review as permitted by Section 153.236 (C)(2).</p>												
Parking and Lighting	<p>The proposed plans show 169 parking spaces, which meet the Code required dimensions (9 feet x 19 feet). The site includes internal parking lot lighting, which include 20-foot tall poles located throughout the parking lot. The site will be required to meet the lighting provisions of the Zoning Code (Section 153.206 (A)) as part of the building permit process.</p>												
Traffic	<p>A traffic study has been accepted by Engineering. Expected traffic generation is as follows.</p> <table border="1" data-bbox="472 1692 1468 1787"> <thead> <tr> <th>Time Period</th> <th>Entering Trips</th> <th>Exiting Trips</th> <th>Total Trips</th> </tr> </thead> <tbody> <tr> <td>Weekday AM Peak</td> <td>85</td> <td>23</td> <td>108</td> </tr> <tr> <td>Weekday PM Peak</td> <td>27</td> <td>80</td> <td>107</td> </tr> </tbody> </table> <p>Although not part of the traffic study, some comparison numbers are appropriate. A regular office building of approximately 46,500 square feet would be within the</p>	Time Period	Entering Trips	Exiting Trips	Total Trips	Weekday AM Peak	85	23	108	Weekday PM Peak	27	80	107
Time Period	Entering Trips	Exiting Trips	Total Trips										
Weekday AM Peak	85	23	108										
Weekday PM Peak	27	80	107										

Details	Conditional Use
	permitted Community Plan density anticipated for this site. According to the Institute of Transportation Engineers, <i>Trip Generation Manual</i> , this use would generate approximately 130 trips in the evening peak hour, or roughly 700 for the day as traffic activity could be expected from visitors, employees leaving for appointments or lunch, or other reasons. This would not apply to the park and ride use as all activity takes place at specific periods of the morning and evening.

Analysis	Conditional Use
<i>Process</i>	Section 153.236 of the Zoning Code identifies criteria for the review and approval of a conditional use (full text of criteria at the end of this report).
1) <i>Harmonious with the Zoning Code and/or Community Plan.</i>	Criterion met: This proposal is consistent with the requirements of the Zoning Code and the Community Plan but requires a rezoning to SO. If approved, the conditional use would not be effective until the completion of the rezoning by City Council.
2) <i>Complies with applicable standards.</i>	Criterion met: This proposal is consistent with the specific requirements of the SO District of the Zoning Code, with the exception of the shelter and sign locations. Planning recommends approval of both deviations as part of the conditional use review (Section 153.236 (C)(2)).
3) <i>Harmonious with existing or intended character in vicinity.</i>	Criterion met: The proposed use will not alter the essential character of the area. The proposed COTA park and ride site is set back significantly from the street with substantial landscaping and mounding to screen the use. Lighting fixtures and levels are managed through the Zoning Code. The operation of the use is concentrated primarily in the morning and early evening, with little to no activity during the day or weekends.
4) <i>Will not have a hazardous or negative impact on surrounding uses.</i>	Criterion met conditions: Proposed operations will not have an adverse effect on surrounding uses. Lighting is managed through the Zoning Code. Nearby uses are sufficiently separated from this site. Activity will be limited generally to daylight and early morning/evening during bus route operating hours. Traffic is generally less than that which would be expected with an office use, and much less intrusive than could be permitted by a sit down or drive through restaurant use (also allowed by conditional use approval).
5) <i>Will provide adequate services and facilities.</i>	Criterion met: Nearby streets are sufficient to accommodate expected traffic movements. The submitted traffic study demonstrates that the access points to the park and ride operate at an acceptable level of service (C). The operator (COTA) has indicated that this facility meets their needs for location and size.
6) <i>Will not harm the economic welfare.</i>	Criterion met: This proposed use contributes positively to the economic climate of the city by providing an additional community amenity and supporting the goal of reducing peak hour vehicular traffic.
7) <i>Create no use or characteristic that is detrimental to the surrounding uses.</i>	Criterion met: The use will not be detrimental to the surrounding area. The proposed park and ride facility will serve an amenity for the area and future office development.

Analysis	Conditional Use
8) <i>Vehicular circulation will not interfere with existing circulation.</i>	Criterion met: Site circulation and stacking are accommodated on site. Nearby streets are sufficient to accommodate expected traffic movements. Access to the site is separated from the beginning of the roundabout area and has been approved by Engineering.
9) <i>Not detrimental to property values in the vicinity.</i>	Criterion met: This proposal will not be detrimental to property values. All site facilities are well set back from the surrounding streets and adjacent properties. The use will generally have fewer and less effects than what might be anticipated by an office development or other uses that could be approved.
10) <i>Will not impede the development or improvement of surrounding properties.</i>	Criterion met: This proposal uses are contained on site and will not impede development or improvement to the surrounding properties. Office and other related uses may be anticipated on adjacent properties to the east as the Community Plan shows these properties as Neighborhood Office.

Recommendation	Conditional Use
Approval	Planning recommends approval of the proposal with the alteration of the shelter location and the sign location, as it complies with the conditional use review criteria of Section 153.236(C).

CONDITIONAL USE REVIEW CRITERIA

Section 153.236(C) sets out criteria for the review and approval of a Conditional Use.

- (C) Action by the Planning Commission. The Planning and Zoning Commission shall hold a public hearing and shall not approve a conditional use unless it finds that such use at the proposed location meets all of the following requirements:
- 1) The proposed use will be harmonious with and in accordance with the general objectives, or with any specific objective or purpose of the Zoning Code and/or Community Plan.
 - 2) The proposed use will comply with all applicable development standards, except as specifically altered in the approved conditional use.
 - 3) The proposed use will be harmonious with the existing or intended character of the general vicinity and that such use will not change the essential character of the same area.
 - 4) The use will not be hazardous to or have a negative impact on existing or future surrounding uses.
 - 5) The area and proposed use(s) will be adequately served by essential public facilities and services such as highways, streets, police, and fire protection, drainage structures, refuse disposal, water and sewers, and schools; or that the persons or agencies responsible for the establishment of the proposed use shall be able to provide adequately any such services.
 - 6) The proposed use will not be detrimental to the economic welfare of the community.
 - 7) The proposed use will not involve uses, activities, processes, materials, equipment and conditions of operations, including, but not limited to, hours of operation, that will be detrimental to any persons, property, or the general welfare by reason of excessive production of traffic, noise, smoke, fumes, glare, odor or other characteristic not comparable to the uses permitted in the base zoning district.
 - 8) Vehicular approaches to the property shall be so designed as not to create interference with traffic on surrounding public and/or private streets or roads.
 - 9) The proposed use will not be detrimental to property values in the immediate vicinity.
 - 10) The proposed use will not impede the normal and orderly development and improvement of the surrounding property for uses permitted in the district.

COTA Park and Ride Summary of Neighborhood Meeting January 7, 2015

A neighborhood meeting was held regarding the proposed relocation of the COTA park and ride located at the northeast corner of Bright Road and Emerald Parkway. City staff and COTA representatives provided an overview of the proposed site to the neighbors. Information was provided regarding the site and why it was selected, long range planning efforts of the city and COTA, proposed site layout, bus routes and times, and details about noise, security, lighting, and site maintenance abatement. The following is a summary of the concerns raised by the neighbors regarding the proposed COTA park and ride facility.

Traffic and Transportation

The specific routes and alignments regarding the bus routes were discussed, particularly regarding the choice to use or limit bus traffic on Bright Road. The neighbors expressed a desire to direct the bus traffic to use Emerald Parkway and Hard Road only due to concerns regarding existing cut-through traffic and speeding on Bright Road. The neighbor also inquired about how the development will impact rush hour traffic in the area.

There was also discussion regarding the status of thoroughfare plan details for the Bright Road area, including, the widening of Bright Road and the cul-de-sac of Bright Road at Riverside Drive. The neighbors also confirmed additional right-of-way was secured on the proposed COTA site in the event Bright Road is widened.

Planning and Zoning

The neighbors inquired about any additional development in the Bright Road area and expressed concern regarding the involvement of the neighborhood early enough in the process to produce significant results from input. They expressed concerns about the prospects of future development in the area due to the development of the park and ride facility. There was also discussion regarding the Bright Road Area Plan and the intended uses. The neighbors are concerned the proposal will cut off the Village of Inverness and not provide the residential component as outlined in the Area Plan because no one will want to live across from the proposed facility.

Public Involvement

The neighbors had questions regarding the status of the project, public notification and future public review. There was some concern about how the project fits with the timeline of City Council's review of the development agreement. The neighbors expressed frustration that the development of the site as a park and ride was a done-deal and does not provide an opportunity for true input regarding its appropriateness on the site. There was discussion regarding the public review process through the Planning and Zoning Commission and City Council.

COTA Services

There was concern regarding why are COTA is relocating the park and ride out of the BSD entirely. There was discussion regarding COTA services within the Bridge Street and future transit services, and the differences between the park and ride services versus local bus services.

Lighting and Stormwater

The neighbors requested clarification about site lighting within the proposed parking lot. They expressed concerns about whether the lights would be on 24 hours and how the lighting from this development affect nearby neighbors. They inquired whether lighting could be reduced to limit the off-site impacts. There was also discussion regarding the proposed shelter and the lighting levels within the shelter and whether it could be minimized.

The neighbors inquired about the stormwater pond and the water would enter the stream to the north. There were concerns raised about the potential for downstream effects and flooding.

Site Security and Maintenance

Residents asked about the maintenance and inspection of the site, with concerns raised about security and abandoned cars.

Alternative Locations

There was discussion about the consolidation of COTA routes and the possibility of a second location in the Avery Road corridor. The neighbors inquired about alternative sites near or in the Dublin Village Center area, as well as the previous proposal along Sawmill Road.

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Vice Mayor Lecklider stated that he, too, is disappointed that the drive alignment could not be altered. The setback could not be at 10 or 15 feet for one building alone; it would be necessary for all three buildings.

Mr. Hale concurred.

Vice Mayor Lecklider stated that although there will now be 30 feet to work with, this is something outside the normal procedure. It has been inferred that Council members may not be overly familiar with nursing facilities, but he has two relatives in such facilities. He does appreciate what this facility will offer the Dublin community.

Mrs. Boring moved to amend the conditions to add that a semi-permanent fence be placed and maintained throughout the entire construction period to protect the trees on the western border.

Ms. Salay seconded the motion.

Mayor Chinnici-Zuercher inquired if Mr. Hale would accept the additional condition.

Mr. Hale indicated that he accepts the additional condition.

Vote on the Ordinance as amended: Mr. Reiner, yes; Mr. Keenan, yes; Mrs. Boring, yes; Mayor Chinnici-Zuercher, yes; Ms. Salay, yes; Mr. McCash, yes; Vice Mayor Lecklider, yes.

POSTPONED ITEM

Ordinance 58-07

Adopting the 2007 Community Plan. (2007 Dublin Community Plan - Case No. 07-056ADM)

Mr. Combs stated that there is no formal presentation tonight. A summary of all of the motions from the December 3 special meeting has been included in the meeting packets (attachment A).

Mayor Chinnici-Zuercher invited public comment.

Jane Swickard, 2755 Terrace Street, Millersport stated that she hopes all have had an opportunity to read her letter of December 4 regarding the new Community Plan and the proposed setbacks for the southwest corner of Avery and Woerner-Temple Roads. As stated previously, the setbacks would significantly affect the value of the property owned by her family -- a conservative estimate is 38 percent of usable land. Her family requests that Dublin's new Community Plan, which encompasses their property, be flexible in regard to setbacks and that any development proposals for this property be considered on the merits of design and what will benefit the Dublin community.

Wallace Maurer, 7451 Dublin Road, stated that he has 4 issues to address.

- Preservation of the Holder-Wright works. This was addressed previously with respect to the Indian Mounds, and he expressed concern about their preservation. Previously, the City was awarded a \$132,000 grant for the site which was canceled when the owner decided not to sell. Does this affect the preservation plans?

Ms. Brautigam responded that Council adopted a policy of intent to preserve that property. The current property owner is aware of the City's desire to purchase the property, but is not yet ready to sell. They will contact the City when they are ready to do so.

- Water towers. There is the possibility of making water towers visually palatable. Along I-270 between Dublin and Worthington, two water towers are visible. Their structure is considerably modified from the typical water tower of the past, and they are painted a soft color combination that reduces the visual impact. This could be a future art project for the Dublin Arts Council -- a large scale "Titration" type project.
- Bike lanes. In Los Angeles, drivers are very respectful of the bike lanes. However, in the Los Angeles culture, pedestrians have the right of way.
- Ponderosa Estates. He has many thoughts on this issue, and will commit himself publicly to sharing them, albeit it will be through the local newspapers.

Claire Wolfe, 5521 Indian Hill Road, River Forest stated that she is here to speak about the Memorial Bridge issue. She is very disappointed with Council's decision to remove the bridge from the Community Plan. The bridge has been in the Plan for ten years, which is very foresighted. Removing it from the Plan is very shortsighted. Its presence in the Plan did not mean that it necessarily must be built in that location. Its proposed location was very close to her home, so she could not be accused of being one of the

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"not in my backyard" citizens. The City needs to make provision for additional traffic across the river. Some of the remarks expressed in the local papers were somewhat inane, such as, "It is not our problem that the roads are so full" and "Much of the traffic is from the north and those communities should participate in the building of a bridge." That may be true, but Dublin recently rezoned a very large area between McKittrick and Brock roads. There is also Deer Run, Glacier Run and Glacier Ridge Park and the northern part of Muirfield road. The Cardinal Health new construction is anticipated to add an additional 600 cars to SR 745 and Emerald Parkway. Removing the provision for the bridge in Amberleigh where rights-of-way have already been identified seems shortsighted.

Mayor Chinnici-Zuercher requested that the Woerner-Temple/Avery Road area plan be displayed.

Mr. Combs noted that the drawing could be found on page 85 of the Community Plan draft.

Ms. Salay stated that originally she did not support a mixed-use development in this location, nor did most of the neighbors. What made the concept palatable was the incorporation of the large setback that would preserve the pastoral feel along Woerner-Temple west and south on Avery Road. That may not be maximizing the value for the landowners, but that is not the standard by which Council makes its decisions. While she is sympathetic to the plight of the landowners, that is the risk of investment -- there is not a guaranteed return. Time and circumstances can affect it. However, the landowners will not lose; they will receive a fair return for their land. Long term, this is the best plan for the community. A Community Plan must reflect the overall interest of the community rather than the individual interests of the landowners. The right thing to do is to keep the setbacks as discussed previously.

Mayor Chinnici-Zuercher inquired if there were any other comments regarding the Community Plan. The staff memo included in the packet lists the changes that were made in the final draft as a result of Council's public hearing discussions and direction.

Mr. Keenan stated that the Community Plan update has encompassed a three-year effort. He thanked everyone for their hard work on the project.

Ms. Salay stated that she recently reviewed the Community Plan materials she has accumulated over course of the project and was struck by the overly optimistic goal of the initial timeline of 12 to 18 months; it has taken nearly four years. She was one of the original advocates of the need to update the Community Plan. A large portion of her ward was undeveloped, and the area was under-planned. Although it has been a long process, it has been very beneficial. Council has addressed many issues, many of which were unexpected. She thanked staff, particularly Planning, for the very long hours committed to this task. She is concerned, however, that in the end Council may have yielded to the political pressure of the year and not adopted the best long-term policy regarding a couple of issues. Dr. Wolfe, who spoke earlier, may be correct. If so, she apologizes to the future residents who may have to re-visit the bridge issue. Former Council Member Kranstuber, who mentored her when she first assumed her seat on Council, once said that during his years on Council, he observed that Council had not bowed to political pressure but had worked together to do what was best for Dublin. She had hoped that would also be the outcome of this effort. Nevertheless, Council must move on. She heartily supports the Community Plan update and is honored to have been part of the process.

Vice Mayor Lecklider stated that he was also involved with the 1997 Community Plan update, and is not certain where that ended and the new one began. In Dublin, if Council errs, they err on the side of inclusion, including the opinions of more rather than fewer, and that ensures a better result. In addition to staff, he would like to recognize the various boards who had input and devoted time to this effort, particularly the Planning and Zoning Commission. Although there may be details that do not meet his expectations, the vast majority of it does. That is the result of a democratic process. The City and the community can be proud of the result.

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Mrs. Boring stated it has been a long three years, and she will therefore make her comments brief. She thanked Mr. Combs and all the Planning staff for their work.

Mr. McCash stated that this began as a simple update, but evolved into a complete re-write of the Community Plan, completed 10 years after the adoption of the previous version. The 1997 process was also lengthy, but probably not as trying as this process. He commended staff. This is probably some of their best work. Unfortunately, Council's subsequent work may not have been their best work. He has debated the proper action for himself tonight in view of the likelihood that future residents will confront a future Council about the need for an additional bridge over the river. He wants to be on the record for his position that Council's decision regarding the bridge may not have been the best. He trusts that in the future, an update or revision will reevaluate this issue.

Mr. Reiner stated that he assumed a seat on Council at the time the 1997 Community Plan was being completed. It is not improbable that a future Council will be doing the same in another ten years. This Plan is based upon 10-15 year projections, and the community will likely change significantly during the next few years, resulting in the need for another review. He thanked Mr. Combs, the Planning staff and the City Manager for taking on the monumental task of a Community Plan re-write simultaneously with the already heavy workload dictated by the high volume of development in Dublin. He believes this is the best plan for the community at this time. A future community and Council will produce another plan, if needed.

Mayor Chinnici-Zuercher also thanked Mr. Combs and all staff who worked on the Community Plan. As she contemplated the point at which the project took a turn for the better, she believes it was when staff took charge of the project, following the early work by the consultants. That is something that needs to be remembered in the future. The staff, Council and citizens know what the community wants and what would be best for it. Consultants can play a role, but not a lead role, in shaping the Community Plan for the community. She commended Mr. Combs for "stepping up to the plate" and committing the extra time to accomplish the task. All Council members, with the exception of Mr. Keenan, were also involved with the 1997 Community Plan. That update was a very community-based effort, with hundreds of people involved. What it resulted in was a tremendous "buy in" of the community for many subsequent years. Many people in the areas that were later developed were involved in the development of that Community Plan and were able to shape what ultimately happened. She agrees that in the next ten years or less, the City will likely re-evaluate the 2007 Plan. She believes there is great value in the active participation of citizens in the process – in fact, they should lead the process. The outcome may or may not be different. The important thing is that it is really their Community Plan. Citizen investment in the application of the Community Plan is the reason Dublin enjoys such a beautiful community. She hopes this perspective is pulled from the archives at the time Dublin again considers changes to the Community Plan.

Vote on the Ordinance: Ms. Salay, yes; Vice Mayor Lecklider, yes; Mr. Reiner, yes; Mr. Keenan, yes; Mayor Chinnici-Zuercher, yes; Mr. McCash, no; Mrs. Boring, yes.

SECOND READING/PUBLIC HEARING - ORDINANCES

Ordinance 87-07

Adopting the Annual Operating Budget for the Fiscal Year Ending December 31, 2008, and Declaring an Emergency.

Ms. Brautigam stated that the information provided in this packet includes the updates made as a result of Council's budget work sessions in November.

Mrs. Boring stated that she missed the second budget work session. She has some major concerns about some of the expenses that have been budgeted. She does not believe that Council has a sufficiently tight handle on the budget and that they should begin to look at certain things more closely. There are tasks that current staff is no longer able to do, so additional full-time staff is being added to do the work. She would like to have an understanding of the reasons for that.

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The purpose of the US 33 area plan is to establish a general vision upon which future policy decisions can be based as conditions warrant.

An issue was raised at the last meeting due to a letter that was received from the Central Ohio Bicycle Advocacy Coalition (COBAC).

Mayor Chinnici-Zuercher requested that Council discuss the US 33/Jerome Township area before moving on.

Mrs. Boring inquired if Council would continue discussion of the Northeast Quad area plans that were not discussed previously.

Mayor Chinnici-Zuercher noted that the only one addressed in the meeting materials is the Bright Road area.

Mr. Combs responded that the memo contains additional information that Council requested on that area.

Mrs. Boring inquired the plan for continuing discussion.

Mayor Chinnici-Zuercher asked for her preference. Both US 33 corridor and Northeast Quad residents are present. Citizens have signed up to speak on the following areas: Tuller Road/Riverside Drive, Rings Road, Northeast Quad, Memorial Drive extension and bridge, US 33 corridor, and the Community Plan in general.

Mrs. Boring stated that for the public's benefit, there should be a schedule for the discussion.

Ms. Brautigam responded that staff's plan was as to discuss the outstanding issues, including any outstanding items in the Northeast Quad; complete staff's report; and conclude with Council discussion.

Mrs. Boring stated that proceeding in a methodical manner, Council could begin with the Northeast Quad, then proceed to the US 33 corridor, then address other areas.

Mr. McCash stated that Council also provided a memo with an alternative timeline for adoption of the Community Plan. Is the intent to adopt the plan at the December 10th meeting, or has that been modified to January 7th?

Mayor Chinnici-Zuercher responded that it was Council's goal that it would be the present City Council that would approve the 2007 Community Plan, and the last meeting this year is December 10th.

Mr. McCash inquired Council's response to staff's suggested alternative timeline. If the adoption is not intended to occur until January 7th, there is no need for him to be present for this discussion.

Mayor Chinnici-Zuercher stated that it was well over a year ago that Council requested that the Plan be completed with this Council, which is the body most knowledgeable of this work. Therefore, she would prefer to maintain the December 10th adoption schedule.

Discussion followed.

Council consensus was to schedule a special meeting on December 3 at 6:00 p.m. for completing discussion of the Community Plan. The goal tonight will be to end discussion at 11 pm.

- **Northeast Quad – Bright Road Area Plan**

Mr. Combs stated that at the last meeting an overview of the plan was given, covering the area east to west along Emerald Parkway. There is Office use along the ravine area; maintenance of park to the north of the ravine to preserve existing cemeteries and the Indian archaeological site, the Holder Wright works. To the west, there is additional Single Family use, and as Emerald Parkway turns to the north, along the final segment to be constructed, there would be a variety of Office use around the interchange at Sawmill Road/I270. Moving further to the north along Bright Road, there would be Medium, Mixed Residential on the north and south sides with additional Office integrated into existing Office along Sawmill. At the last discussion, Mrs. Boring raised a question about the proposed density. Comparative densities are noted within the staff memo.

Mrs. Boring stated that at one time, Area 3 was proposed as Office. Converting it to Office rather than Residential has been proposed. There is currently a mix there. This

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is a critical issue to the area residents. Perhaps those residents should have an opportunity to speak.

Jim Hendrix, Continental Real Estate, indicated he is representing Alan Vrabel who owns the 33 acres at the corner of Tuller and Riverside Drive. Previously, Paul Ghidotti presented a bubble plan of what they hope to develop on that site -- a mixed use of senior housing, nursing home, medical and ancillary retail. Mr. Vrabel purchased the property approximately 13 years ago and cleaned up the driving range with the intent to develop a mixed use office campus on the site. That plan has evolved over the years. He is also in the nursing home business. It is their belief that the mixed use of senior housing, medical and retail would be a better use of the property. They request that the City consider those plans in connection with this property.

Mack Parkhill, 7879 Riverside Drive, stated that he is a trustee with the East Dublin Civic Association. As well as speaking for himself, he indicated to Randy Roth earlier today, whose father is ill, that he would present his concerns. The residents believe that the proposed Summitview/Sawmill area plan is good. They do not object to alternative land use plans as long as retail is not included, and it has not been. There are concerns about the SR 161/Sawmill Road/Riverside Drive area. Many suggestions have been made for the Digger & Finch, formerly Bash, property. However, the scenic corridor designation for Riverside Drive does not begin at Tuller Road; it has always started at SR 161 and proceeded to the county line. In the past, the residents have opposed most of the proposals, which have included a large, 3-4 story apartment building and a large, Florida-style high-density residential project. The City Planning Commission agreed that those proposals were not appropriate for a scenic route. They ask that Council keep that in mind as they review this area. At this time, another high density housing development is proposed for the area. The residents ask that Council protect this scenic route. The residents love the existing greenspace. However, if plans for the area do not remain exactly the same, they ask that whatever the plan is that it be more in line with the existing use than what is proposed, which is to fill in the site and completely change the entire character of this entry point to Dublin.

Speaking for Mr. Roth regarding the Bright Road area, there is concern regarding staff's plan for land along Bright Road east of the power lines. The 1997 Plan suggested that the land north of Bright Road be re-developed as Office, similar to the existing professional offices on Bright Road, and that the 10-acre site immediately south of Bright Road be redeveloped as multi-family condominiums to protect the Village of Inverness. Recently, staff has proposed inverting the plan so that the Office use would be contiguous with the existing Office use on the south side of Bright Road. The residents agreed, so the Plan proposed multi-family condominiums in the area north of Bright Road and professional Office to the south. Last month, the residents were startled to see a new draft of the plan, which shows both areas developing as multi-family. The residents were not consulted about the last-minute change, and they are concerned about it. Previously, any proposals were discussed with the civic association. It is their belief that the recent change is a mistake for the following reasons:

(1) The professional offices on the east side of Bright Road and to the east of Sawmill along Billingsley and Sawbury are fully occupied. There is a market for professional office space in the Sawmill corridor because Columbus did not zone enough space on the east side of Sawmill Road. Additional professional offices would serve residents in the Sawmill corridor and would not compete with Dublin's new Innovation Center. A large, multi-family zoning on the east side of Bright Road would make it difficult to attract upscale office projects to the neighborhood. A large multi-family area would damage the Office use potential of the surrounding land just as the apartments on Sycamore Ridge damaged the Office potential along Tuller Road. Not one new office building has been built in that area since the apartments were added.

(2) It sets a double standard for the appearance of Emerald Parkway east of the Scioto River. The City has not allowed a multi-family rezoning anywhere along Emerald Parkway since it was planned in 1990. All the zonings have been for Office, Institutions, or Single Family Housing.

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(3) Less than half of the dwelling units in east Dublin will be owner occupied if the suggested plan is followed. The Civic Association is adamantly opposed to lowering the proportion further. It is very easy to attract residents to apartments in Dublin because of the schools, but the high proportion of transient students who come from areas with very poor schools has had an extremely negative impact on the schools that East Dublin children are attending. It is incumbent upon Dublin not to exacerbate this problem.

(4) The plan shows a parking lot in the area north of Billingsley Creek where the Indian burial mounds exist, an area designated for archaeological preservation. The parking lot should be moved to the south of the creek or the western end of Bright Road where it will not compromise the historic district.

(5) They oppose several components of the Sawmill-SR 161 area plan. They believe the Sawmill Road frontage should remain commercial. They support the City's long-standing goal to generate revenue from this corner, and they would prefer to see it developed imaginatively as a retail center rather than abandoned to multi-family or a village concept zoning. They would like to see Snouffer Road continued west across Sawmill Road to improve access to the interior of that site, and they would like to encourage the development industry to acquire the small frontage properties along Sawmill Road and include them in a larger, retail PUD. The new retail developments along SR 161 send a message that the land is suitable for retail where the road access is adequate and the sites are visible. The problem is addressed by improving the flow of traffic and visibility and redeveloping the frontage. It is doubtful an eastern-style, urban village development would succeed in the Sawmill corridor because the area is dominated by mid-scale, bargain retailers, not upscale retailers. They do not believe the character of the area can be changed east of the power lines.

Mr. Reiner inquired if his statement is that there is 50 percent existing rental there now. Mr. Parkhill responded that would be the proportion with the additional proposed multi-family. That is of great concern to the existing residents.

Mayor Chinnici-Zuercher requested that Mr. Combs explain the reason the area plan was changed.

Mr. Combs responded that it has not changed from the June 2007 draft. Although there were concerns voiced at one of the public meetings and the option of placing Office on one of the two sites suggested, there was no direction given at any of the joint work sessions.

Multi-family condominiums north of Bright Road, east of Emerald Parkway

Mrs. Boring stated that the residents desire that the Village of Inverness be surrounded by multi-family condos. The area they are concerned about is the area north of Bright Road and east of Emerald Parkway. The adjacent area is designated as Office, and those offices are always full. She would suggest that this area also be changed to Office use.

Mrs. Boring moved to revise the area plan to designate this particular area as Neighborhood Office.

Mr. Reiner seconded the motion.

Mr. Keenan requested clarification of the site.

Mr. Combs responded that it is Bright Road between Sawmill and Emerald Parkway.

Mr. Keenan inquired if the Office use would have appropriate access.

Mr. Combs indicated it would.

Mr. Keenan inquired if any issues were envisioned with the proposed change.

Mr. Combs responded that an Office use rather than Residential would generate a difference in traffic, but he could not say specifically how it would impact the intersection.

Mayor Chinnici-Zuercher noted that these would be neighborhood office size buildings.

Mr. Combs responded that by definition, it would be within a range of 9,000 sq. ft. /acre.

Vote on the motion: Mr. Keenan, yes; Mr. Reiner, yes; Mayor Chinnici-Zuercher, yes; Mrs. Boring, yes; Mr. McCash, yes; Vice Mayor Lecklider, yes.

Proposed Parking Lot on Indian Mounds Site

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Mr. Combs noted that given the decision that was made to cul de sac Bright Road along Riverside Drive, the expectation is that this would become a community-scale park due to its importance. That generates the need for some level of parking provision. The intent was to represent a very small parking lot that would be integrated into the design. The idea was to keep all of the park traffic off of Bright Road as a residential road, and focus it off of Emerald Parkway.

Mrs. Boring stated that it is a good idea to provide sufficient parking for these parks. Did Mr. Parkhill understand the reason for the parking space? What was the specific concern?

Mr. Parkhill responded that the concern is that the parking is located much too close to the archaeological site itself, which was intended to be preserved as it is. The civic association suggested that the parking lot be moved south of the creek and west of Bright Road, where it would impact the archaeological site must less.

Vice Mayor Lecklider stated that this is conceptual only. He is confident that the City would not create a parking lot that would damage the integrity of the archaeological site. He is not certain the City would want to incur the expense of a roadway in that location.

Mr. McCash noted that there is the issue of the Billingsley Ravine. It is better to show it this way with the understanding that a later Council could decide to build it across the ravine. The important thing is to remember the impact on the ravine itself.

Mrs. Boring inquired if Council would consider a curbcut on Riverside Drive.

Council indicated they would not.

Mrs. Boring stated that she is concerned about the neighborhood response if the road is shown with an access off Bright Road.

Mr. Keenan made a motion that the Bright Road area plan indicate only that there would be parking provided, but not a specific location.

Mayor Lecklider seconded the motion.

Vote on the motion: Mrs. Boring, yes; Mr. Keenan, yes; Mr. Reiner, yes; Mayor Chinnici-Zuercher, yes; Mr. McCash, yes; Vice Mayor Lecklider, yes.

Sawmill Road and SR 161

Mrs. Boring stated that the proposed plan was probably well received due to the greenway along Riverside Drive and the pedestrian pathway that runs east and west. The alternate proposal that was forwarded to the City was included in the meeting materials. She requested staff's comments.

Mr. Combs stated that this is the plan Mr. Hendrix referred to earlier. They propose a second-story office building on Tuller Drive, one to three stories in height. The plan continues the concept of a pedestrian greenway. Their proposed changes would include retail along Riverside Drive and a higher density mixed residential. In general, staff has no significant objections. Placing a lot of retail along Riverside Drive is not the best alternative for the area. The Community Plan provides for a road with river heritage character, with minimum setbacks of 60-100 feet. He is not certain Office at that scale would work.

Mr. McCash moved to leave the area plan as it is shown in the Community Plan, where it has been tested and modeled. This would not preclude the submission of future rezoning requests, and traffic studies could be conducted at that point.

Mrs. Boring requested input regarding plans for the Sawmill Road area. Mr. Parkhill has expressed concerns with staff's plan.

Mr. McCash stated that he disagreed somewhat with Mr. Parkhill's comments that it is not possible to change the mix in that area. With the right development plan, it would be possible to change and improve the mix. A good example is the southwest area of Dublin. The development, which includes the Golf Club of Dublin, has distinctly changed the area for the better. On the west side of Sawmill Road, Dublin has the opportunity to demonstrate to Columbus how to do development exactly how to do it right.

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Mrs. Boring stated that she does believe Dublin should consider the suggestion to extend Snouffer Road.

Mr. Combs responded that this has been suggested previously. The City of Columbus does have jurisdiction over Sawmill Road. The plan does include the comment that Dublin would be willing to work with Columbus to address traffic issues at the various intersections, however, no specific provision was made regarding Snouffer Road.

Mrs. Boring inquired if a notation could be made in the Community Plan that it is Dublin's desire that Snouffer Road be extended across Sawmill Road.

Mr. Combs responded that a comment to that effect would be added.

Mrs. Boring referred to the provision for a mixed-use town center with a greenspace setback. Could this site be marketed for something educational, such as an institute – something other than mixed use?

Mr. Combs responded that the definition of mixed use is quite broad. It actually provides for a mix of government offices and institutions such as an educational use. That use could be incorporated into the plan. A walkable environment where an educational use could be integrated with the surrounding uses could attract interest.

Mrs. Boring inquired if that use should be specifically suggested, or should the plan remain as it is.

Mr. Combs responded that it is already covered in the list of mixed uses, but a note could be added to indicate an interest in having an educational use integrated into that area.

Mayor Chinnici-Zuercher suggested that it be added to the Planning Issues and Challenges on page 138. An educational use would not typically be thought of as a town center use, so it should be specifically noted.

Mayor Chinnici-Zuercher inquired about the absence of Lowe's on the map.

Combs responded that the policy direction seemed to discourage big box retail development. Those types of uses typically do not have longevity, and eventually this site will need to be redeveloped.

Mrs. Boring suggested that this specific planning area be extended further south to SR 161, retaining the existing bank building.

Mr. Combs stated that the general concept is to push the buildings to the street, in some areas providing greenway connections in some areas, but taking more of an urban feel. That pattern can be duplicated. The larger issue would be the type of uses. Would there be a different policy direction for that area, or would it be part of the town center development area.

Mr. Reiner stated that he would think it could certainly be part of the town center developments. For the present, Dublin is happy to have Lowes and the other businesses that are active there. This is a long-range plan to year 2050.

Mayor Chinnici-Zuercher moved to extend the town center concept to the SR 161-Sawmill intersection.

Mrs. Boring seconded the motion.

Vote on the motion: Mrs. Boring, yes; Mr. Keenan, yes; Mayor Chinnici-Zuercher, yes; Mr. McCash, yes; Vice Mayor Lecklider, yes; Mr. Reiner, yes.

Summitview and Sawmill

Mrs. Boring stated that the recommendations for this area and the areas to be protected are satisfactory. The association is hoping for flexibility in the plan. They are interested in maintaining a more "country" use, such as an equestrian park.

US 33 Corridor-Jerome Township Area Plan

Mayor Chinnici-Zuercher invited citizen comments.

Jesse Dickinson, 10144 Brock Road, Plain City, stated that he believes his comments reflect the opinions of others in Jerome Township. He would like to comment on three issues: remarks at a previous meeting, the views of the citizens of Jerome Township, and remedies for the US 33 corridor. At a previous meeting, Mr. Guerin summed up the views of 100 residents. In the Industrial Parkway corridor, approximately one half of the

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residential homes are within ½ mile of the US 33 corridor. The proposal is to have businesses develop along that corridor. He considers that to be a transfer of value from the individual homeowners to corporations. The homeowners' homes become valueless. The property becomes more valuable, but the businesses that move in will destroy the lifestyle of the existing residents and the potential for anyone else to use the land. Look at the Industrial Parkway area today for example.

[The meeting was briefly recessed for technical/recording difficulty.]

Mr. Dickson stated that another comment was made by a Council member that a township trustee's remarks were disingenuous. He concurs with that comment. Many of the citizens want low density, residential development that can support the three school districts. The citizens group, originally designed for the citizens, has been infiltrated by architects and developers. The township trustees do not listen to the people, the residents. There have been 10 referendums. He has attended the MORPC and LUC meetings and spoken on behalf of the residents. He is providing a CD to Council tonight with a survey conducted of their area. It is well done, and he hopes Council reviews it. He sees three possible remedies to change Dublin's plan: (1) The US 33 corridor plan be revised to resemble Dublin's earlier plans for development of a lighter density. (2) Referendum of the proposed plans. He believes that in Dublin he has found people who believe they should be representatives of the people, not dictators to the people. (3) Merge. Annex the area and provide the proper zoning.

Kathleen Crowley, Planning and Zoning Coordinator for Jerome Township, stated that she realizes Dublin sees this area as its growth corridor. She would like to ask a couple of questions on behalf of Jerome Township residents that are in attendance tonight. She inquired if the US 33 corridor planning area that is being discussed, a couple thousand acres, is currently in Jerome Township.

Mayor Chinnici-Zuercher affirmed that it is.

Ms. Crowley stated that in order for the City of Dublin to implement this type of planning, the property owners would have to annex to the City of Dublin. The area plan being discussed by Dublin City Council is solely Dublin's plan, not Jerome Township's or the City of Marysville's. As it is now, those 2,000 acres are within Jerome Township. The citizens of Jerome Township could only be subject to Dublin's taxes if they were to annex to the City.

Mr. McCash responded that the residents are already paying school district taxes, which is the greater tax.

Ms. Crowley that the financial situation in a township is different than in the City.

Mr. Keenan stated that the millage is the same in the township as it is in the City. The only difference is the 1/2 mill the City collects. This issue is not about taxation, however; it is about planning.

Ms. Crowley responded that it is about land use, and Dublin can determine the land use only if the area is annexed into the City.

Mayor Chinnici-Zuercher stated the City's plan is conceptual only. When Jerome Township does their area plan, they do not look only at the township area. A plan looks at contiguous area factors that would have an impact on the municipality or township.

Mr. Reiner stated that there is often a misunderstanding that if an area annexes into the City, their taxes will greatly increase. The largest portion of the taxes paid are to the school district. The City portion is minimal. Additionally, the property owners are often concerned that the City will annex their property. Only the property owner can initiate an annexation.

Mr. Keenan stated that for the small amount of millage the City collects, a great number of services are provided.

Vice Mayor Lecklider stated that he would like to emphasize what Mr. Reiner alluded to and that annexation is not an action the City pursues. To be annexed, the property owner must initiate the process.

Ms. Crowley stated that she is aware of that. She also wanted to confirm that this is not a zoning; it is a community concept plan. She is simply confirming the facts for the township citizens.

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Mr. Reiner stated that as Mr. Keenan indicated, with an annexation the property owner receives a large number of City services in return for a nominal tax increase. Dublin has the one of the highest levels of public services provided for its residents.

Mayor Chinnici-Zuercher inquired if Council members would like to request any changes to the US 33 Corridor plan, which is a conceptual design for areas outside the City's current jurisdiction.

Mrs. Boring stated that she believes some property owners in this area are aware of the high level of planning, including buffering, that Dublin provides and would be interested in annexing to Dublin. However, is it possible to plan around those neighborhoods, not over them?

Mr. McCash stated that this is essentially a future redevelopment concept. If this land were to become more valuable as Office use, the property owners would be inclined to sell their property to benefit from the higher value and move from the US 33 corridor.

Mrs. Boring stated that the "cashing in" concept is often misunderstood. \$300,000 acre for raw land may seem to be a good price, but selling the property for \$300,000 when a house is included does not seem to be a "windfall."

Mr. McCash responded that the land involved in the Tuttle Crossing extension and rezoning increased much more in value than the houses sitting on the land. If the land is sold, the property owner will realize a much greater profit that they would have before it was rezoned. If the land in the US 33 corridor were to be annexed into Dublin sometime in the future and zoned as Commercial but the land around it remained residential, Dublin would be sensitive to the adjoining neighbors. Dublin's zoning code requires a buffering element between commercial and residential properties.

Mrs. Boring noted that is how the "islands" are formed. She inquired what low density is contemplated here.

Mr. Combs responded that it would be single family.

Vice Mayor Lecklider stated that development happens incrementally and unavoidably creates islands. It is unlikely development would occur 300-400 acres at a time.

Mayor Chinnici-Zuercher inquired Mrs. Boring's recommendation.

Mrs. Boring responded that she would like the plan to indicate the existing residential. If she lived in this area, this plan would be very unsettling to her.

Mr. Keenan stated that US 33 is similar to Bethel Road, which eventually became retail.

Mrs. Boring stated that Bethel Road, however, is a major collector.

Mr. Keenan responded that US 33 is as well. This plan is a concept for 30 years in the future. If proper planning does not occur now, problems will result from uncontrolled development. The planning has no real effect, unless the land is annexed.

Mr. Keenan noted that, in his opinion, there is a significant problem with the plan. Prime real estate on a limited access highway is designated as Low Density Office use. What is the reason for that provision? This area is within the City's planning area.

Mr. McCash responded that he believes that provision has been carried over from the once contemplated Erickson plan.

Mr. Keenan stated that he recalls the City decided to save this site for a use better suited for this prime real estate.

Ms. Brautigam stated that when this area was last discussed, staff recommended that the land be zoned as High Density Office use. The issue was raised whether transportation planning for high density has been conducted. As that had not occurred, staff agreed to remove the high density indication for this area. However, staff does agree that the proper plan for that area would be high density, and if Council would like to re-insert that into the plan, they would be happy to do so.

Mr. Keenan moved to revise the use from Low Density Office to High Density Office use for this site.

Mr. Reiner seconded the motion.

Mayor Chinnici-Zuercher clarified that the motion is to change the Cosgray/Shier Rings/SR 161 Low Density Office to High Density Office.

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Vote on the motion: Mr. McCash, yes; Mr. Reiner, yes; Mrs. Boring, yes; Vice Mayor Lecklider, yes; Mr. Keenan, yes; Mayor Chinnici-Zuercher, yes.

Mr. McCash inquired if there is an aerial of the area.

Vice Mayor Lecklider responded that it is on page 167 and 171 of the draft plan.

Mr. McCash referred to the Industrial Parkway area. Except for a small area, the majority of the area up to the Post Road interchange is designated General Industrial.

Mr. Combs indicated the areas that included Residential -- Industrial Parkway in the center of the planning area, Mitchell-DeWitt Road, Warner Road and area to the north.

Mr. McCash inquired if the homes preceded the industrial, or the reverse. He is curious about the Jerome Township planning.

Mr. Combs indicated he is not aware of the answer.

Mr. McCash stated that issues have been raised about the proposed Office and Industrial designations, yet, in Jerome Township, General Industrial exists next to Residential. That does not occur in Dublin.

Mayor Chinnici-Zuercher indicated that Mrs. Boring's question is currently on the table, which is, can the Community Plan indicate the existing Residential in the area.

Mrs. Boring asked if staff had discussed the concept plan with any of the residents of this area.

Mr. Combs responded that staff had spoken with a couple of the residents. Copies of their correspondence were included in the last Council packet. Throughout the process, various residents of Jerome Township have attended Community Plan workshops to learn the intent of the plan and offer comments.

Mr. McCash moved to add an asterisk which states that it is not the City's intent to displace residential properties within the area. However, if the properties are re-developed, the designation indicated would be the preferred scenario.

Mr. Reiner seconded the motion.

Vote on the motion: Mrs. Boring, yes; Vice Mayor Lecklider, yes; Mr. Keenan, yes; Mayor Chinnici-Zuercher, yes; Mr. Reiner, yes; Mr. McCash, yes;.

Mayor Chinnici-Zuercher stated that there would be two additional citizen comments before the Community Plan discussion is concluded.

John Pelton, Dublin resident, stated that he is a realtor and he owns property on Rings Road. The Southwest Plan will significantly impact seven contiguous properties on Rings Road. He referred to the map of the Rings and Avery roads area on page 157. Several properties on Rings Road are being acquired by Dublin Engineering. City staff indicates that those houses will be removed and the road will be widened in that area. He inquired if it would be widened to four lanes.

Ms. Brautigam stated that she does not believe the City transportation plan provides for Rings Road to become four lanes. She asked Mr. Hammersmith for clarification.

Mr. Hammersmith stated that there are plans for Avery Road to become four lanes, but not Rings Road.

Mr. Pelton responded that he had meant to say Avery Road would be widened to four lanes. West of that intersection are the seven contiguous properties on Rings Road to which he refers. According to the Southwest Plan, the area across the street from those properties will become Standard Office. Behind those properties is a reserve area with dense woods and undergrowth, which provides a buffer to an adjacent upscale condominium neighborhood. These seven beautiful properties have now become unsaleable as homes. There are no sidewalks and no curb and gutter. The properties have been so devalued that the property owners will not be able to afford connection to City water and sewer when it becomes available. As mentioned earlier in tonight's discussion, these properties have become an island area. He rents his property to a family with a child who attends a Hilliard elementary school. This year, Hilliard Schools terminated bus service to these homes as they are within a mile of the newly opened Washington Elementary.

Mr. Keenan stated that he does not concur with his argument about the negative impact of Standard Office across the street. The Killilea subdivision does not appear to have

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suffered negatively by the Cardinal Health development across the street. Is Mr. Pelton suggesting the properties should be rezoned?

Mr. Pelton responded that he is not. He does not know the answer for these properties, but they can no longer be sold as residential homes. Perhaps senior housing would be an alternative. However, he does want Council to be aware of the negative impact on these once valuable homes, now an island area.

Mr. Keenan inquired the amount of acreage involved.

Mr. Pelton responded that they are one to two-acre sites, a total of 10 to 11 acres.

Bob Warne, 5808 Tartan Circle, stated that he attended an earlier meeting where the proposed Memorial Drive extension and bridge across the river were discussed. The significant problem with that proposal is increased traffic volume. Between Dublin and Muirfield, there are 22 entrances. Between Avery Road and Muirfield, there are 11 entrances; three of those are offices and one is the golf course entrance, which generates a high volume of traffic. How many homes would be impacted by the extension of Muirfield Drive? In that area there are a minimum of 450 two-car garage homes accessing Memorial Drive an average of 3 times daily. In addition to the number of Memorial Drive accesses generated by those homes would be the traffic that would come from southern Delaware. The increased traffic volume will result in a larger number of accidents. He would like to remind Council of the old adage, "If it isn't broke, don't fix it." Memorial Drive isn't "broke," and extending it will only create greater traffic issues for the residents in this area. He requested that Council reconsider their vote on this proposal and completely remove it from the City's agenda.

Mayor Chinnici-Zuercher stated the discussion of the Community Plan is completed for this evening and will be continued at a December 3 Special Meeting. She requested that the public notices list the areas that will be discussed at that meeting.

COUNCIL COMMITTEE REPORTS/COUNCIL ROUNDTABLE

Mr. McCash noted that he would be out of town on November 26-28.

Mrs. Boring stated that:

1. She would ask Ms. Clarke to include in Council's next packet a list of high school stadium rentals for various activities.
2. She recently attended the National Leagues of Cities conference in New Orleans. She found a great spirit in the residents with whom she spoke. It is a unique, yet diverse City.

Mr. Keenan, Finance Committee chair, stated that:

1. There have been four Finance Committee meetings in the past week and a half. The last of those occurred this evening at 6 pm, during which the City's cost of services legislation was reviewed. A public hearing on that ordinance will occur at the December 10 meeting.
2. On November 15, Ms. Brautigam, Mr. Hammersmith, Mr. Combs and he attended the annual LUC Regional Planning Commission meeting in Urbana. The speaker, Dr. Robert Head, was very dynamic.

Vice Mayor Lecklider thanked staff for their willingness to commit the extra time needed to complete the Community Plan this year.

ADJOURNMENT

The meeting was adjourned at 11:15 p.m.



Mayor – Presiding Officer



Clerk of Council

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along Bright Road, additional residential would be integrated with the Village of Inverness. To the west of the future extension of Emerald Parkway would be additional office development.

Bright Road at Future Emerald Parkway and Riverside Drive

The concept provides for the preservation of the existing Indian mounds site and cemetery area as a large park area that can be connected with pedestrian paths. South of the ravine, which is a major natural feature in this area, integrated office development that faces Emerald Parkway is proposed.

Mrs. Boring stated that this is essentially the plan that is currently in existence. Mr. Combs responded that it is a refinement of the 1997 Bright Road area plan.

Mrs. Boring referred to #12 - medium density residential that backs up to Grandee Cliffs. Would the lot lines be aligned with this, or is that not essential? She is concerned about placing medium density development in that area versus low density.

Mr. Combs responded that it is generally intended to be consistent with the surrounding residential areas. Staff could verify the correct category with the Future Land Use map.

Mrs. Boring asked that staff do so. She believes this may have changed, as medium density seems somewhat high considering the older properties in that area.

Mr. Keenan inquired the difference between low and medium density.

Mr. Combs responded that with a medium density, there would probably be a single-family development. With mixed residential, there would be a variety of housing types within the same development. He believes the concept for this area is single-family, two units per acre. The medium density mixed residential provides for five units per acre with integrated types of housing. He will check to verify the densities.

Mrs. Boring stated that the premium office provided for at the interchange would certainly be a positive. However, in the past, there was an issue with the impact of such density on the traffic. She assumes that if there is development interest, the developer would be required to conduct traffic studies to ensure that the density of the proposed office is appropriate.

Mr. Combs responded that when any major office development is proposed, a traffic study of trip generation and access issues would be required.

Mrs. Boring stated that she believes there has been a change from the 1997 plan. With the Emerald Parkway extension in this area, what is the feedback from the residents regarding the change from medium density development to office?

Mr. Combs responded that staff has heard responses on both sides of the issue. However, there has not been overwhelming response for one side over the other, so the concept has remained the same throughout the process. From a planning perspective, there is existing residential development just off of Sawmill, and surrounding it completely with office development would not be good land use. It would be preferable to add a blend of additional residential and office. Keeping in mind the commercial/residential balance and what is anticipated with the Central Ohio Innovation Center and some other areas, it would not be wise to include too much office in these area plans. That might make it counterproductive to focus on office development in some of the other areas.

Mrs. Boring asked that staff verify that the proposed density matches the existing density for the adjacent land.

Ms. Salay asked if there was a timetable for the cul de sac of Bright Road at point one on the map.

Mr. Hammersmith responded that the expectation is that it would occur as part of the Emerald Parkway Phase 8 construction, which is currently in the design stage.

Ms. Salay stated that the reason she wanted to clarify that point is that in the past, Council has made decisions to cul de sac various roads, which the residents relied

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upon. Later, Council has reversed such decisions. She wants to verify this is scheduled.

US 33 Corridor Area

Alan Guerin, 10240 Mitchell DeWitt Road, Plain City stated that he speaks for the owners of the 100 plus homes located in the US 33 corridor. They reviewed the City's plan for their community and are not in favor of that plan. They ask City Council not to approve the U.S. 33 corridor plan as this time. They realize that the City has been involved with the planning process for some time and has obtained input from the Dublin community. However, the City has not contacted the residents in the US 33 corridor or taken into consideration the impact of Dublin's community plan on their homes. With the proposed plan, three neighborhoods – Frazier Road, Weldon Road, and the area of Warner Road and Mitchell DeWitt would be completely replaced with high density housing, high density office, a town center, a village center and extensive acres of low density office. How can the City make plans for this land without consideration of and without the input of the present owners? Even though this plan is a projection of the possible development within the next 15-30 years, planning to eliminate their homes and develop at such intensity is not reasonable. The present homeowners moved into the community, and built or purchased these homes in large open spaces for a reason. To see their future alternatively planned in this manner is disheartening. Their concerns are as follows:

1. They have not been provided the opportunity to voice their opinions. They are not Dublin residents so do not receive the Dublin newspapers.
2. The Dublin draft community plan is not consistent with the Jerome Township draft community plan, the southeast corridor plan, or the 1997 Union County Comprehensive Plan. Therefore, this is not what Jerome Township wants for their community.
3. They understand Dublin prefers compatible uses, however, elimination of their homes and replacing them with new compatible uses is not reasonable.
4. They are the families who will be forced to feel the impact of the Dublin tech park initiative, yet they have no voice in this plan. Why is Jerome Township the proposed area to handle hundreds of acres of high density housing and office?
5. Why are their homes not recognized on the maps, as every other Dublin home is? At their last meeting, Council indicated that the Ponderosa renters, not homeowners, should be recognized on the Plan's maps. Do the U.S. 33 corridor homeowners not have the same value?
6. Why are their communities not planned for in such a way that they can remain in their homes and continue to experience the life they all moved there to live? The City's plan calls for smart growth principles. Is there not room within the smart growth principles for larger tracts of land or open space or a right not to live in dense housing surrounded by retail and office?
7. If this plan is approved as is, the City has cost them both their way of life and their money. They are completely trapped by this plan. If they stay, they lose everything they moved to this community to achieve and experience. If they try to hurry up and sell their homes, the City has lowered the value of their homes. Who would want to buy their homes, knowing what Dublin has planned for that land?
8. As Mrs. Boring indicated in the discussion regarding the O'Shaughnessy Hills plan, there is a fear factor involved for the homeowners. That is the situation for the U.S. 33 corridor homeowners as well. The US 33 corridor plan impacts a greater number of homes than the O'Shaughnessy plan.

In summary, they ask that Council not approve the plan for the US 33 corridor area, and that the homeowners be involved in future decisions. He urges the City to work with the township trustees and residents in any planning for this area.

Mayor Chinnici-Zuercher stated that tonight Council would accept public comments on any of the other proposed area plans. Council will discuss those plans at the next Council meeting. Council has seen a couple of email communications between staff

community to simply support others to use Glick Road or Memorial Drive as a cut-through to get to Powell instead of using Home Road, which should be the main east-west connection according to MORPC. He said there will be sufficient east-west connectivity, and the community should not be destroyed to support development outside Dublin. He said that roads should be kept narrow because the wider you build them, the more traffic will be attracted. Mr. Saneholtz said that the engineering answer is not always the correct answer for aesthetic and livability reasons.

Mr. Gerber confirmed that consideration #18 should be included.

Mr. Gerber confirmed that #19 should be added to identify the existence of the Ponderosa and to eliminate the planning model found in the Community Plan.

Mr. Fishman clarified that was just for that part of the acreage. He said that there is a bigger part of the site, but they are referring to the portion that is just the Ponderosa Park.

Mr. Gerber said that the consideration proposed for the Memorial Drive Bridge should be "...to eliminate the Memorial Drive Bridge from the Community Plan."

Ms. Amorose Groomes said that all of this is because of what we have been educated. She said if we do it for one issue, it could potentially communicate to the rest that the other items were not as a result of community input.

Mr. Gerber confirmed that the Memorial Bridge language was acceptable. He said that #17 was to have an annual joint meeting between Planning Commission and City Council to review and evaluate the Community Plan, and that #19 was to identify the existence of Ponderosa Park and to eliminate the planning model from the proposed Community Plan.

Motion and Vote

Mr. Gerber made a motion to provide a positive recommendation for this Administrative Review of Ordinance 58-07 and the 2007 Dublin Community Plan to Dublin City Council for a public hearing and final vote, with 19 considerations:

- 1) That bikepath connectivity be increased between neighborhoods east of the Scioto River (new paths and missing segments) and provide more connections for pedestrians across the Scioto River;
- 2) That a phasing plan/timetable for the construction of needed connections in the bikepath network be established;
- 3) That the City work with the City of Columbus to resolve transportation issues along the Sawmill Road corridor, particularly in the areas of Bright Road and Billingsley Road, and at the O'Shaughnessy Dam;
- 4) That bike lanes be incorporated into future road construction projects;
- 5) That other alternatives to access Dublin's interstate system be considered, as well as other alternatives to cross the Scioto River;
- 6) That traffic issues relating to the O'Shaughnessy Hills Area Plan be reviewed;
- 7) That bikepath connectivity in the Hyland-Croy Road area be increased to provide greater access to schools and parks;
- 8) That coordination between jurisdictions and provide additional language be provided in the plan to describe how Dublin will communicate/coordinate with other jurisdictions;

- 9) That setbacks be maintained along Hyland-Croy Road;
- 10) That focus be provided on smaller areas such as Sawmill Road and Hyland-Croy Road for coordinated planning with other jurisdictions;
- 11) That increased communication be provided about the flexibility and purpose of the Community Plan, including the identification of existing properties and an explanation of the Plan's intentions;
- 12) That a list of "Property Owner's Rights" be included in future brochures about the Community Plan and within the Community Plan document;
- 13) That existing neighborhoods (i.e. Ponderosa) be acknowledged and indicate them on the Plan;
- 14) That rural characteristics be recognized as an important component of the City's identity;
- 15) That a consistent greenbelt/open space system be maintained throughout the City;
- 16) That care is used in the redevelopment of Historic Dublin to link all areas of the District and maintain appropriate massing, scale, materials and character with vernacular architecture;
- 17) That an annual workshop or joint meeting of City Council and the Planning and Zoning Commission be held to review and evaluate the Community Plan;
- 18) That the Memorial Drive bridge be eliminated from the Plan; and
- 19) That the area plan be modified to identify the Ponderosa and eliminate the proposed subarea for that site.

Mr. Fishman seconded the motion. The vote was as follows: Mr. Sanholtz, yes; Ms. Amorose Groomes, yes; Mr. Zimmerman, yes; Mr. Fishman, yes; and Mr. Gerber, yes. (Approved 5 – 0.)

Mr. Gerber announced that the second reading of the Community Plan is slated for October 15, at 7 p.m., in Council Chambers before City Council. He asked that those interested attend and said that everybody's involvement is what makes Dublin great. He said it was not just the Commission or City Council trying to figure out what to do. Mr. Gerber said the reason why we are such a great community is because we go to great lengths to talk with one another and try to work together. He said that was what a community was all about. He thanked the residents for their comments and contributions over the three year process. He noted that staff has done a fantastic job with all of the hours that have been involved and listening to the Commission talk and debate. He thanked staff and everyone on the Commission for their dedication through out the August meetings and the many Joint Work Sessions that were held. He said he was proud to say that the Commission's attendance was fantastic.

Administrative Business

Mr. Gerber made a motion to cancel the October 4, 2007 Commission meeting and that the only October meeting will be on the 11th at 6:30 p.m. Mr. Fishman seconded the motion. The vote was as follows: Mr. Zimmerman, yes; Ms. Amorose Groomes, yes; Mr. Sanholtz, yes; Mr. Fishman, yes; and Mr. Gerber, yes. (Approved 5 – 0.)

The meeting was adjourned at 7:45 p.m.

Respectfully submitted,

Flora Rogers and Libby Farley
Administrative Assistants

Additional Options:

- Add design note to area plan concepts that encourage increased connectivity

2) *Establish a phasing plan/timetable for the construction of needed connections in the bikepath network*

Considerations:

As a specific implementation process, the establishment of phasing plans and specific timetables are not addressed as part of the Community Plan. Individual projects that are targeted for design and construction are included as part of the Capital Improvements Program adopted by City Council. Council has provided direction to speed the completion of important bikepath projects, and prioritization and funding allocation are ongoing.

Additional Options:

- Add additional strategy to Objective #11 on page 185 to address the expedited pursuit of programming for key bikepath segments

3) *Work with the City of Columbus to resolve transportation issues along the Sawmill Road corridor, particularly in the areas of Bright Road and Billingsley Road, and at the O'Shaughnessy Dam*

Considerations:

Regionalism plays a major policy role in the Plan. As part of Chapter One, "regional cooperation" is specifically noted on page 30 as one of the major building blocks of the Community Plan. Objective 9 in the Transportation Chapter (pp. 183-184) notes that the City should "...work cooperatively with surrounding jurisdictions to coordinate regional transportation planning and programming." Associated strategies also specifically note that Dublin should aggressively explore bridge locations outside Dublin with surrounding jurisdictions and should encourage Columbus and State of Ohio officials to improve the Sawmill Road Corridor north of I-270.

Additional Options:

- Modify the transportation strategy to specifically include the O'Shaughnessy Dam as a bridge alternative for further consideration.
- Add design recommendation to the Bright Road Area Plan to specifically note the need to work toward improvements in the Bright Road and Billingsley Road area.

4) *Incorporate bike lanes into future road construction projects*

Considerations:

The City is currently considering the feasibility and design implications for integrating bikelanes into future roadway projects. Objective 11 on page 185 call for the promotion of bicycle and pedestrian mobility throughout the City. Particular strategies recommend that bikepaths and bike lanes should be considered as integral parts of the roadway design process.

Additional Options:

Mr. Gerber said that congestion has always been a major problem. He said that staff and Council should be encouraged to be mindful of solutions in the Bright Road Area and to discuss options with other jurisdictions. He said that this part of Dublin is paying the brunt of problems and perhaps other plans should be considered in this area.

Mr. Fishman said Dublin needs to work closely with Columbus to help ease the problem on Sawmill and in the Smokey Row and I-270 area.

Mr. Walter said the Billingsley – Bright Road debacle should be a focus area for the City. He said that the plan appears to change traffic patterns and tends to suggest an immovable partner in Columbus. He said that ways to work with Columbus should be sought to solve the problem and that there are options available. He questioned the connection on Bright Road between Sawmill Road and Emerald Parkway.

Mr. Zimmerman said Emerald Parkway is a key factor that once completed will allow other elements to be completed. He said it will connect both sides of the river and destinations. He said it will allow the neighborhoods to get more connections.

Mr. Walter agreed that Emerald Parkway is important, but that enough alternatives to cross the river and to get to the interstate have not been considered through the process. He said Council should use efforts to make better access.

Mr. Saneholtz said that the plan works to provide greater connection across the river to Historic Dublin. He said that with the Dublin Village Center he is concerned about the idea of additional retail. He said the correspondence from residents is that Dublin does not need more rental property in that location. Mr. Saneholtz said owner occupied housing may bring the younger generation into the area. He said the area has a lot of potential and someone will come along with a great idea. He said that there should be more open space and pedestrian connection.

Mr. Walter said the O'Shaughnessy plan should potentially be reviewed for traffic issues. He said that over time the area should be connected with the rest of the city and that decisions have been made to close connectivity with new development. He gave Bryson Cove as an example.

Ms. Amorose-Groomes said a unified effort would be needed for developers in that area, but the plan is a stretch.

Mr. Gerber said that there should be a balance between moving traffic and preserving neighborhoods and protecting character.

Ms. Amorose-Groomes said that making the connection on Macbeth Drive would have been a disaster because of the setbacks in that area and resulting cut-through traffic. She said that connectivity should not be made just for connectivity's sake.

high densities will be considered to facilitate the redevelopment of the Dublin Village Center as long as quality architecture and design are provided.

Coffman Park Area

Participants agreed that a combination of office/commercial development (without retail) should be incorporated along the Emerald Parkway and I-270 frontage. Parkland should be integrated, and surface parking should be minimized. There was a detailed discussion about the configuration of Post Road will be deferred until modeling results are known. No decision was made as to the location of City Hall.

Historic Dublin Area

Improved pedestrian connections should be made to the river with additional spaces and plazas as overlooks that are more removed from Bridge Street. Interest in boardwalks and a pedestrian bridge should be explored. Ms. Rauch noted that the block system was supported and staff will reevaluate signalization at Bridge and High Streets. Traffic patterns on Franklin Street will also be considered. Significant discussion about the library and cemetery occurred, but decisions will be deferred until a later date.

Northeast Area

Discussion on the Bright Road area resulted in concerns about natural features, an historic cemetery and the Holder-Wright Works. Ms. Rauch noted that staff was instructed to modify plans to indicate parkland on both the Holder and McDowell properties.

Ms. Rauch concluded her summary and indicated that comments were received from Cathy Boring, who was not present this evening. She said that Ms. Boring agreed with the preservation of park areas in the Bright Road area and noted a desire for additional buffering between residential uses on Bright Road and office development. She said that Ms. Boring also discussed providing more office component at the southeast corner of the area plan and the potential for some support services. Ms. Rauch explained that Ms. Boring indicated that proposed plans in the Summitview area addressed concerns raised at the last meeting about retail and that the residential and office mix was more in line with prior comments. She also said that Ms. Boring was concerned about protecting the ravine at O'Shaughnessy Hills and wanted sufficient buffering with Wedgewood Hills. Comments also included limiting access on Riverside Drive for safety. Ms. Rauch concluded by reminding members that comments should be provided by email or in writing for distribution to the Work Session participants.

Amy Salay asked for confirmation about the designation of parkland in the Bright Road area. Carson Combs clarified that areas south of the ravine on the Holder and McDowell properties are designated as office (fronting onto Emerald Parkway). The ravine and areas to the north on both properties will be noted as future parkland.

Ms. Salay noted that many issues have been deferred due to modeling issues, but questioned whether or not decisions will need to be made to move the process forward.

Ms. Rauch said that staff can proceed at this time, but that ultimately decisions will need to be made. Mr. Combs added that upcoming discussions will need to include general concepts such as scenic roads and character. More specific issues such as the location of a city hall or library is more entwined with the transportation model and answers will not be possible until next year. He said that upcoming discussions will lay the groundwork for those decisions.

Mike Keenan emphasized that the transportation model must be completed to make key decisions. Mr. Combs agreed and asked for additional questions. (No questions were asked.)

Public Input/Area Plan Discussion

Mr. Combs stated that over 115 people participated in the June open house and full reports were provided to City Council and the Planning Commission that include verbatim comments. He proceeded to discuss area plans.

Summitview & Sawmill Area

Mr. Combs said that no specific plan was completed for this area in 1997 and that the Future Land Use Map denoted the entire area as “Mixed Use Employment Emphasis.” Concepts developed in 2005 included the realignment of Summitview Road with mixed neighborhood uses that follow the ten Land Use Principles. He said that the 2006 draft conceptually shows one-story office with residential development that would transition to Glenree Place while providing a greenway corridor that will link with nearby parkland. Mr. Combs explained that the latest draft attempts to minimize traffic congestion and provides a layout that minimizes cut-through traffic.

Ms. Salay asked if the power line was on the western edge of the area in question, and Tim Lecklider asked what the acreage for the area is. Mr. Combs indicated that the high-tension lines do run along the west edge of the site, but the exact acreage was not known.

Mr. Lecklider asked for the proposed density of the concept and what type of character was proposed. Mr. Combs said that the residential density of the concept was approximately two du/ac. and that zonings for such developments are generally in the range of 9,000 square feet per acre. He noted that the character of the development is more of a typical suburban pattern.

Warren Fishman asked why so many homes were being proposed with the office when that use generates revenue for the City. He said that no one wants more retail along Sawmill Road and office would be a more ideal situation. John Reiner agreed.

Mr. Combs noted that these issues are the point of discussion. He said that staff is trying to mitigate future traffic impacts, and placing significant office development along Sawmill will further congest Sawmill Road and Summitview Road. He asked participants to suggest an appropriate balance.

Mr. Lecklider questioned the style and layout of the proposed residential uses. Mr. Reiner and Mr. Fishman agreed.

Mr. Fishman said that the office will create traffic only twice a day and that it will be less obtrusive than housing.

Mr. Lecklider suggested the incorporation of townhomes and a layout that was not a typical suburban pattern.

Jamie Greene noted that in earlier discussions, concepts did show a mix of housing types with a traditional neighborhood pattern that included mixed use, small retail, and a realignment of the road. Mr. Greene said the previous concept provided a greater mix of housing.

Bright Road Area Plan

Mr. Combs referred the Joint Work Session to the plans within the packets. He pointed out the future Emerald Parkway extension, recent parkland acquisition and existing floodplain areas. Mr. Combs mentioned the expected widening of Bright Road east of Emerald Parkway and its impact on future land uses. He pointed out the ravine west of Riverside Drive and the Indian mounds off Bright Road. He concluded with other site issues and noted that access and safety was previously discussed.

Mr. Combs said that the plans intend to preserve key natural features and to maintain the residential character along Bright Road. The plans also continue the high quality design and incorporation of offices along Emerald Parkway. He said that the concepts give the general expectations for future development with buildings closer to the street, internalized parking lots and appropriate landscaping and buffering. He said that the concept is very similar to the 1997 Community Plan and looks at clear pedestrian connectivity and providing an architectural appearance at the interchange.

Ms. Boring raised concerns about placing offices behind the Village of Inverness and isolating that development from other residential uses in the area.

Mr. Combs said that the issue has not been raised through public input, but can be considered further.

Ms. Boring suggested swapping proposed office and residential areas along Bright Road. She said that previously a development proposal at the interchange had proposed larger office and the Traffic Impact Studies failed. She asked if that was taken into consideration.

Mr. Combs said that past studies were not considered and that the particular parameters by which that study was carried out is not known. He said that the plan includes considerations for the completion of Emerald Parkway and the widening of Bright Road from Emerald Parkway to Sawmill Road. He said he is not familiar with the particular segments of Emerald Parkway that were completed at that time.

Ms. Boring said that the intersection at Bright Road and Sawmill Road failed.

Ms. Salay asked whether all of the traffic is being modeled off of the land uses. She recalled reducing densities and changing uses to match the capacities of the transportation network.

Mr. Combs said that the uses have been included in the modeling. He said that a final iteration of the model will be completed to incorporate any adjustments made in the area plans. Mr. Combs said the process is iterative and that all of the land uses have been looked at through the transportation and fiscal models. He said that final adjustments will be made between completion of the final draft and adoption.

Mr. Fishman said that evolution in office space and its use should be considered. He said phone banks have a substantially higher employment density than traditional offices. Mr. Fishman said that the new uses need to be examined in terms of traffic and parking.

Ms. Boring said that the area is ready for nice offices. She said she wants to ensure the plan is on target to allow such development in the future.

Mr. Sanholtz voiced concern to maintain pedestrian connections to Lifetime Fitness and the High School.

Mr. Gerber asked if the area plan is consistent with existing zoning.

Mr. Combs said that the Area Plan does not consider existing zoning. He said that parcels in the area are primarily zoned R-1 or equivalent.

Mr. Zimmerman asked about the unmarked cemetery and asked for the rough location.

Mr. Combs said that the land is located near the Arts Council along Riverside Drive and that a sign has been erected at its location. He said that the location is generally known, and Ms. Salay added that there have been surveys completed in the past by OSU or other entities.

Mr. Zimmerman asked if the cemetery would be fenced in and the graves marked.

Mr. Combs said that Parks and Recreation would look at design issues as part of the park development plans for the site.

Ms. Salay asked about the timetable for installing a cul-de-sac on Bright Road.

Mr. Combs said he was not aware of specific timing and indicated that the final segment of Emerald Parkway would be needed.

Mr. Gerber said that it will be tough to coordinate publishing the Community Plan with the results of the modeling. He said that the same methods may need to be employed as with the 1997 Plan.

Mr. Greene said that there should not be any significant issues to deal with unless the Area Plans are significantly changed.

Ms. Boring said that the densities are a factor, but the land uses are on target.

Coffman Park Area

Mr. Combs said that City Council has taken action on the Post Road issue since the last discussion. He said that Post Road will be redirected to Commerce Parkway. He said that the major planning issue was the "bowtie" area between Emerald Parkway and I-270.

Ms. Salay said the option would have to be in the context of economic development and whether funding would be available to finance. She said it merits further consideration to see the benefits of the transportation network and commended staff for looking at other options.

At the request of Ms. Boring, Ms. Willis explained the scenario would improve the traffic situation for Willow Grove because much of the business traffic is being removed.

Ms. Boring asked which properties would be impacted to create the east-west road, and Mr. Combs noted OCLC and Cardinal Health comprise the bulk of the land, and there is not enough detail yet to know the impacts to existing businesses along Post Road. He stressed staff is at a broader modeling level at this time.

Mr. Phillabaum said discussion of this option has not yet occurred with Cardinal and OCLC. He said a connection to benefit existing and future Cardinal facilities, as well as OCLC, is being studied. However, study is not at a detail level to determine an actual alignment, but there is ongoing discussion to determine a solution that will address access and traffic issues.

Mr. Sanholtz said just creating a “T” intersection at Dublin Road will take pressure off of Historic Dublin.

Vice Mayor Lecklider summarized the option would be kept for further modeling consideration. [No one disagreed].

Bright Road Options

Mr. Martin showed an exhibit showing Bright Road from Riverside Drive to Sawmill Road. He said the extension of Emerald Parkway will improve traffic in the future and under consideration is which end of Bright Road to disconnect. He said if the Riverside Drive end is removed, approximately 100 cars in the peak hour will go elsewhere in the network, but will not really impact other areas and disconnecting the Emerald Parkway end will still result in LOS “F” regardless. He explained the option at Riverside Drive is recommended because of the high injury crash rate at 52%.

Ms. Boring asked if a cul-de-sac could be placed at both ends of Bright Road. She suggested a cul-de-sac east of Emerald Parkway. [Discussion ensued]. She said the latest preference of the residents was to cul-de-sac at Riverside Drive for safety. Ms. Willis confirmed the preference.

Mr. Combs noted the current Community Plan indicates a cul-de-sac at the east end of Bright Road, but due to safety reasons both options are being considered to see which has more benefit to the network.

Vice Mayor Lecklider said the recommendation would be to place the cul-de-sac on the west side of Bright Road. Ms. Willis clarified Bright Road should include a cul-de-sac at the west side near Riverside Drive and no other locations will be considered. [There was no additional comment].

Mr. Messineo thought others would like a more prominent location, but it was not important. He said the location at Riverside and SR 161 is a prime commercial spot – he would like to see an entertainment venue.

Mr. McCash said the site was a commercial location. He said he would place City Hall between Post Road and SR 161.

Mr. Keenan said the group should be cognizant of using up prime commercial areas.

Mr. Greene acknowledged Dublin's success through using commercial property wisely. He stated that the fiscal status of the City does not hinge on this one area and noted that other communities have a civic gesture in the center of town and that economics should not be the only criteria.

Mr. Keenan said he is interested in what other comparable communities are doing. The City should be careful with its tax dollars.

Ms. Chinnici-Zuercher said the new City Hall, regardless of its location, is intended to bring everybody under the same roof. She said it will be grandiose by size but should not be too ornate.

Ms. Salay agreed that acquiring land just for the City Hall might garner a negative reaction.

Mr. Greene challenged the group to think whether or not the site is a piece of key real estate and whether they want to see a civic use.

[The group shook their heads affirmatively, indicating that they wanted this to be a civic area.]

Northeast Quad Area Discussion.

Jeremy Rowan, ACP, showed a slide of the 1997 Bright Road Area Plan. He noted that public feedback indicated that substantial work was done to create the 1997 Plan and little adjustment was warranted. He described the draft plan, noting areas of office along I-270, the extension of Emerald Parkway, and buffers to the single-family neighborhoods. Mr. Rowan described proposed changes along Bright Road that included preservation of archeological and natural resources and the incorporation of residential development.

Mr. McCash voiced concerned about not impacting the Indian mounds.

Mr. Rowan explained that drawing shows a concept that is not to scale. The idea is to preserve the mounds with open space that connects to the ravine, while allowing for development on the adjacent site.

Ms. Boring asked why development was being forced there and asked if the City had applied for grants to preserve this historic area.

Ms. Salay said there have been excavations and that the family desires to keep the area preserved as open space. She said she did not think any type of development could be built.

Mr. Greene asked if the group wanted the area (Holder and McDowell Properties) designated as park.

Mr. McCash said they were expecting a park use with an area designated for a museum or visitor's center. He did not want multi-family uses.

Mr. Greene asked if it would be either park or civic uses.

Mr. McCash agreed.

Ms. Brautigam clarified that there were two property owners in the area. Staff has been working with the Holder family to preserve the site. The McDowell property is not included in the City acquisition requests. Ms. Brautigam said part of the McDowell property is possible for development, and clarifications will be made to the plans.

Mr. Greene asked what land use is appropriate for the balance of the area not within the Holder site.

Ms. Salay said she thought development was on the south side of the creek.

Mr. Reiner wanted to know if that area could be disturbed since it was one of the Hopewell Indian sites.

Mr. Greene explained that the plan intends to protect the Indian mound site, but that it sounded as though the whole area north of the ravine was something Council would like to protect.

Mr. Rowan noted areas to the south of the ravine are proposed for office uses, similar to the 1997 Plan. He then described other areas of the plan and noted that portions along Emerald Parkway south of Hard Road were shown as single-family consistent with the 1997 Plan.

Ms. Boring asked if there was enough room for single-family uses, and Ms. Chinnici-Zuercher asked for the acreage.

Mr. Rowan was unaware of the acreage.

Mr. McCash stated that only a right-in/right-out curb cut would likely be allowed on Hard Road. He questioned the single-family use.

Mr. Greene explained that is how the 1997 Plan currently designates the area.

Ms. Chinnici-Zuercher said Lifetime Fitness and other development was not known at the time, and the context should be taken into consideration.

Mr. McCash said a park buffer or something smaller was needed, even if it was a single-story office similar to the west side of Llewellyn Farms.

Ms. Boring noted the stand of trees on the site and asked where power lines are located.

Mr. Greene said realistically the site will not be single-family housing with all of the woods. The power lines are also located along the back of the lots.

Ms. Boring commented that today they would not place as many curb cuts on Hard Road. She asked if small areas of commercial support are factored into the office uses.

Mr. Rowan agreed that land uses are placed into the model; there is a component of supportive retail assumed (for “mixed use employment emphasis”).

Mr. Sanholtz asked if the additional residential proposed south of Bright Road was to keep existing homes from being on an island next to commercial.

Mr. Rowan explained that the public expressed that new residential be added as a buffer next to future offices.

Summitview and Sawmill Discussion

Mr. Rowan introduced the plan and described an option to realign Summitview at Sawmill. The 1997 Plan recommends mixed-use employment and the proposed plan would provide office at the corner with supportive retail that could serve both businesses and residents. Buffers along Sawmill Road are provided that create an open space link to the park on Summitview Road, as well as providing separation for the existing substation. Additional residential uses are provided for transition between the mixed-use and existing single-family. Architecture at the corner will be two-story and transition into the residential uses.

Ms. Chinnici-Zuercher requested the number of commercial acres located between Summitview and Sawmill Roads, and Mr. Greene said they would look into the number. [His later reply was 12 to 15 acres]

Ms. Boring said that Summitview Road was designated as rural, and that is the reason why Hard Road was built. She said there is no way the City wants to create a cut-through for traffic to go through Summitview. She stated that the proposal was unacceptable.

Mr. Rowan asked for clarification as to whether the road alignment was unacceptable.

Ms. Boring said the realignment is a huge burden that should not be considered.

Ms. Salay said she was frustrated that the group is not given more time to think about the concepts, much less get the reaction of residents. She requested that packets be provided in advance with an explanation and phone number to call if they had questions. Ms. Salay shared concerned that there were different ideas for Summitview Road.

Mr. Green clarified that the group was given a notebook in which this information was provided.

Ms. Salay said she did not recall hearing this information at the public meetings and noted that Ms. Boring had not based upon her reaction.

**FOR
REFERENCE
ONLY**

GENERAL NOTES

- City of Columbus and Ohio Department of Transportation Construction and Material Specifications, current editions, and any supplements thereto (hereafter referred to as Standard Specifications), shall govern all construction items unless otherwise noted. If a conflict between specifications is found, the more strict specification will apply as decided by the City Engineer. Item Numbers listed refer to City of Columbus Item Numbers unless otherwise noted.
- The City Engineer will not be responsible for means, methods, procedures, techniques, or sequences of construction that are not specified herein. The City Engineer will not be responsible for safety on the work site, or for failure by the Contractor to perform work according to contract documents.
- The Developer or Contractor shall be responsible to obtain all necessary permits including but not limited to Ohio EPA Permits to Install (PTI) and Notices of Intent (NOI), Building Permits, etc.
- The Contractor shall notify the City of Dublin Division of Engineering in writing at least 3 working days prior to beginning construction.
- The Contractor shall be solely responsible for complying with all federal, state and local safety requirements including the Occupational Safety and Health Act of 1970. The Contractor shall exercise precaution always for the protection of persons (including employees) and property. It shall also be the sole responsibility of the Contractor to initiate, maintain and supervise all safety requirements, precautions and programs in connection with the work, including the requirements for confined spaces per 29 CFR 1910.146.
- Following completion of construction of the site improvements and before requesting occupancy, a proof survey shall be provided to the Division of Engineering that documents "as-built" elevations, dimensions, slopes and alignments of all elements of this project. The proof survey shall be prepared, signed and submitted by the Professional Engineer who sealed the construction drawings.
- The Contractor shall restrict construction activity to public right-of-way and areas defined as permanent and/or temporary construction easements, unless otherwise authorized by the City Engineer.
- The Contractor shall carefully preserve bench marks, property corners, reference points, stakes and other survey reference monuments or markers. In cases of willful or careless destruction, the Contractor shall be responsible for restorations. Resetting of markers shall be performed by an Ohio Professional Surveyor as approved by the City Engineer.
- Non-rubber tired vehicles shall not be moved on or across public streets or highways without the written permission of the City Engineer.
- The Contractor shall restore all disturbed areas to equal or better condition than existed before construction. Drainage ditches or water courses that are disturbed by construction shall be restored to the grades and cross-sections that existed before construction.
- Tracking or spilling mud, dirt or debris upon streets, residential or commercial drives, sidewalks or bike paths is prohibited according to Section 37.38 of the Dublin Code of Ordinances. Any such occurrence shall be cleaned up immediately by the Contractor at no cost to the City. If the Contractor fails to remove said mud, dirt, debris, or spillage, the City reserves the right to remove these materials and clean affected areas, the cost of which shall be the responsibility of the Contractor.
- Disposal of excess excavation within Special Flood Hazard Areas (100-year floodplain) is not permitted.
- All signs, landscaping, structures or other appurtenances within right-of-way disturbed or damaged during construction shall be replaced or repaired to the satisfaction of the City Engineer. The cost of this work shall be the responsibility of the Contractor.
- All field tile broken or encountered during excavation shall be replaced or repaired and connected to the public storm sewer system as directed by the City Engineer. The cost of this work shall be the responsibility of the Contractor.
- All precast concrete products shall be inspected at the location of manufacture. Approved precast concrete products will be stamped or have such identification noting that inspection has been conducted by the City of Columbus. Precast concrete products without proof of inspection shall not be approved for installation.
- Backfill within a 1:1 influence line of existing structures (houses, garages, etc.) or public infrastructure (pavement, curbs, sidewalks, bike paths, etc.) shall be compacted granular backfill according to Item 912 of the Standard Specifications or Flowable CDF, Type III according to Item 636. Item 911 of the Standard Specifications may be used elsewhere.
- The Contractor shall submit a copy of the approved construction drawings and a list of proposed precast concrete product manufacturers to the City of Columbus Construction Inspection Division before commencing construction.

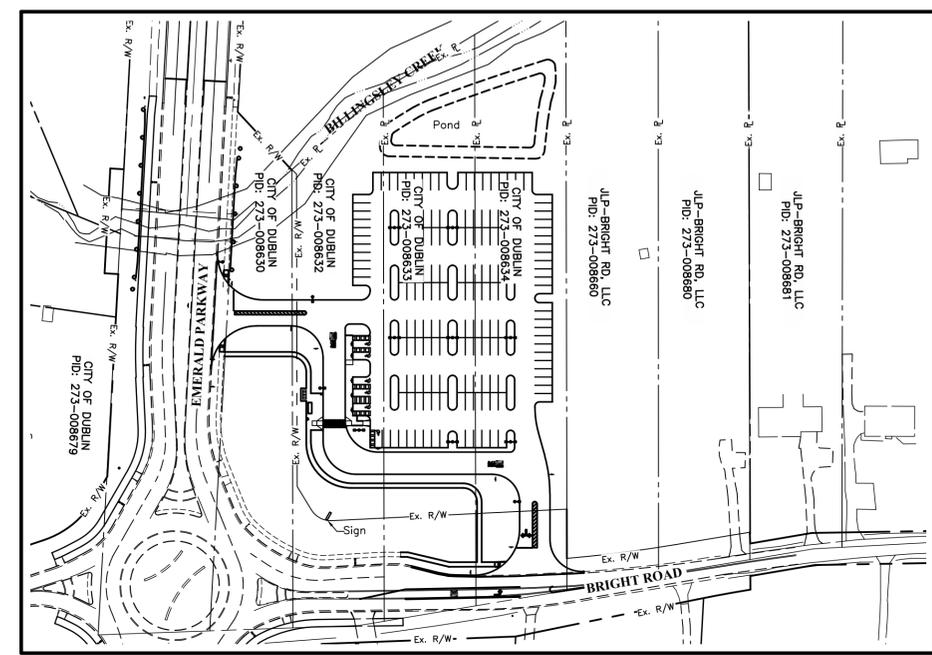
- Send the information to the following address:
 Construction Inspection Division
 City of Columbus
 1800 East 17th Avenue
 Columbus, Ohio 43219
- Send a copy of the transmittal letter to the following address:
 Division of Engineering
 City of Dublin
 5800 Shier Rings Road
 Dublin, Ohio 43016
- All trenches within public right-of-way shall be backfilled according to the approved construction drawings or securely plated during nonworking hours. Trenches outside these areas shall be backfilled or shall be protected by approved temporary fencing or barricades during nonworking hours. Clean-up shall follow closely behind the trenching operation.
 - All trees within the construction area not specifically designated for removal shall be preserved, whether shown or not shown on the approved construction drawings. Trees to be preserved shall be protected with high visibility fencing placed a minimum 15 feet from the tree trunk. Trees 6-inches or greater at DBH (Diameter Breast Height) must be protected with fencing placed at the critical root zone or 15 feet, whichever is greater. Trees not indicated on the approved construction drawings for removal may not be removed without prior approval of the Division of Engineering.
 - Conduit must be directionally bored across streets instead of open cut, unless specifically approved by the City Engineer. Use of pneumatic air ram devices is not permitted. Permits to construct in the right-of-way of existing streets must be obtained from the City of Dublin Division of Engineering before commencing construction. Should open cutting of existing pavement be permitted, Controlled Density Backfill (Type III) shall be used in place of compacted granular backfill, according to Item 636 of the Standard Specifications.
 - The Contractor shall be responsible for the condition of trenches within the right-of-way and public easements for a period of one year from the final acceptance of the work, and shall make any necessary repairs at no cost to the City.
 - Pavements shall be cut in neat, straight lines the full depth of the existing pavement, or as required by the City Engineer. Pavement replacement shall be conducted according to City of Columbus Standard Drawing 1441 and applicable City of Dublin standard drawings. The replacement of driveways, handicapped ramps, sidewalks, bike paths, parking lot pavement, etc. shall be provided according to the approved construction drawings and City of Dublin standard construction drawings.
 - Tree trimming within the construction zone is to be completed by a certified Arborist. At the completion of the project the Arborist is to return and trim any broken branches as needed.
 - Any modification to the work shown on drawings must have prior written approval by the City Engineer, City of Dublin.
 - All inlets shall be channelized.
 - Park areas shall be fine-graded and seeded with the following mixture:
 Improved Kentucky Bluegrass, 40% of weight (2 varieties in equal parts)
 Improved Perennial Ryegrass, 60% of weight (2 varieties in equal parts)
 Germination Rate: 85%
 Application Rate: 7 lbs per 1000 sq ft as directed by the Division of Parks & Recreation, City of Dublin, Ohio.
 - Traffic control and other regulatory signs shall be Type S with a square post anchor base installation and meet all requirements of ODOT TC-41.20 and applicable City of Dublin specifications.
 - Street signs shall meet all City of Dublin specifications with lettering colored in white displayed over a brown background. Sign tubing shall be brown in color and conform with the Type S, square post anchor base installation requirements of ODOT TC-41.20.

UTILITIES

- The following utilities are known to be located within the limits of this project:

Columbia Gas of Ohio Rob Caldwell - Field Engineer 1600 Dublin Road Columbus, Ohio 43212	City of Dublin Division of Engineering Ken Richardson, P.E. 5800 Shier Rings Road Dublin, Ohio 43016 (614) 410-4631	City of Columbus Division of Power and Water (Water) 910 Dublin Road, 2nd Floor Columbus, Ohio 43215 (614) 645-7677	Verizon Bill Muehler 550 Leader Street Marion, Ohio 43302 (740) 383-0527
American Electric Power Rob Sloneker 850 Tech Center Drive Gahanna, Ohio 43230-6605 (614) 883-6829	Time Warner Cable Roy Maurer 3760 Interchange Road Columbus, Ohio 43204 (614) 481-5262	AT&T of Ohio Tom Ziomek 111 North 4th Street Columbus, OH 43215 (614)223-7162	Wide Open West Jytee Novario 3675 Corporate Drive Columbus, Ohio 43231 (614) 948-4653

CITY OF DUBLIN, FRANKLIN COUNTY, OHIO PRIVATE SITE IMPROVEMENT PLAN FOR COTA PARK AND RIDE 2015



INDEX MAP
Scale: 1" = 100'

ZONING SUMMARY

Impacted Parcels:		
273-008630	0.8 Acres	
273-008632	1.2 Acres	
273-008633	1.4 Acres	
273-008634	1.5 Acres	
Total:	±4.9 Acres	
Ex. Site Area:	±3.33 Acres	
Total Impervious Area:	±1.63 Acres (49%)	
Disturbed Area:	±2.92 Acres	
Proposed COTA Park and Ride Parking Stalls:	169 Spaces	

DEVELOPER/OWNER

City of Dublin
 5800 Shier Rings Road
 Dublin, Ohio 43016
 Tel: (614) 410-4600
 Fax: (614) 410-4747

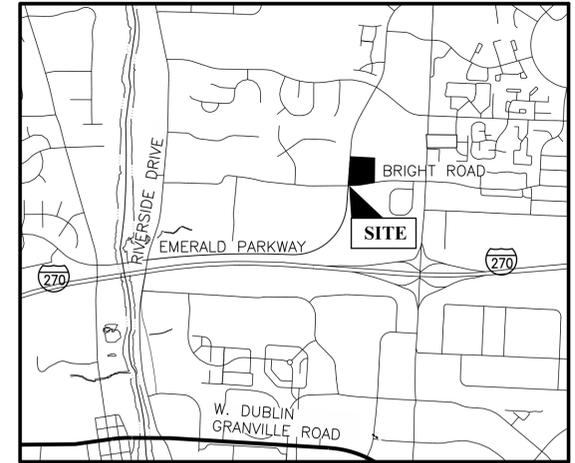
BENCH MARKS
(NAVD 1988)

- BM#1 Railroad spike in the south side of a wooden utility pole located on the north side of Bright Road, at the east entrance to the roundabout with Emerald Parkway. Elev. = 899.91
- BM#2 Chiseled "X" on the west flange bolt of a fire hydrant located on the east side of Emerald Parkway, being the first hydrant north of the roundabout with Bright Road. Elev. = 893.88
- BM#3 Chiseled "X" on the west flange bolt of a fire hydrant located on the east side of Emerald Parkway, being the second hydrant north of the roundabout with Bright Road. Elev. = 893.16

STANDARD CONSTRUCTION DRAWINGS

The Standard Construction Drawings listed on these plans are to be considered a part thereof.

City of Dublin	City of Columbus
PD-01	AA-S102
PD-02	AA-S106
PD-03	AA-S107
PD-06	AA-S112
PD-08	AA-S117
PD-11	AA-S119
RD-02	AA-S125A
RD-05	
RD-07	
ST-03	
ST-04	
ST-05	



LOCATION MAP
Not to Scale

CITY OF DUBLIN APPROVAL

The signatures below signify only concurrence with the general purpose and general location of the project and does not constitute assistance to operate as intended. All technical details remain the responsibility of the Engineer preparing the plans.

City Engineer, City of Dublin, Ohio _____ Date _____

Director of Land Use & Long Range Planning, City of Dublin, Ohio _____ Date _____

SHEET INDEX

Sheet Title	Sheet Number
Title Sheet	1
General Notes and Details	2
Existing Conditions, Tree Survey, and Demolition	3
Site Staking Plan	4
Utility Plan	5
Grading Plan	6
Storm Sewer Profiles	7
Storm Sewer Profiles	8
Erosion Control Details	9
Outlet Control Structure Details	10
Landscape Plan	11
Landscape Details	12
Lighting Plan	13

PREPARED BY:

PRELIMINARY
 NOT TO BE USED FOR
 CONSTRUCTION

PLAN SET DATE
 January 26, 2015

Registered Engineer No. 67880 _____ Date _____

MARK	DATE	DESCRIPTION



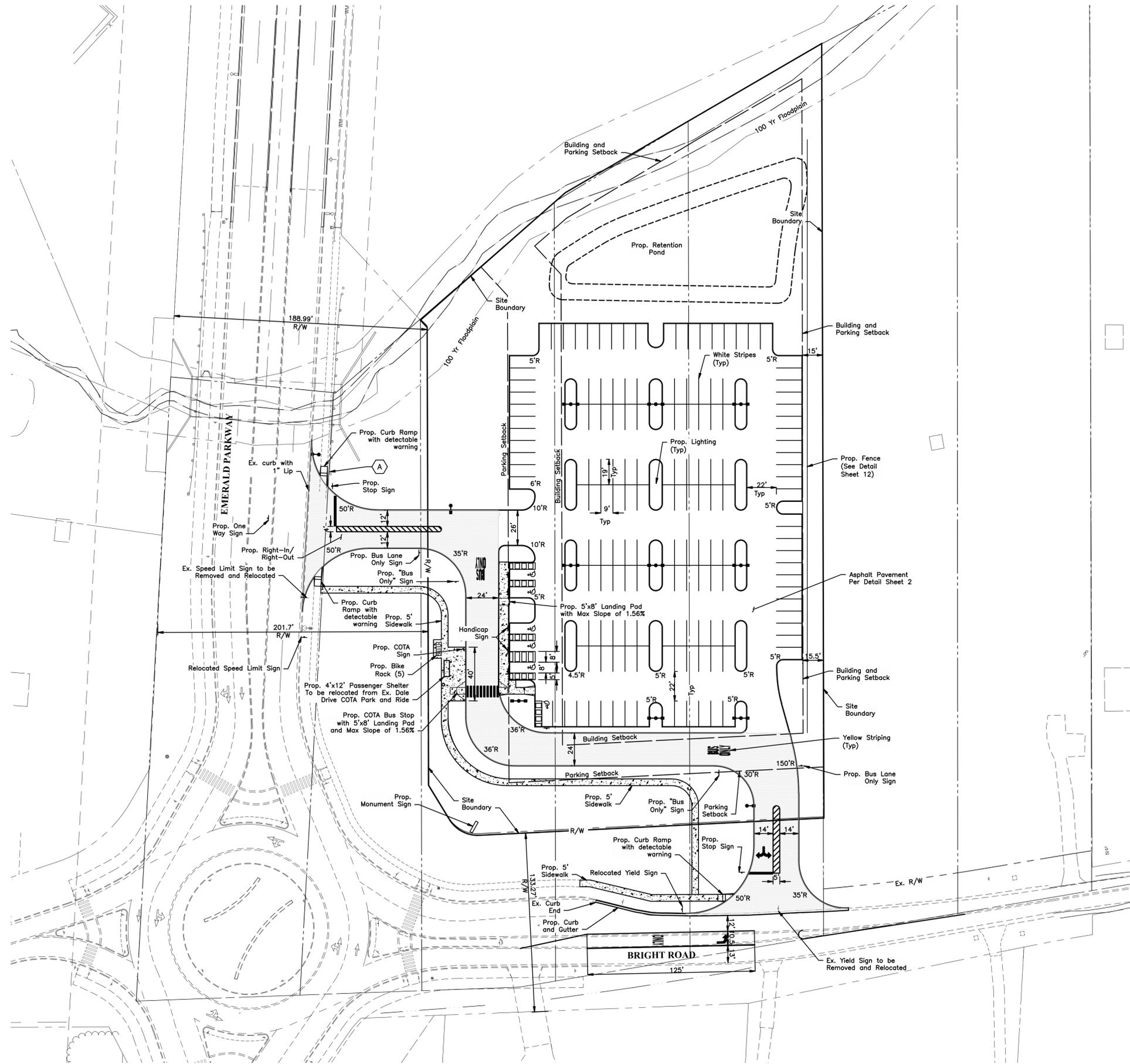
CITY OF DUBLIN, FRANKLIN COUNTY, OHIO
 PRIVATE SITE IMPROVEMENT PLAN
 FOR
 COTA PARK AND RIDE
 TITLE SHEET

DATE
 January 26, 2015

SCALE
 As Noted

JOB NO.
 2014-0588

SHEET
 1/14

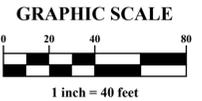


NOTE:
 (A) Existing guard rail to be reconstructed as a beginning flare per Emerald Parkway Phase 8 Plans (07-008-CIP)

NOTE:
 All Radii are 5' unless otherwise noted.

LEGEND

	Concrete Pavement, See Detail, Sheet 2
	Concrete Sidewalk, See Detail, Sheet 2



PRELIMINARY
 NOT TO BE USED FOR CONSTRUCTION

PLAN SET DATE
 January 26, 2015

MARK	DATE	DESCRIPTION



CITY OF DUBLIN, FRANKLIN COUNTY, OHIO
 PRIVATE SITE IMPROVEMENT PLAN
 FOR
COTA PARK AND RIDE
 SITE STAGING PLAN



DATE
 January 26, 2015

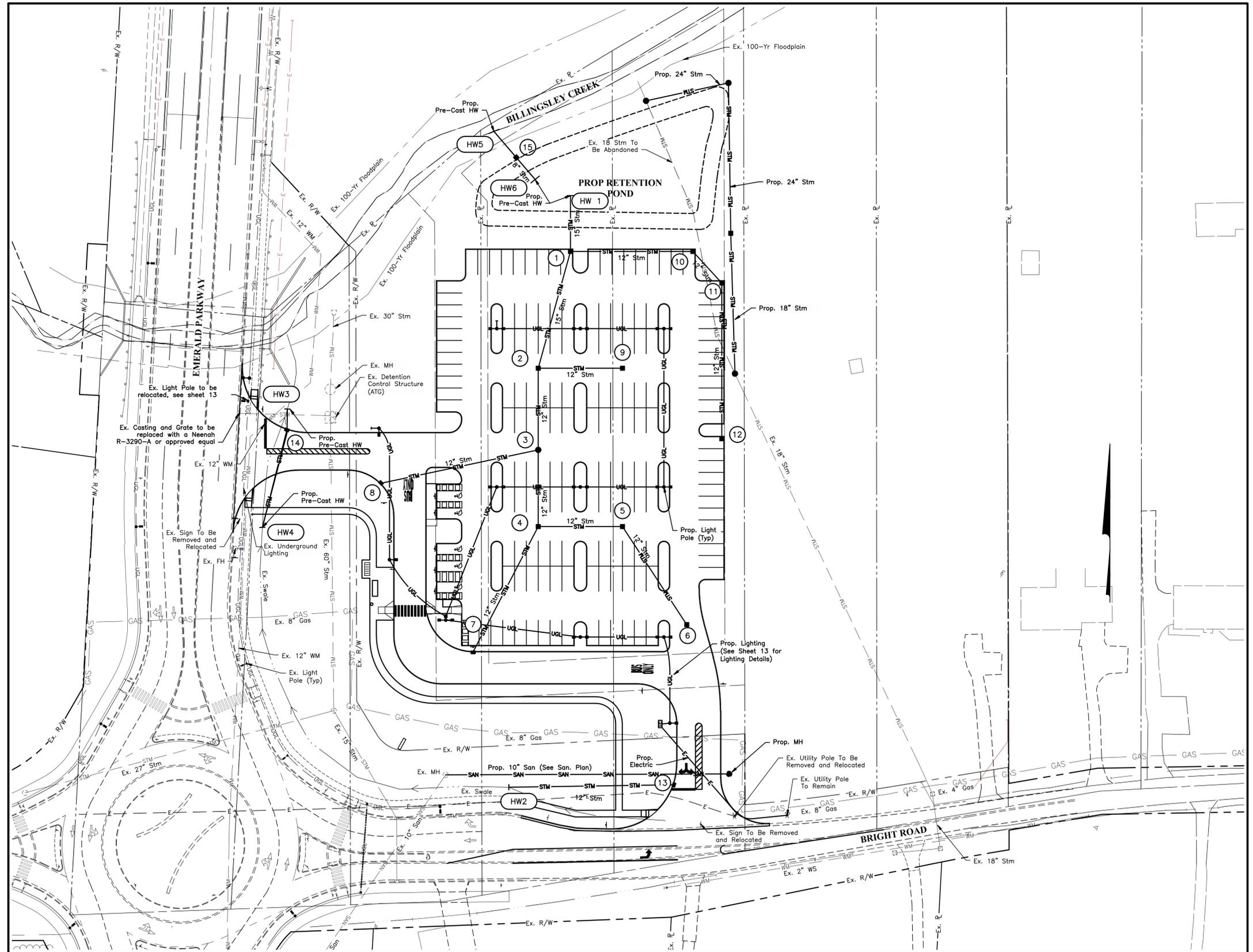
SCALE
 1" = 40'

JOB NO.
 2014-0588

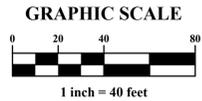
SHEET
 4/14

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- LEGEND**
- WM — Existing Watermain
 - STM — Existing Storm Sewer
 - GAS — Existing Gas Main
 - OHE — Existing Overhead Electric
 - E — Existing Underground Electric
 - UGL — Existing Underground Street Lighting (DOP)
 - OHC — Existing Overhead Communication
 - C — Existing Underground Communication
 - OHE-OHC — Existing Overhead Communication & Electric
 - FO — Existing Fiber Optic
 - STM — Proposed Storm Sewer Main
 - WS — Proposed Water Service (By Separate Plan)
 - DWS — Proposed Domestic Water Service (By Separate Plan)
 - FWS — Proposed Fire Service (By Separate Plan)
 - GM — Proposed Gas Main (By Separate Plan)
 - SAN — Proposed Sanitary Sewer Main (By Separate Plan)
 - SAS — Proposed Sanitary Service (By Separate Plan)
 - C — Proposed Underground Communication (By Separate Plan)
 - E — Proposed Underground Electric (By Separate Plan)
 - UGL — Proposed Underground Lighting
 - SD — Silt Fence
 - 854-855 — Existing Contours
 - 854-855 — Proposed Contours
 - x 856.00 — Proposed Spot Elevation (Top of Pavement Unless Noted Otherwise)
 - — Flow Direction Arrow
 - ↺ — 100-Yr Flood Routing In Pipe
 - — Grade Break
 - ME — Match Existing
 - HP — High Point
 - HW — Headwall
 - C&GI — Curb and Gutter Inlet
 - CB — Catch Basin
 - ATG — Adjust To Grade



PRELIMINARY

 NOT TO BE USED FOR
 CONSTRUCTION

PLAN SET DATE
 January 26, 2015

REVISIONS
MARK DATE DESCRIPTION



CITY OF DUBLIN, FRANKLIN COUNTY, OHIO
 PRIVATE SITE IMPROVEMENT PLAN
 FOR
COTA PARK AND RIDE
 UTILITY PLAN



DATE
January 26, 2015

SCALE
1" = 40'

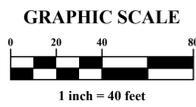
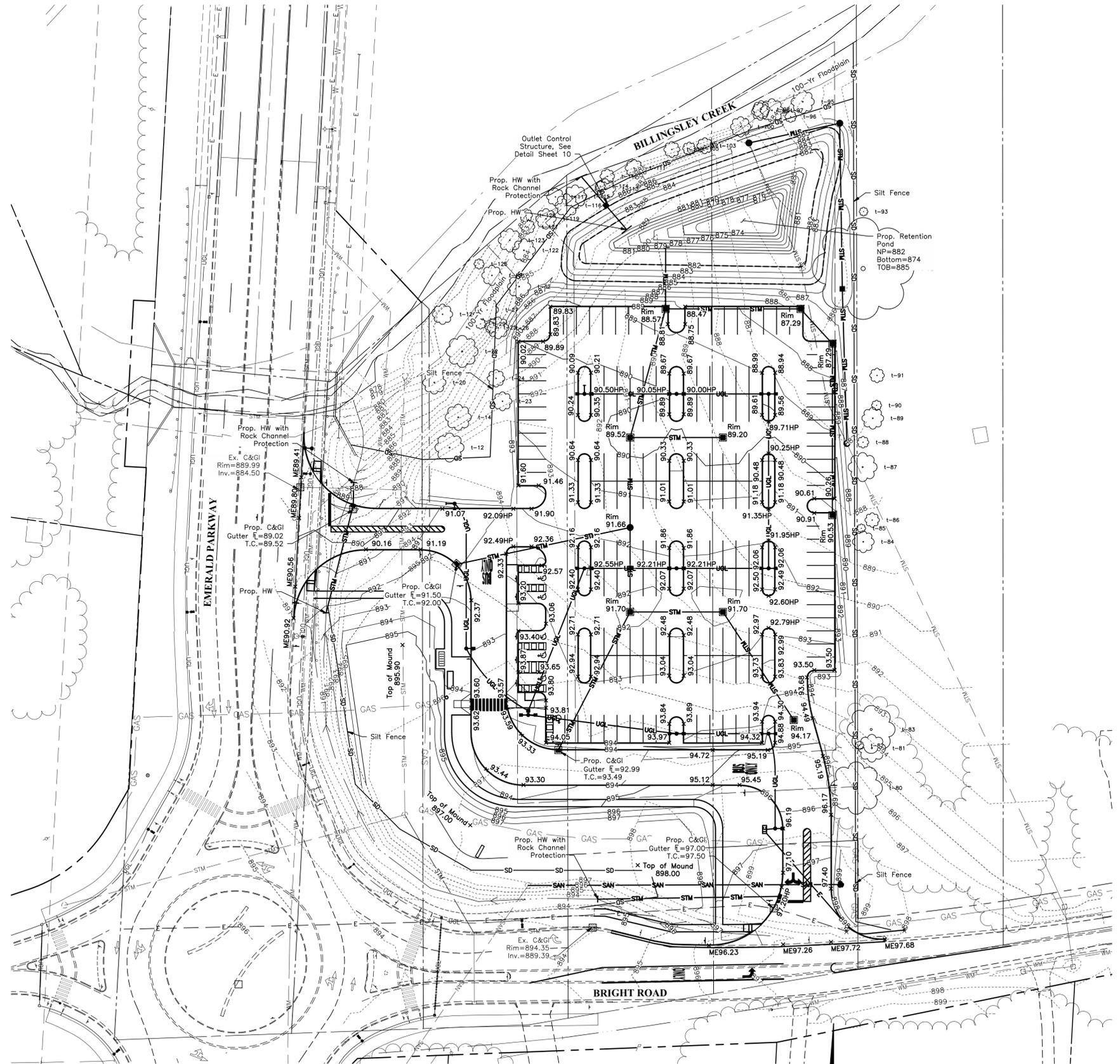
JOB NO.
2014-0588

SHEET
5/14

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NOTES:
 All elevations are to the top of pavement unless otherwise noted.
 Add 800 to spot elevations to obtain final grade elevations.

LEGEND	
	Existing Watermain
	Existing Storm Sewer
	Existing Gas Main
	Existing Overhead Electric
	Existing Underground Electric
	Existing Underground Street Lighting (DOP)
	Existing Overhead Communication
	Existing Underground Communication
	Existing Overhead Communication & Electric
	Existing Fiber Optic
	Proposed Storm Sewer Main
	Proposed Water Service (By Separate Plan)
	Proposed Domestic Water Service (By Separate Plan)
	Proposed Fire Service (By Separate Plan)
	Proposed Gas Main (By Separate Plan)
	Proposed Sanitary Sewer Main (By Separate Plan)
	Proposed Sanitary Service (By Separate Plan)
	Proposed Underground Communication (By Separate Plan)
	Proposed Underground Electric (By Separate Plan)
	Proposed Underground Lighting
	Silt Fence
	Existing Contours
	Proposed Contours
	Proposed Spot Elevation (Top of Pavement Unless Noted Otherwise)
	Flow Direction Arrow
	100-Yr Flood Routing In Pipe
	Grade Break
	Match Existing
	High Point
	Headwall
	Curb and Gutter Inlet
	Catch Basin
	Adjust To Grade



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CITY OF DUBLIN, FRANKLIN COUNTY, OHIO
 PRIVATE SITE IMPROVEMENT PLAN
 FOR
COTA PARK AND RIDE
 GRADING PLAN

EMHT
 Evans, Meecham, Henderson & Tilton, Inc.
 5500 New Albany Road, Columbus, OH 43254
 Phone: 614.775.4500 Toll Free: 888.775.3448
 emht.com

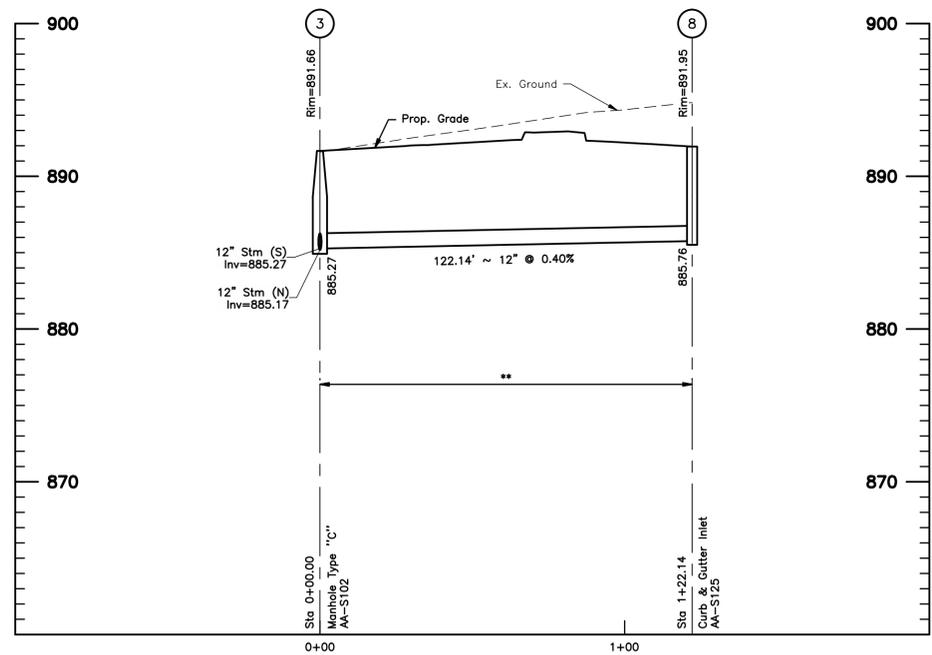
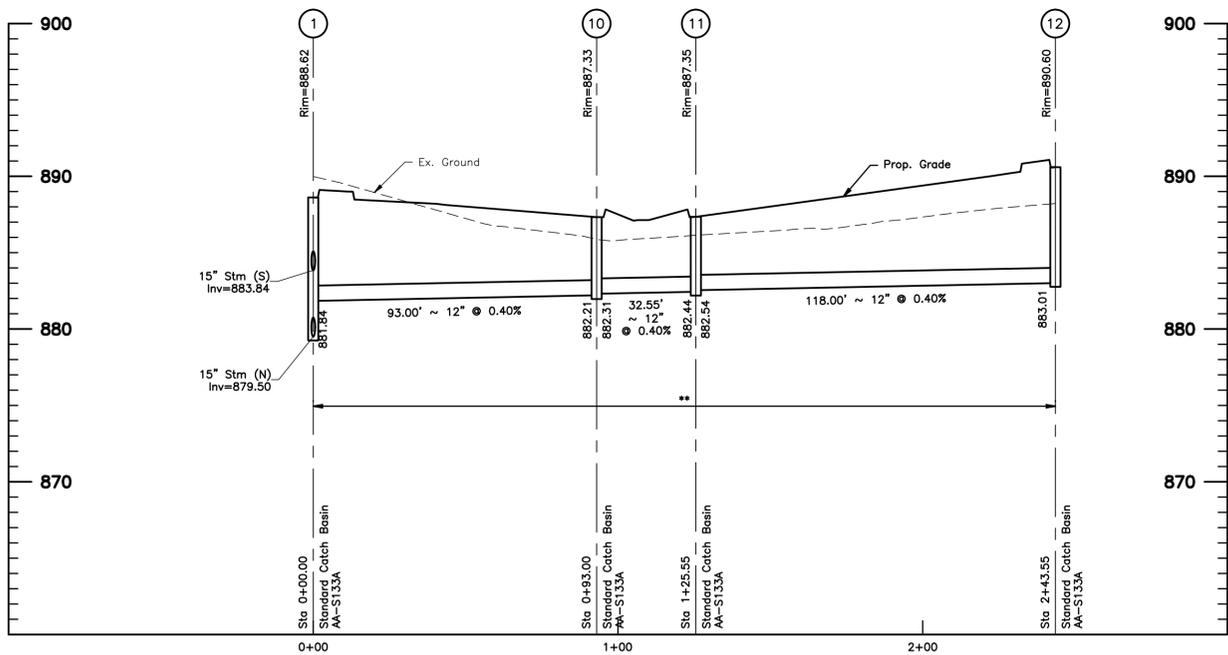
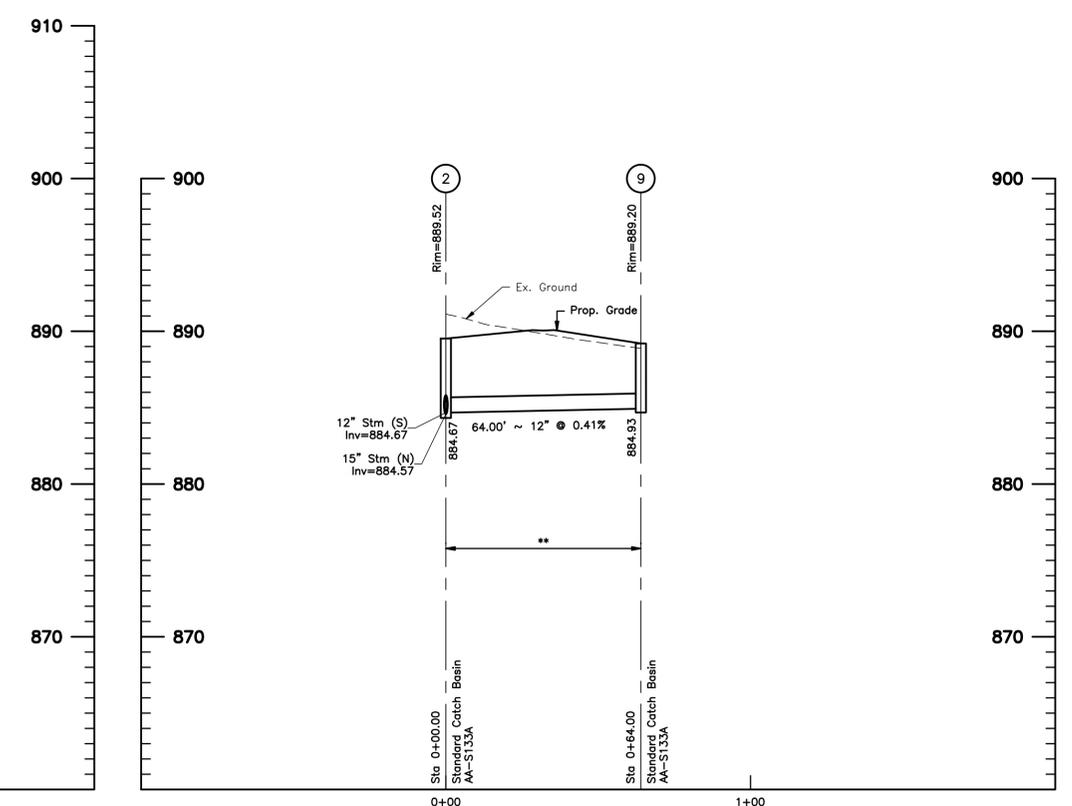
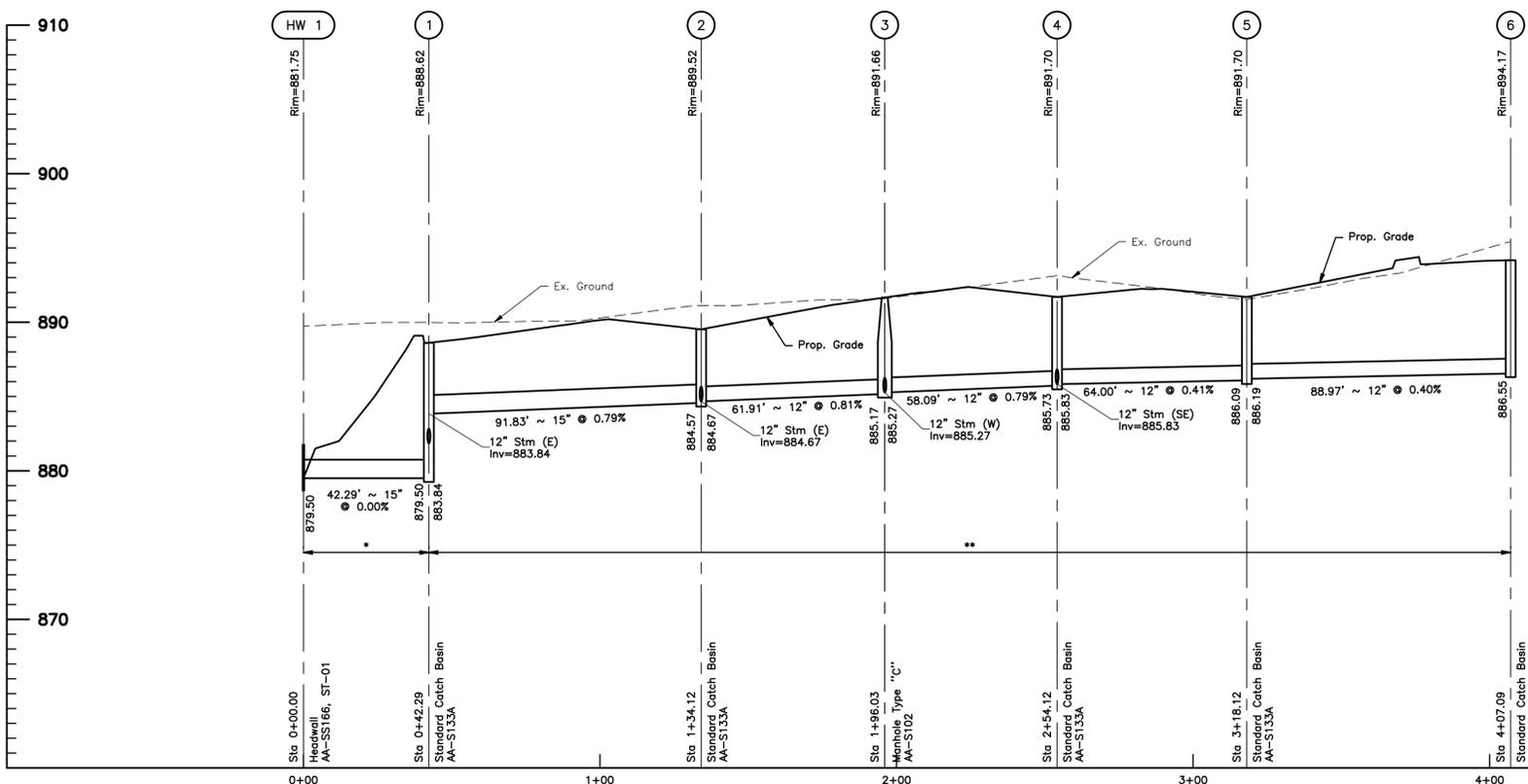
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SCALE
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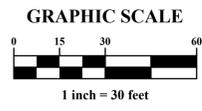
JOB NO.
 2014-0588

SHEET
 6/14

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- NOTES**
- The Contractor shall field verify locations of any present existing utilities and report any conflicts discovered to the Engineer.
 - Proposed ground shown on the profiles indicates final elevations. All elevations in parking/pavement areas are final pavement elevations unless otherwise noted.
 - All headwalls are to include stone veneer per City of Dublin Standard Drawing ST-01.
 - All manhole lids are to be solid lids unless otherwise noted on plans and shall conform with City of Dublin Standard Drawing ST-03.
 - Backfill shall be as indicated on the profiles and in accordance with the following:
 - * Compacted Backfill per CMSC Item 911.
 - ** Compacted Granular Backfill per CMSC Item 912.
- Limits to be filled prior to construction of sewer in accordance with ODOT item 203.
- # 1/1 for proposed storm crossing, see Detail, This Sheet. Proposed storm shall be watertight sanitary grade pipe and joints from structure to structure.



PRELIMINARY
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CITY OF DUBLIN, FRANKLIN COUNTY, OHIO
 PRIVATE SITE IMPROVEMENT PLAN
 FOR
COTA PARK AND RIDE
 STORM SEWER PROFILES

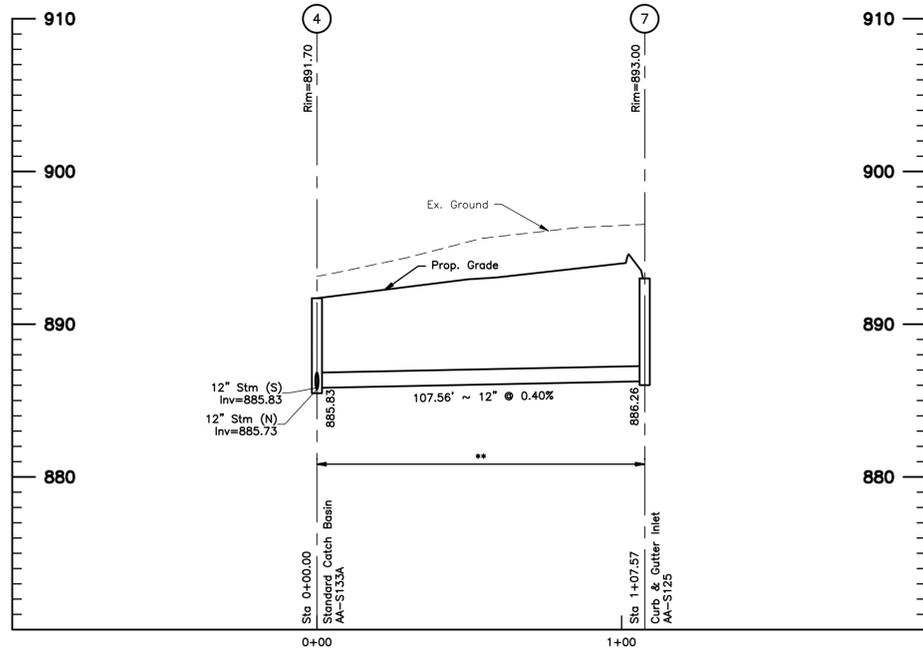


DATE
January 26, 2015

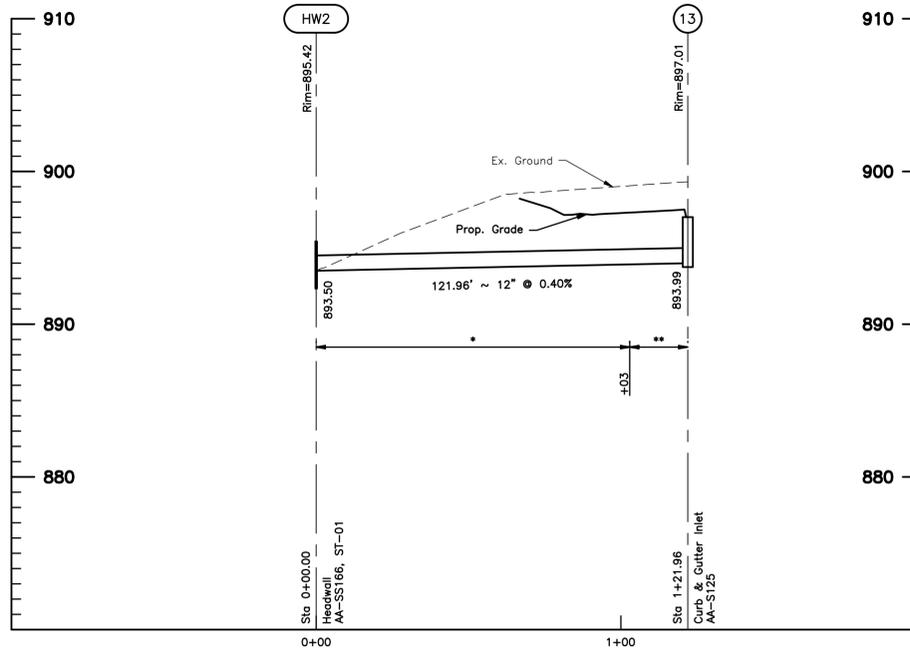
SCALE
Horiz: 1" = 30'
Vert: 1" = 5'

JOB NO.
2014-0588

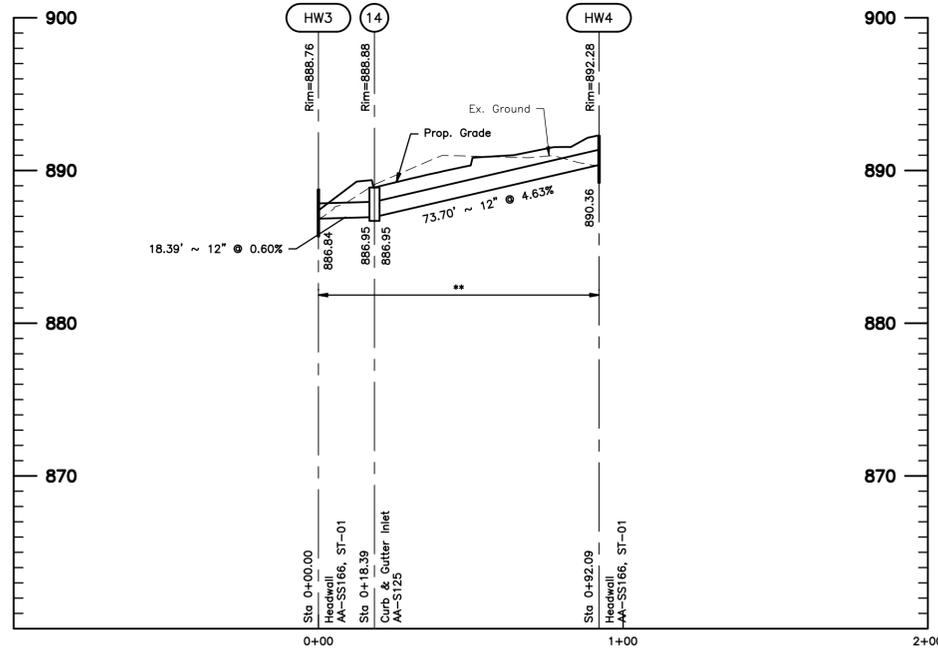
SHEET
7/14



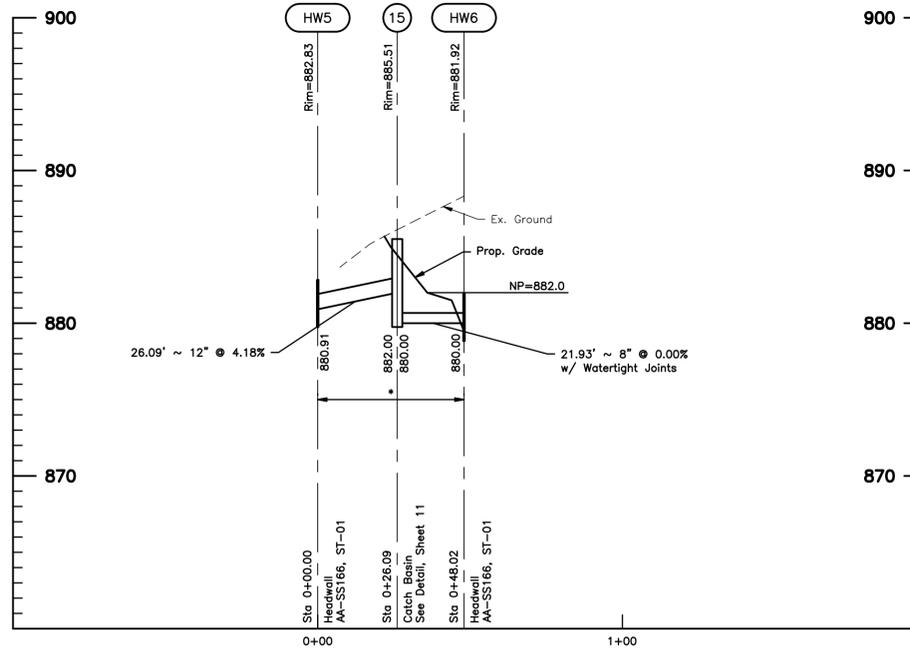
STORM PROFILE 4-7
Horizontal Scale: 1"=30'
Vertical Scale: 1"=5'



STORM PROFILE HW2-13
Horizontal Scale: 1"=30'
Vertical Scale: 1"=5'

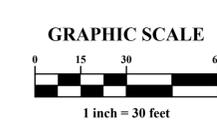


STORM PROFILE HW3-HW4
Horizontal Scale: 1"=30'
Vertical Scale: 1"=5'



STORM PROFILE HW5-HW6
Horizontal Scale: 1"=30'
Vertical Scale: 1"=5'

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 - All headwalls are to include stone veneer per City of Dublin Standard Drawing ST-01.
 - All manhole lids are to be solid lids unless otherwise noted on plans and shall conform with City of Dublin Standard Drawing ST-03.
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PLAN SET DATE
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CITY OF DUBLIN, FRANKLIN COUNTY, OHIO
PRIVATE SITE IMPROVEMENT PLAN
FOR
COTA PARK AND RIDE
STORM SEWER PROFILES



DATE
January 26, 2015

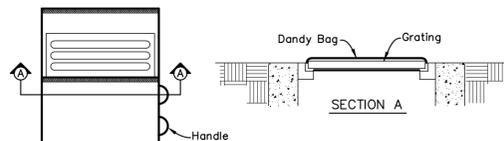
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Horiz: 1" = 30'
Vert: 1" = 5'

JOB NO.
2014-0588

SHEET
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DANDY BAG SEDIMENT FILTER DETAIL



INSTALLATION:

Stand grate on end. Place Dandy Bag over grate. Roll grate over so that open end is up. Pull up slack. Tuck flap in. Be sure end of grate is completely covered by flap or Dandy Bag will not fit properly. Holding handles, carefully place Dandy Bag with grate inserted into catch basin frame so that red dot on the top of the Dandy Bag is visible.

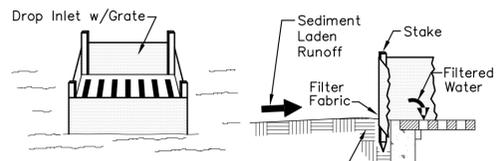
MAINTENANCE:

With a stiff bristle broom or square point shovel remove silt & other debris off surface after each event.

PROVIDE FOR THE FOLLOWING STRUCTURES:

Existing parking lot structures receiving flow from construction area.

FILTER FABRIC DROP INLET SEDIMENT FILTER DETAIL



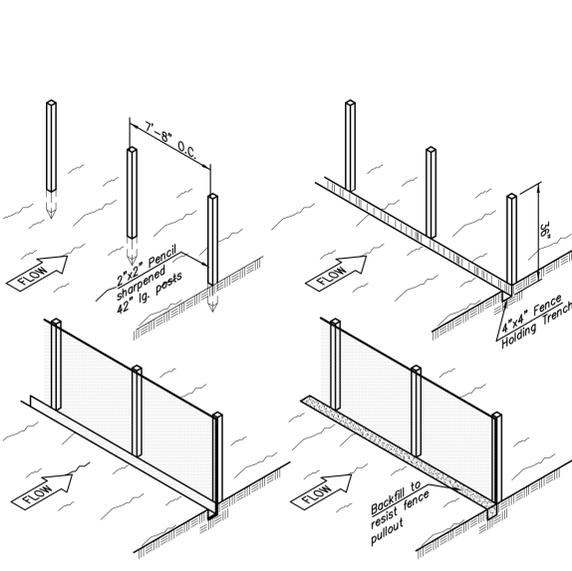
SPECIFIC APPLICATION:

To Prevent Piping
This method of inlet protection is applicable where the inlet drains a relatively flat area (slopes no greater than 5 percent) where sheet or overland flows (not exceeding 0.5 cfs) are typical. This method shall not apply to inlets receiving concentrated flows, such as in street and highway medians.

PROVIDE FOR THE FOLLOWING STRUCTURES:

1, 4, 5, 6, 7

SEDIMENT FENCE BARRIER DETAIL



SILT FENCE:

This sediment barrier utilizes standard strength or extra strength synthetic filter fabrics. It is designed for situations in which only sheet or overland flows are expected.

MATERIAL PROPERTIES ARE:

- The height of a silt fence shall not exceed 36-inches (higher fences may impound volumes of water sufficient to cause failure of the structure). The filter fabric shall be purchased in a continuous roll cut to the length of the barrier to avoid the use of joints. When joints are necessary, filter cloth shall be spliced together only at a support post, with a minimum of a 6 inch overlap, and securely sealed.
- Posts shall be spaced a maximum of 10 feet apart at the barrier location and driven securely into the ground (minimum of 12-inches). Wood posts will be a minimum of 3/2" long. When extra strength fabric is used without the wire support fence, post spacing shall not exceed 6 feet.
- A trench shall be excavated approximately 4-inches wide and 6 inches deep along the line of posts and upslope from the barrier.
- When standard strength filter fabric is used, a wire mesh support fence shall be fastened securely to the upslope side of the posts using heavy duty wire staples at least 1-inch long, tie wires or hog rings. The wire shall extend into the trench a minimum of 2-inches and shall not extend more than 36-inches above the original ground surface.
- The standard strength filter fabric shall be stapled or wired to the fence, and 8-inches of the fabric shall be extended into the trench. The fabric shall not extend more than 36-inches above the original ground surface. Filter fabric shall not be stapled to existing trees.
- When extra strength filter fabric and closer post spacing are used, the wire mesh support fence may be eliminated. In such a case, the filter fabric is stapled or wired directly to the posts with all other provisions of Item No. 6 applying.
- The trench shall be backfilled and soil compacted over the filter fabric. Silt fences shall be removed when they have served their useful purpose, but not before the upslope area has been permanently stabilized.
- Silt fences and filter barriers shall be inspected immediately after each rainfall and at least daily during prolonged rainfall. Any required repairs shall be made immediately.
- To prevent water ponded by the silt fence from flowing around the ends, each end shall be constructed upslope so that the ends are at a higher elevation.

MAINTENANCE:

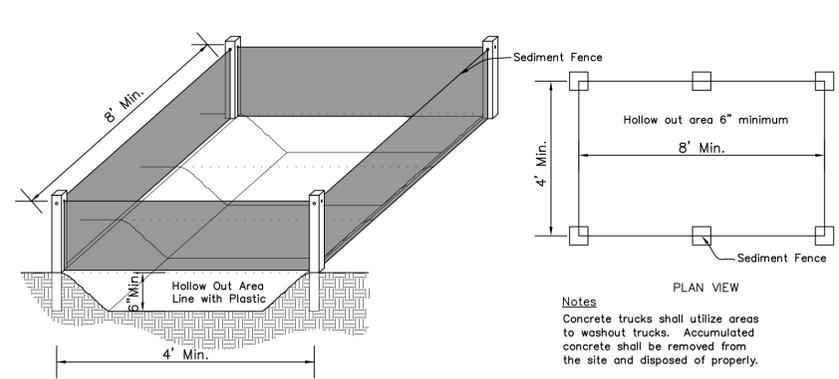
Should the fabric on a silt fence or filter barrier decompose or become ineffective prior to the end of the expected usable life and the barrier is still necessary, the fabric shall be replaced promptly.

Sediment deposits should be removed after each storm event. They must be removed when deposits reach approximately one-half the height of the barrier.

Any sediment deposits remaining in place after the silt fence or filter barrier is no longer required shall be dressed to conform with the existing grade, prepared and seeded.

FABRIC PROPERTIES	VALUES	TEST METHOD
Grab Tensile Strength	90 lb. Minimum	ASTM 1682
Mullen Burst Strength	190 psi Minimum	ASTM 3786
Slurry Flow Rate	0.3 gal./min./ft ² Maximum	
Equivalent Opening Size	40-80	U.S. Std. Sieve CW-02215
Ultraviolet Radiation Stability	90% Minimum	ASTM-G-26

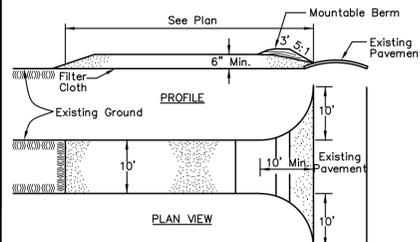
CONCRETE WASHOUT AREA



Notes

Concrete trucks shall utilize areas to washout trucks. Accumulated concrete shall be removed from the site and disposed of properly.
Contractor to determine location of Concrete Washout Area.

STABILIZED CONSTRUCTION ENTRANCE



CONSTRUCTION SPECIFICATIONS

- Stone Size - Use 2 inch stone, or reclaimed or recycled concrete equivalent.
- Length - As required.
- Thickness - Not less than six (6) inches.
- Width - Ten (10) foot minimum, but not less than the full width at points where ingress or egress occurs.
- Filter Cloth - will be placed over the entire area prior to placing of stone.
- Surface Water - All surface water flowing or diverted toward construction entrances shall be piped across the entrance. If piping is impractical, a mountable berm with 5:1 slopes will be permitted.
- Maintenance - The entrance shall be maintained in a condition which will prevent tracking or flowing of sediment onto public right-of-way. This may require periodic top dressing with additional stone as conditions demand and repair and/or cleanout of any measures used to trap sediment. All sediment spilled, dropped, washed or tracked onto public rights-of-way must be removed immediately.
- Washing - Wheels shall be cleaned to remove sediment prior to entrance onto public right-of-ways. When washing is required, it shall be done on an area stabilized with stone and which drains into an approved sediment trapping device.
- Periodic inspection and needed maintenance shall be provided after each rain.

EROSION & SEDIMENT CONTROL NARRATIVE

Plan Engineer: Evans, Mechwart, Hambleton & Tilton, Inc.
5500 New Albany Road
Columbus, OH 43054
Phone: (614) 775-4500
Fax: (614) 775-4800

Owner's Representative: City of Dubin
Ken Richardson
5800 Shier Rings Road
Dublin, OH 43016
Phone: (614) 410-4631

On-Site Contact: City of Dubin
Ken Richardson
5800 Shier Rings Road
Dublin, OH 43016
Phone: (614) 410-4631

CONTRACTOR RESPONSIBILITY: Details have been provided on the plans in an effort to help the Contractor provide erosion and sedimentation control. The details shown on the plan shall be considered a minimum. Additional or alternate details may be found in the O.D.N.F. Manual "Rainwater and Land Development." The Contractor shall be solely responsible for providing necessary and adequate measures for proper control of erosion and sediment runoff from the site along with proper maintenance and inspection in compliance with the NPDES General Permit for Stormwater Discharges Associated with Construction Activity.

All Erosion & Sediment Control practices are subject to Field Modification at the direction of the City of Dubin and/or Ohio EPA.

Existing Site Conditions: The proposed development is located on approximately 3.3± acres within an existing site consisting of a mown grassy field with interspersed clusters of trees and shrubs. The existing topography of the site generally slopes from the southwest towards the northeast.

Existing Site Drainage Condition: Stormwater run off generated by the site discharges into Billingsley Creek.

Proposed Site Drainage Condition: The stormwater runoff generated by the site under post-developed conditions will be collected in catch basins and piped to a retention basin and released to Billingsley Creek.

Adjacent Areas: The site is located near adjacent to the existing Emerald Parkway and Bright Road roundabout.

Critical Areas: The most critical areas related to implementing the erosion and sediment control are the northern and eastern boundaries.

StormWater Pollution Prevention Measures: Approximately 2.5± acres of land will be disturbed during the construction of this project. Stormwater pollution prevention will be accomplished through the implementation of the BMP's detailed on this sheet.

Sequence of Construction:

1. Install the tree protection fence and erosion control devices.
2. Relocate existing utilities, remove trees, and demolish pavement, walks and curbs.
3. Perform mass earthwork activities and begin building foundations. Install temporary seeding as needed.
4. Install storm sewer and other utilities.
5. Construct remainder of building.
6. Fine grade the site and install paving and landscape.
7. Once site is stabilized, remove tree protection and erosion control devices.

MARK	DATE	DESCRIPTION

CITY OF DUBLIN, FRANKLIN COUNTY, OHIO
PRIVATE SITE IMPROVEMENT PLAN
FOR
COTA PARK AND RIDE
EROSION CONTROL DETAILS

EMHT
Evans, Mechwart, Hambleton & Tilton, Inc.
5500 New Albany Road, Columbus, OH 43054
Phone: (614) 775-4500 Toll Free: 888.775.3448
emht.com

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Wet Basin

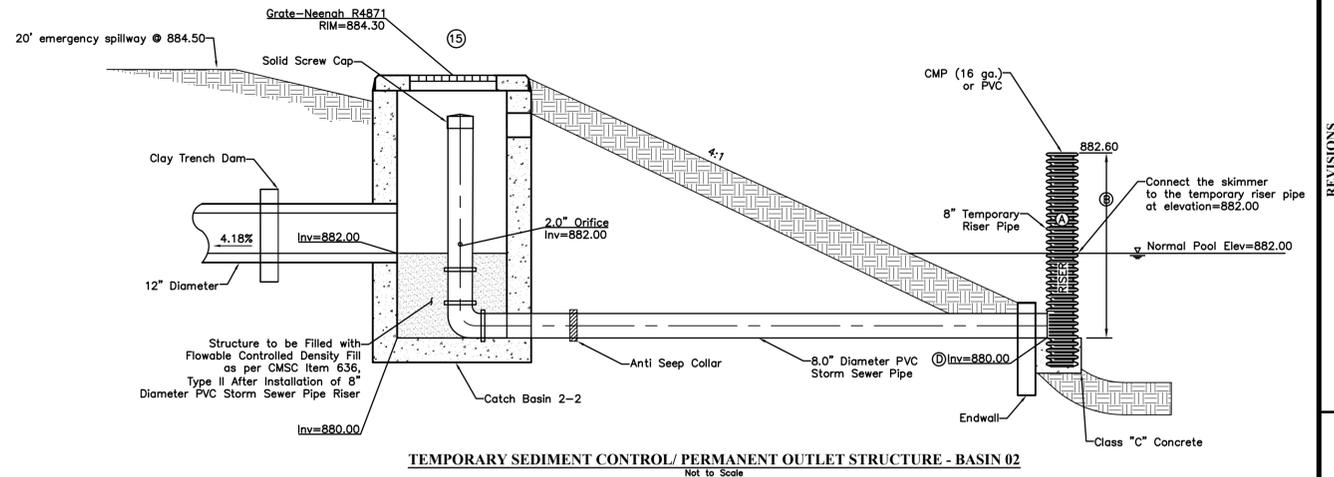
Inspection Item	Maintenance Procedures	Frequency of Inspection
Inlet/Outlet Structure & Side Slopes	<ul style="list-style-type: none"> Do not fertilize vegetation surrounding basin. Remove accumulated sediment and debris from inlet and outlet structures. Mow side slopes. 	Monthly
Basin Embankment	<ul style="list-style-type: none"> Repair undercut/eroded areas and stabilize. 	Every 6 months
Storm Sewer System	<ul style="list-style-type: none"> Remove debris from the sewer system to ensure positive flow to the basin. 	Every 6 months
Stormwater Basin	<ul style="list-style-type: none"> Inspect for damage, paying particular attention to the outlet control structure Check for signs of eutrophic conditions (algae buildup) Note signs of hydrocarbon build-up, remove appropriately Monitor sediment accumulation in the facility Examine to ensure inlet and outlet devices are free of debris and are operational. Inspect for invasive vegetation if wetland components included. 	Annually
Stormwater Basin Sediment Accumulation	<ul style="list-style-type: none"> Monitor sediment accumulations, and remove sediment when the pool volume has become reduced significantly (25% of permanent pool volume lost), or the pond becomes eutrophic. 	5 to 10 years

The Owner shall be responsible for the inspection and maintenance of the stormwater basin, associated outlet structure and all other maintenance procedures listed above. Inspections and maintenance that are conducted shall be documented and filed for future reviews by the City of Dublin.

Stormwater Basins treat incoming stormwater runoff by physical, biological, and chemical processes. The primary removal mechanism is the gravitational settling of particulates, organic matter, metals, bacteria and organics as stormwater runoff resides in the basin. Another mechanism for pollutant removal is uptake by algae and wetland plants in the wet basin permanent pool, particularly removing nutrients. Other contaminants such as hydrocarbons are broken down and eliminated by volatilization and chemical activity. Stormwater Basins are utilized to remove 80% of the total suspended solids load in typical urban post-development runoff when designed and maintained properly.

Stormwater basins naturally collect sediment, including gravel, sand, and mud, as well as other debris like litter. To maintain its capacity and function, a basin should be kept free of excessive debris, litter, and sediment. The permanent pool for the proposed basin is designed to be eight feet in depth. This design depth should be verified every 5-10 years to ensure that the basin will continue to function properly. Property owners or contracted personnel shall use a boat, canoe, kayak, or similar means to position themselves in the middle of the stormwater basin. Several measurements around center of the stormwater basin shall be taken using a Stadia Rod to determine the depth of the permanent pool. Measurements taken when basin water level is at N.P. Elevation (Min. 72 hours after rain event). Once the depth of the stormwater basin reaches four feet or less, the accumulated sediment shall be excavated to restore the permanent pool depth to eight feet in depth. The stormwater basin is to be temporarily drained/pumped down so that the accumulated sediment can be removed. Sediment excavated from stormwater basin is required to be tested to determine where to appropriately dispose the material offsite. Sediment removed from the stormwater basin should be stored properly until disposal to ensure no exposure to stormwater runoff and properly disposed of per local guidelines.

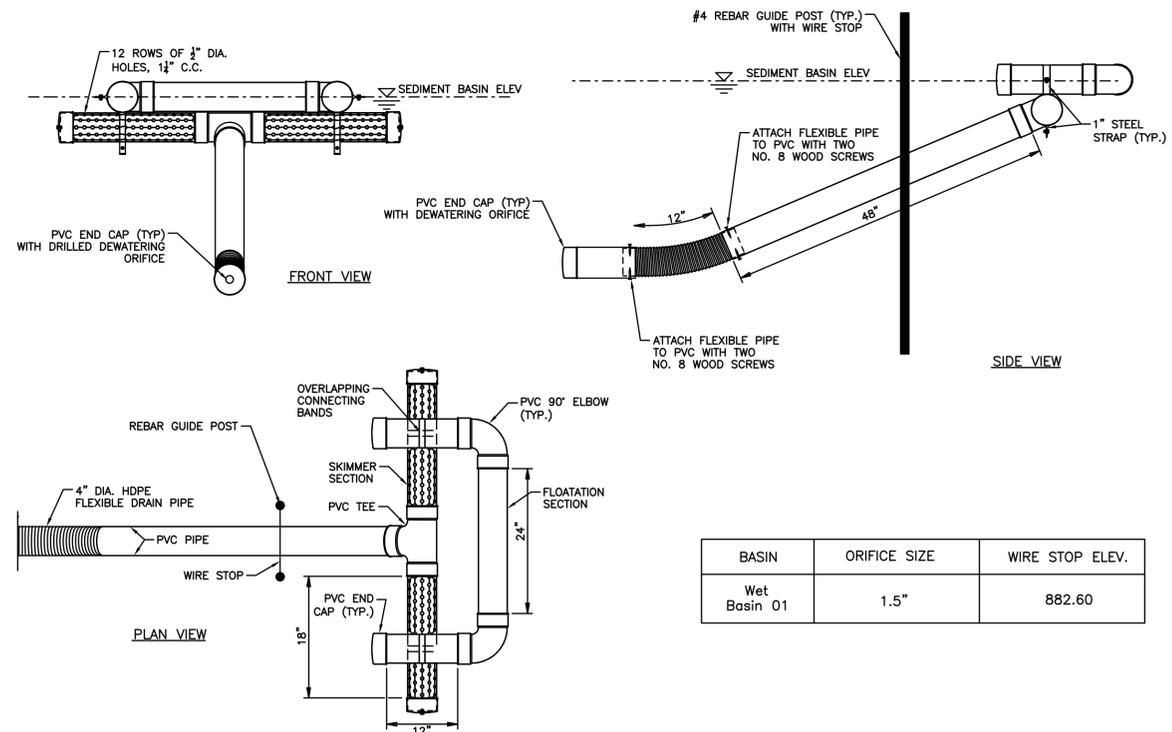
INSPECTION AND MAINTENANCE



TEMPORARY SEDIMENT CONTROL/ PERMANENT OUTLET STRUCTURE - BASIN 02
Not to Scale

BASIN	TRIBUTARY ACREAGE	DISTURBED ACREAGE	REQUIRED BASIN DEWATERING VOLUME (67 CY/AC)	PROVIDED BASIN DEWATERING VOLUME	REQUIRED SEDIMENT STORAGE VOLUME (37 CY, DISTURBED A.C.)	PROVIDED SEDIMENT STORAGE VOLUME	CONTROL STRUCTURE	
							TEMPORARY RISER PIPE DIAMETER (A)	TEMPORARY RISER HEIGHT (B)
Wet Basin 01	2.62 Ac	2.62 Ac	177.5 CY	876.0 CY	96.8 CY	195.2 CY	8"	2.6'

Required Dewatering Volume Drawdown > 48 Hrs.



BASIN	ORIFICE SIZE	WIRE STOP ELEV.
Wet Basin 01	1.5"	882.60

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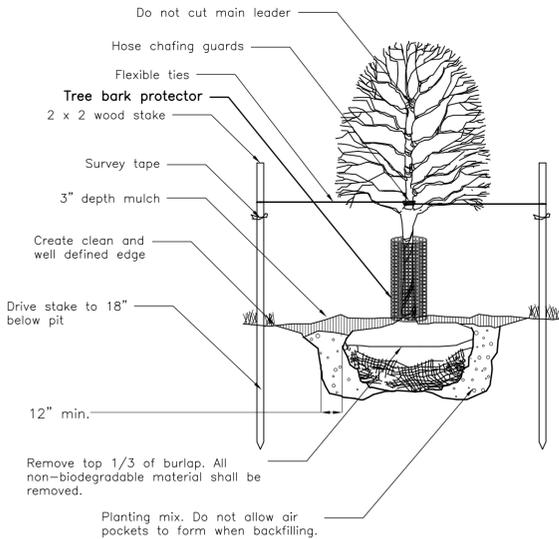


CITY OF DUBLIN, FRANKLIN COUNTY, OHIO
PRIVATE SITE IMPROVEMENT PLAN
FOR
COTA PARK AND RIDE
OUTLET CONTROL STRUCTURE DETAILS



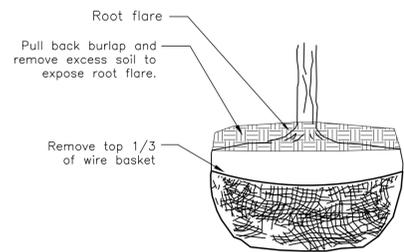
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SCALE	1" = 40'
JOB NO.	2014-0588
SHEET	10/14

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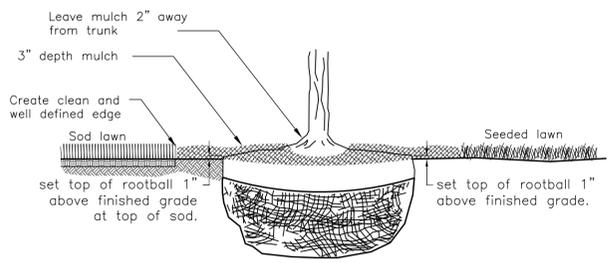


Note: Do NOT stake replacement trees

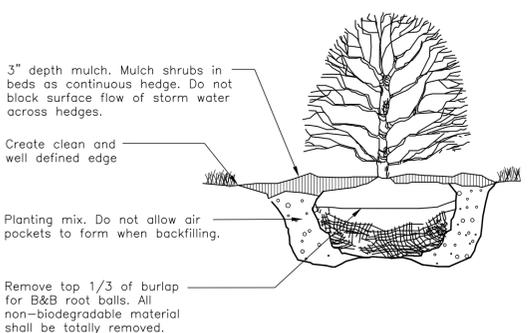
Deciduous Tree Planting
No Scale



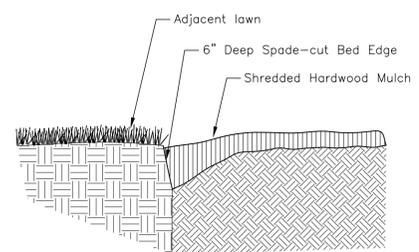
Rootball Preparation
No Scale



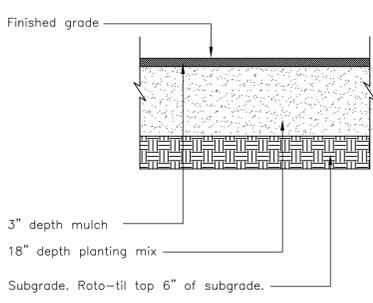
Rootball Setting
No Scale



Shrub Planting
No Scale



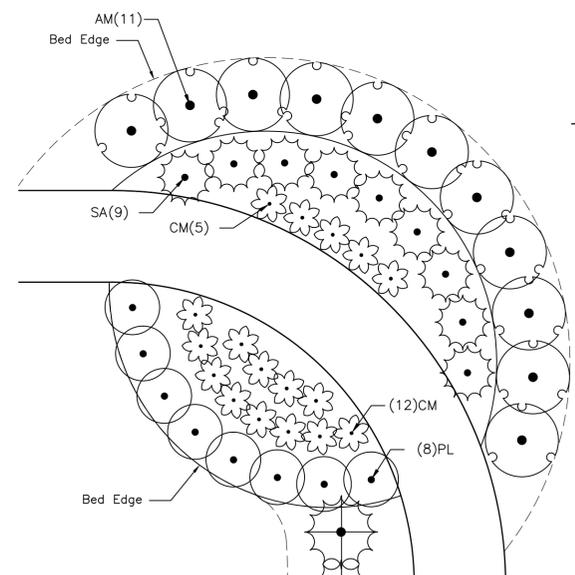
Planting Bed Edge
No Scale



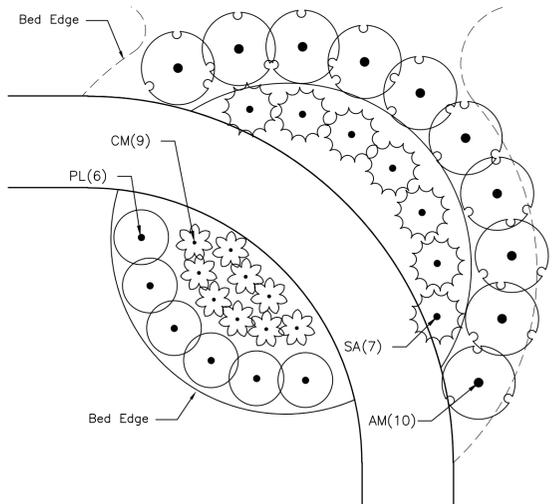
Planting Area Establishment
No Scale

GENERAL NOTES

- Prior to installation, the landscape contractor shall inspect the general site conditions and verify the subgrade, elevations, utility locations and topsoil provided by general contractor. The landscape contractor shall notify the general contractor of any unsatisfactory conditions and work shall not proceed until such conditions have been corrected and are acceptable to the landscape contractor.
- All plants shall meet or exceed standards set in the American Standard for Nursery Stock, ANSI Z60.1, 2004. All plants shall equal or exceed the measurements and sizes specified in the schedule.
- All planting operations shall adhere to American Nursery & Landscape Association standards unless noted otherwise.
- Substitutions shall be permitted with notification and written approval from the Owner. Substituted material shall be equivalent or greater in size than the specified plant. Substituted plants shall have the same essential characteristics and growth habit of the specified plant.
- Confirm location of all utilities and subsurface drain lines prior to plant installation.
- Contractor may slightly field adjust plant locations as necessary to avoid utilities. Finished planting beds shall be graded to provide positive drainage. Contractor shall repair all lawn areas disturbed during construction with seed and warrant a healthy, weed free lawn prior to project acceptance.
- Seed all areas within contract limits that are not covered by paving, buildings or planting beds unless otherwise noted. Seeding shall not begin until area has received topsoil and finished grade.
- Mulch planting beds with shredded hardwood mulch of uniform dark brown color. It shall be free of twigs, leaves, disease, pest or other material unsightly or injurious to plants. Average applied thickness shall be 3" depth. Mulch hedges in a continuous bed.
- Planting beds shall be covered with pre-emergent herbicide applied at product specified rate unless otherwise noted.
- Bed edge shall be smooth, consistent, hand trenched 6" deep and "V" shaped unless otherwise noted. All excavated material shall be removed from the bed edge and planting bed.
- All planting bed edges to be smooth flowing arcs or straight lines as shown on plan. Plant locations and layout of beds shall be located by Contractor and approved by Landscape Architect prior to planting.
- Parking lot and street trees shall have a clear canopy height of 6' min.
- Tree shall be placed a minimum of 3' from sidewalks and curbs.
- All planting beds to be tilled and backfilled with prepared planting mix to a minimum depth of 18 inches unless otherwise indicated. All individually planted trees and shrubs to be backfilled with prepared planting mix. Prepared planting mix shall be mixed on site and consist of one part topsoil, one part soil amendment, one part soil from excavation. Topsoil: ASTM D5268, pH range of 5.5 to 7, min. 4 percent organic material, free of stones 1 inch and larger. Soil amendment: Source separated yard waste compost from an Ohio EPA rated class IV compost facility.
- Raised beds, including mulch, shall be no higher than 6 inches above adjacent grade.
- Lawn areas to be backfilled with topsoil to a minimum settled thickness of 6 inches.
- All trees, shrubs, groundcover, and lawns to be fertilized with a commercial grade fertilizer consisting of fast and slow release nitrogen.
- Composition and application rate of fertilizer shall be sufficient to amend soil according to recommendations of a qualified soil testing agency. Submit test results and amendment recommendations to Landscape Architect. Fertilizer shall be in a dry granular form for lawns and granular or tablet form for plants.
- Contractor to determine plant list quantities from the plan. Graphic representation on plan supersedes in case of discrepancy with quantities on schedule.
- Any item or areas damaged during construction shall be repaired or replaced to its original condition at the contractor expense.
- Contractor shall thoroughly water all plants at time of installation and as needed until project acceptance by owner. Contractor shall guarantee all plants installed for one full year from date of acceptance by the Owner. All plants shall be alive and at a vigorous rate of growth at the end of the guarantee period.
- Lawn seed mix shall be proportioned by weight as follows: 10 percent NuBlue or Blue Chip Kentucky Bluegrass; 10 percent Caddieshack or GoalKeeper Perennial Ryegrass; 80 percent Quest, Inferno, Arid 3 and/or Pixie Tall Fescue (select 2). Sodded lawns shall match seeded lawns.
- Lawn seed shall not have less than 95 percent germination, not less than 85 percent pure seed, and not more than 0.5 percent weed seed.



West Planting Bed Enlargement
1"=5'



Southeast Planting Bed Enlargement
1"=5'

PLANT SCHEDULE WEST PLANTING BED

SHRUBS	QTY	BOTANICAL NAME	COMMON NAME	SIZE	CONDITION
AM	11	Aronia melanocarpa 'Autumn Magic'	Autumn Magic Black Chokeberry	#5	Cont.
PERENNIALS	QTY	BOTANICAL NAME	COMMON NAME	SIZE	CONDITION
CM	17	Coreopsis verticillata 'Moonbeam'	Moonbeam Coreopsis	#2	Cont.
PL	8	Perovskia atriplicifolia 'Little Spire'	Little Spire Russian Sage	#2	Cont.
SA	9	Rudbeckia hirta	Black-eyed Susan	#2	Cont.

PLANT SCHEDULE SOUTHEAST PLANTING BED

SHRUBS	QTY	BOTANICAL NAME	COMMON NAME	SIZE	CONDITION
AM	10	Aronia melanocarpa 'Autumn Magic'	Autumn Magic Black Chokeberry	#5	Cont.
PERENNIALS	QTY	BOTANICAL NAME	COMMON NAME	SIZE	CONDITION
CM	9	Coreopsis verticillata 'Moonbeam'	Moonbeam Coreopsis	#2	Cont.
PL	6	Perovskia atriplicifolia 'Little Spire'	Little Spire Russian Sage	#2	Cont.
SA	7	Rudbeckia hirta	Black-eyed Susan	#2	Cont.

PRELIMINARY
NOT TO BE USED FOR CONSTRUCTION

PLAN SET DATE
January 26, 2015

MARK	DATE	DESCRIPTION



CITY OF DUBLIN, FRANKLIN COUNTY, OHIO
PRIVATE SITE IMPROVEMENT PLAN
FOR
COTA PARK AND RIDE
LANDSCAPE DETAILS



DATE
January 26, 2015

SCALE
As Noted

JOB NO.
2014-0588

SHEET
12/14

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STREET LIGHTING NOTES

PLAN AND SPECIFICATION COMPLIANCE

These specifications, together with the accompanying plans, are to describe the type, size, and location of the products and material to be provided and installed under various bid items related to Street Lighting. The Contractor shall furnish and install Street Lighting items and related material in compliance with these plans and specifications, as well as the current Ohio Department of Transportation Construction and Material Specifications, and the City of Dublin Standard Detail drawings for Street Lighting. Street Lighting plans shall meet or exceed the standards specified. In case of a conflicting specification statement, the specification document hierarchy shall be in the order listed from (A) highest to (C) lowest.

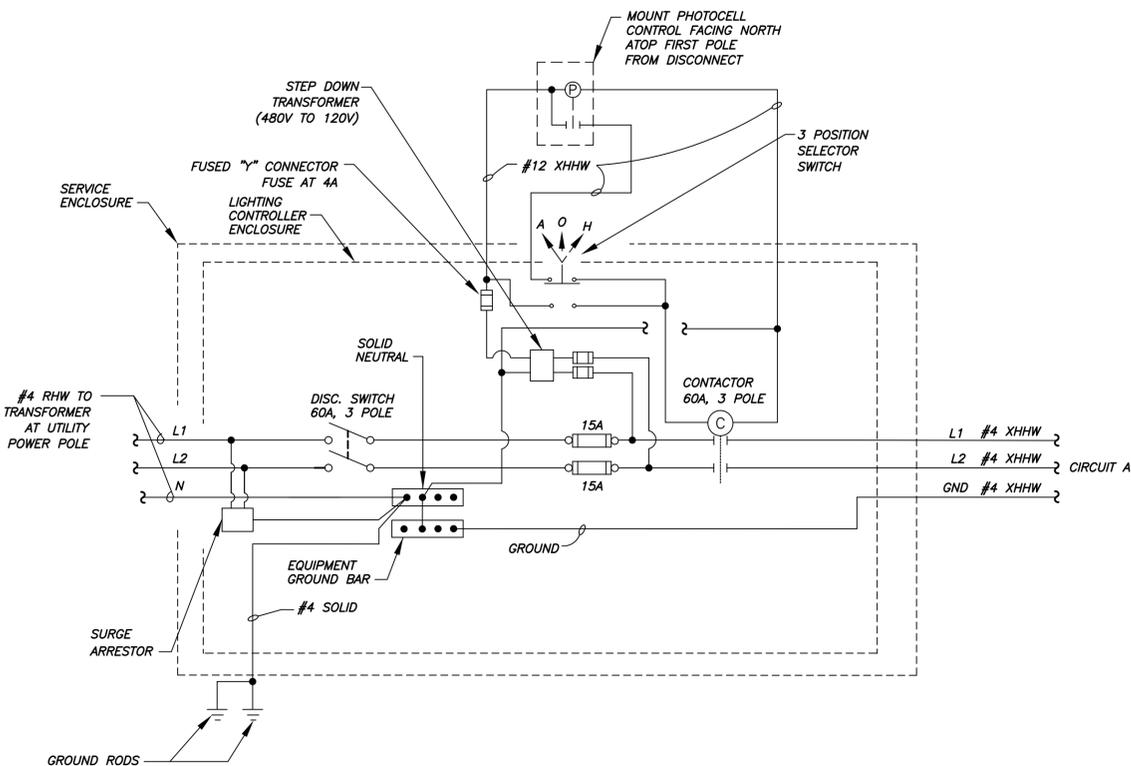
- (A) Specifications listed in this plan
- (B) City of Dublin Street Lighting Standard Drawings and Specifications
- (C) ODOT Construction and Material Specifications

ITEM 625 - POWER SERVICE, AS PER PLAN

Power Service shall be as per Item 625, the power service schematic diagram shown on this sheet, and the City of Dublin Standard Drawing SL-13.

LEGEND

- LIGHT POLE (SL-03, 20')
W/ (1)-LUMINAIRE (SL-01), ARE-EDG-2M-DA-06-E-UH-BZ-525-40K
- LIGHT POLE (SL-03, 20')
W/ (2)-LUMINAIRES (SL-01), ARE-EDG-5M-DA-06-E-UH-BZ-525-40K
- UCL 3" SCH 40 CONDUIT IN TRENCH
W/ (3)-#4 AWG, 600V LIGHTING CABLES
- UCL 6" SCH 80 SLEEVE AND 3" SCH 40 INNER CONDUIT
W/ (3)-#4 AWG, 600V LIGHTING CABLES
- PAD MOUNTED LIGHTING CONTROLLER (SL-13)
- PULL BOX, 725.06, POLYMER CONCRETE, 24"
- EX. LIGHT POLE (TO REMAIN UNLESS OTHERWISE NOTED)
- UCL EX. UNDERGROUND STREET LIGHTING (TO REMAIN)
- R&R REMOVE AND RELOCATE

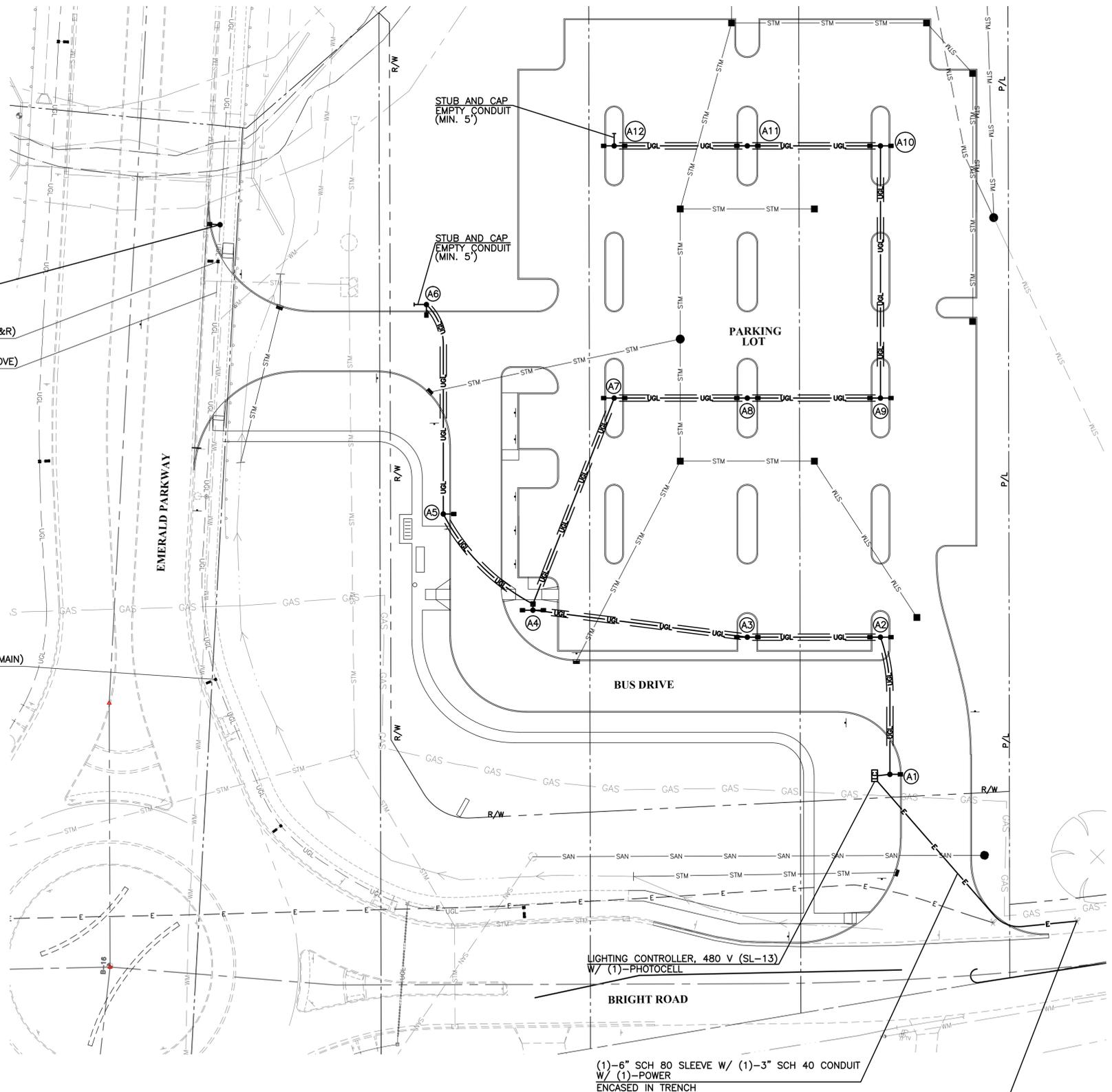


POWER SERVICE SCHEMATIC DIAGRAM
SCALE: NONE

NEW LIGHT POLE LOCATION SHALL INTERCEPT EXISTING LIGHTING CONDUIT W/ (1)-NEW FOUNDATION (POLE TO BE WIRED)

Ex. Light Pole/Luminaire (R&R) (REMOVE FOUNDATION)
Ex. Lighting Conduit/Cables (REMOVE)

Ex. Light Pole/Luminaire (REMAIN) (POLE TO BE WIRED)



STREET LIGHTING PHOTOMETRIC RESULTS					
DESCRIPTION	AVE	MAX	MIN	MAX/MIN	AVE/MIN
PARKING LOT	1.9 fc	4.1 fc	0.5 fc	8.2:1	3.8:1
BUS DRIVE	2.0 fc	4.0 fc	0.6 fc	6.7:1	3.3:1

(1)-6" SCH 80 SLEEVE W/ (1)-3" SCH 40 CONDUIT
W/ (1)-POWER
ENCASED IN TRENCH

Ex. Wood Pole (TO REMAIN)
W/ (1)-NEW 480V AEP TRANSFORMER
W/ (1)-2" CONDUIT RISER, SCH 80
(AEP SHALL INSTALL TRANSFORMER)

NOTE TO REVIEWER: POWER SOURCE CURRENTLY UNDER INVESTIGATION

MARK	DATE	DESCRIPTION

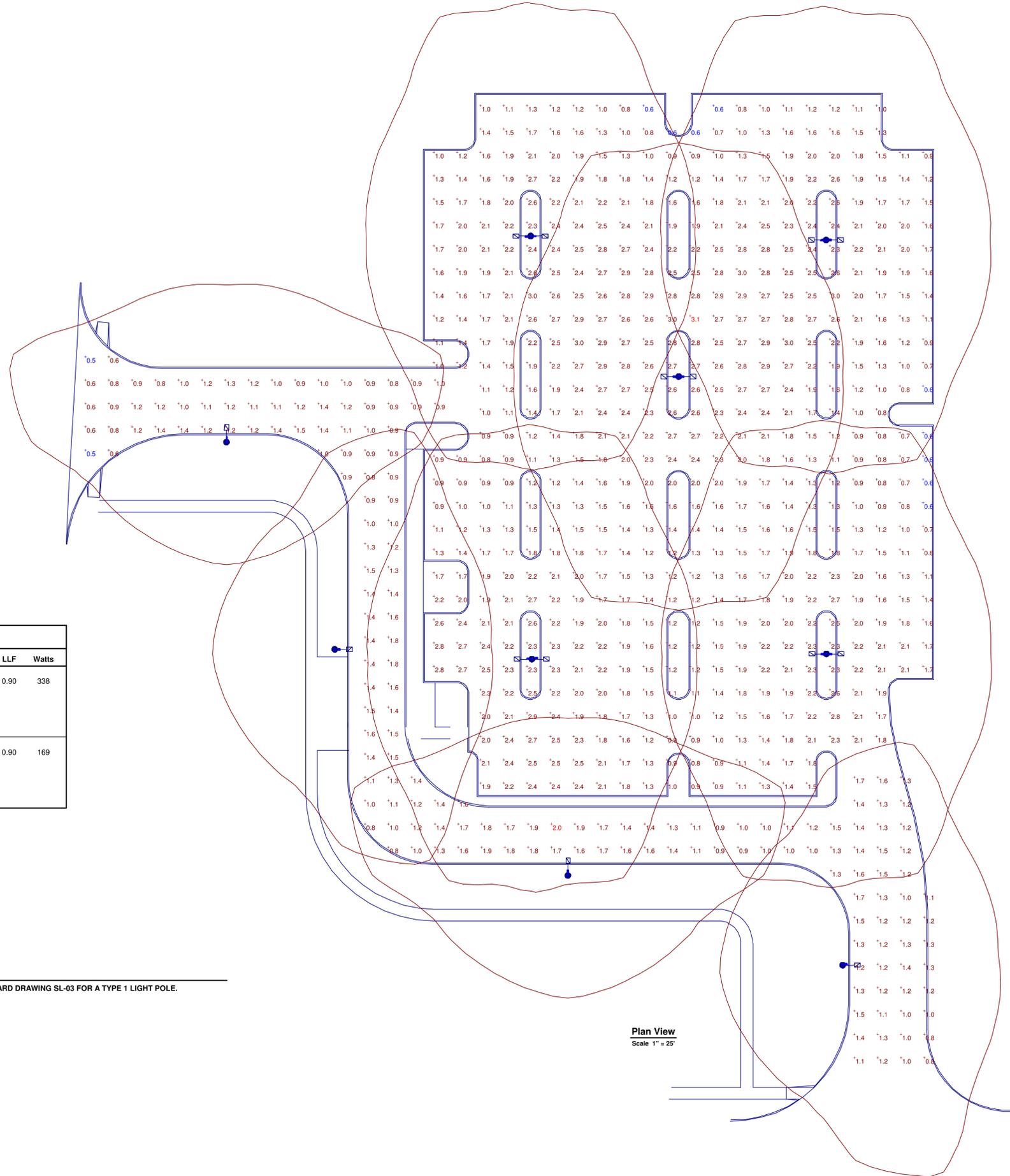


CITY OF DUBLIN, FRANKLIN COUNTY, OHIO
PRIVATE SITE IMPROVEMENT PLAN
FOR
COTA PARK AND RIDE
LIGHTING PLAN



DATE	January 15, 2015
SCALE	1" = 30'
JOB NO.	2014-0588
SHEET	13/14

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Plan View
Scale 1" = 25'

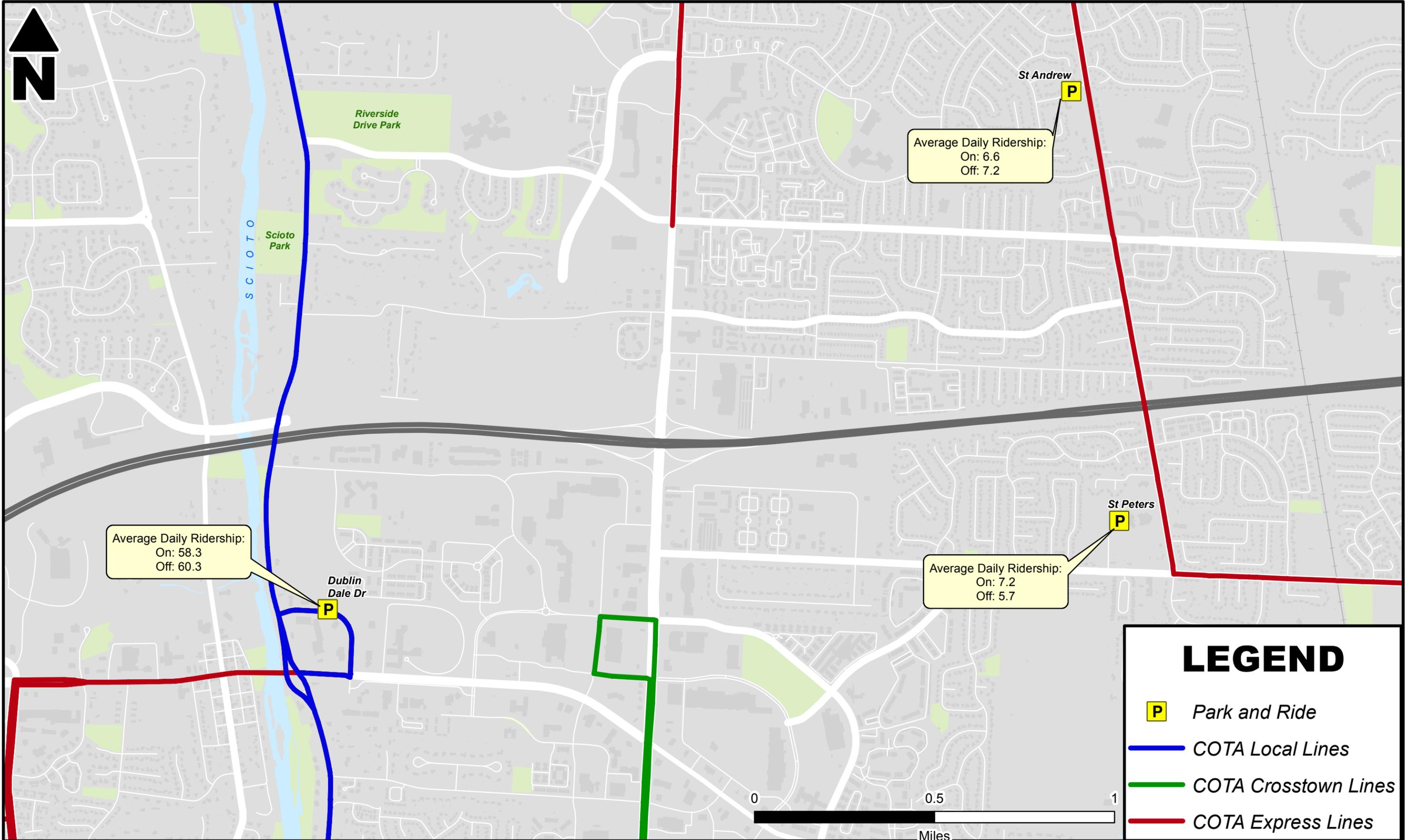
LUMINAIRE SCHEDULE							
Qty	Catalog Number	Description	Lamp	File	Lumens	LLF	Watts
5	ARE-EDG-2M-**-10-E-UH-525-40K (525mA)	CONFIGURED FROM Cree Edge Area, Type II Medium, 60 LEDs, 700mA, 4000K	CONFIGURED FROM Sixty White LEDs, Vertical Base-Up Position	ARE-EDG-2M-**-10-E-UH-525-40K-CONFIGURED. IES	Absolute	0.90	338
4	ARE-EDG-2M-**-10-E-UH-525-40K (525mA)	CONFIGURED FROM Cree Edge Area, Type II Medium, 60 LEDs, 700mA, 4000K	CONFIGURED FROM Sixty White LEDs, Vertical Base-Up Position	ARE-EDG-2M-**-10-E-UH-525-40K-CONFIGURED. IES	Absolute	0.90	169

STATISTICS			
Description	Avg	Min	Avg/Min
Bus Drive	1.2 fc	0.5 fc	2.4:1
Parking Area	1.8 fc	0.6 fc	3.0:1

NOTES
1. LUMINAIRES ARE MODELED USING A MOUNTING HEIGHT OF 36'-8" AS GIVEN BY DUBLIN STANDARD DRAWING SL-03 FOR A TYPE 1 LIGHT POLE.

Designer
EMHT
Date
Jan 6 2015
Scale
As Noted
Drawing No.

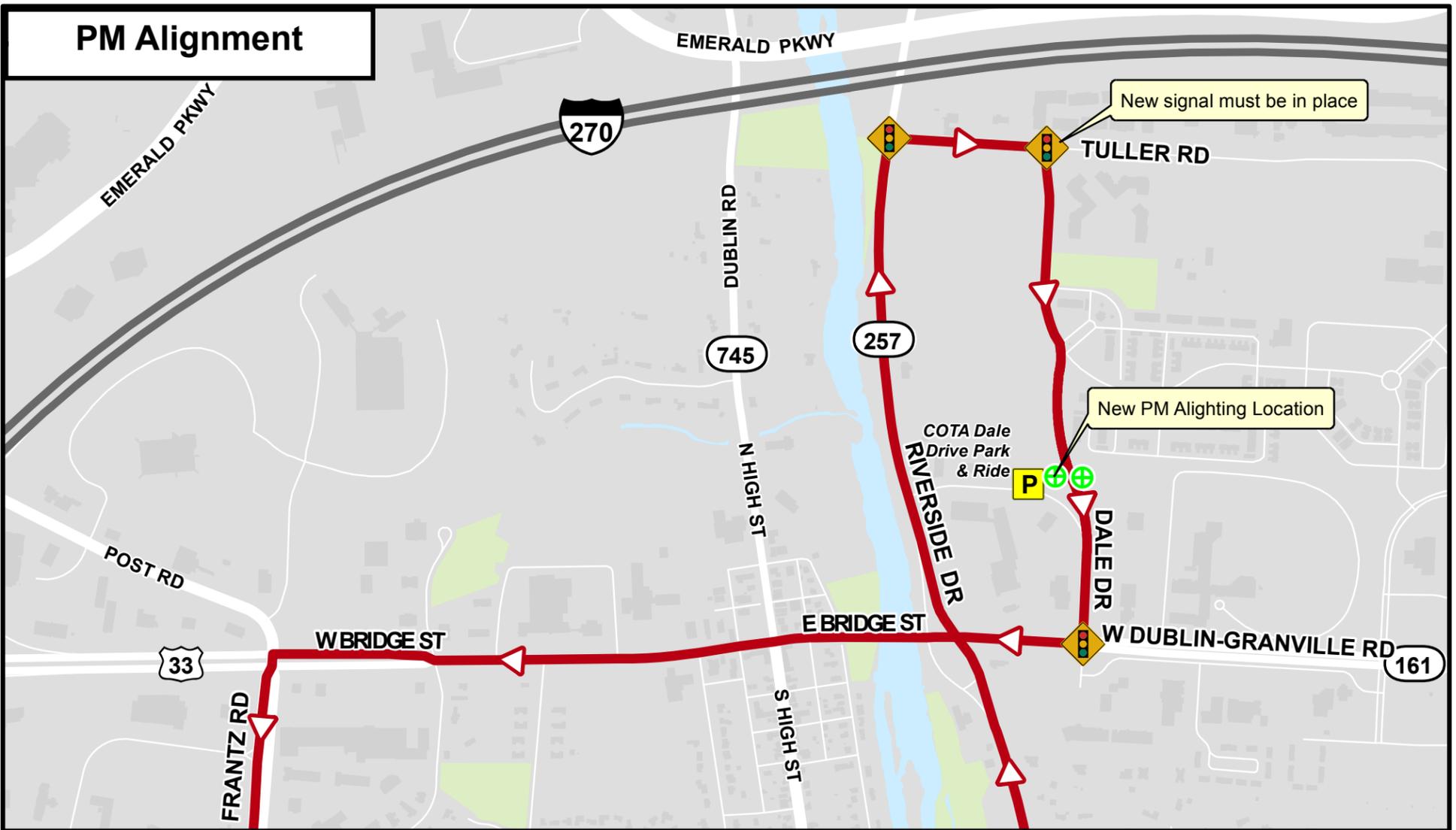
Average Daily Ridership at Dublin Area Park and Rides (Sept. 2013 APC)



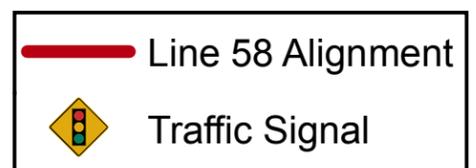
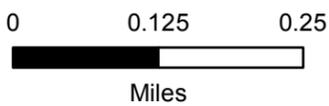
Line 58 Dublin Express Bus Alignments Dale Drive Park and Ride



6 morning trips leaving park and ride between approximately 6 a.m. and 8 a.m.



6 evening trips arriving at park and ride between approximately 4:30 p.m. and 6:30 p.m.



Line 58 Dublin Express Bus Alignments Bright Road Park and Ride



6 morning trips leaving park and ride between approximately 6 a.m. and 8 a.m.



6 evening trips arriving at park and ride between approximately 4:30 p.m. and 6:30 p.m.

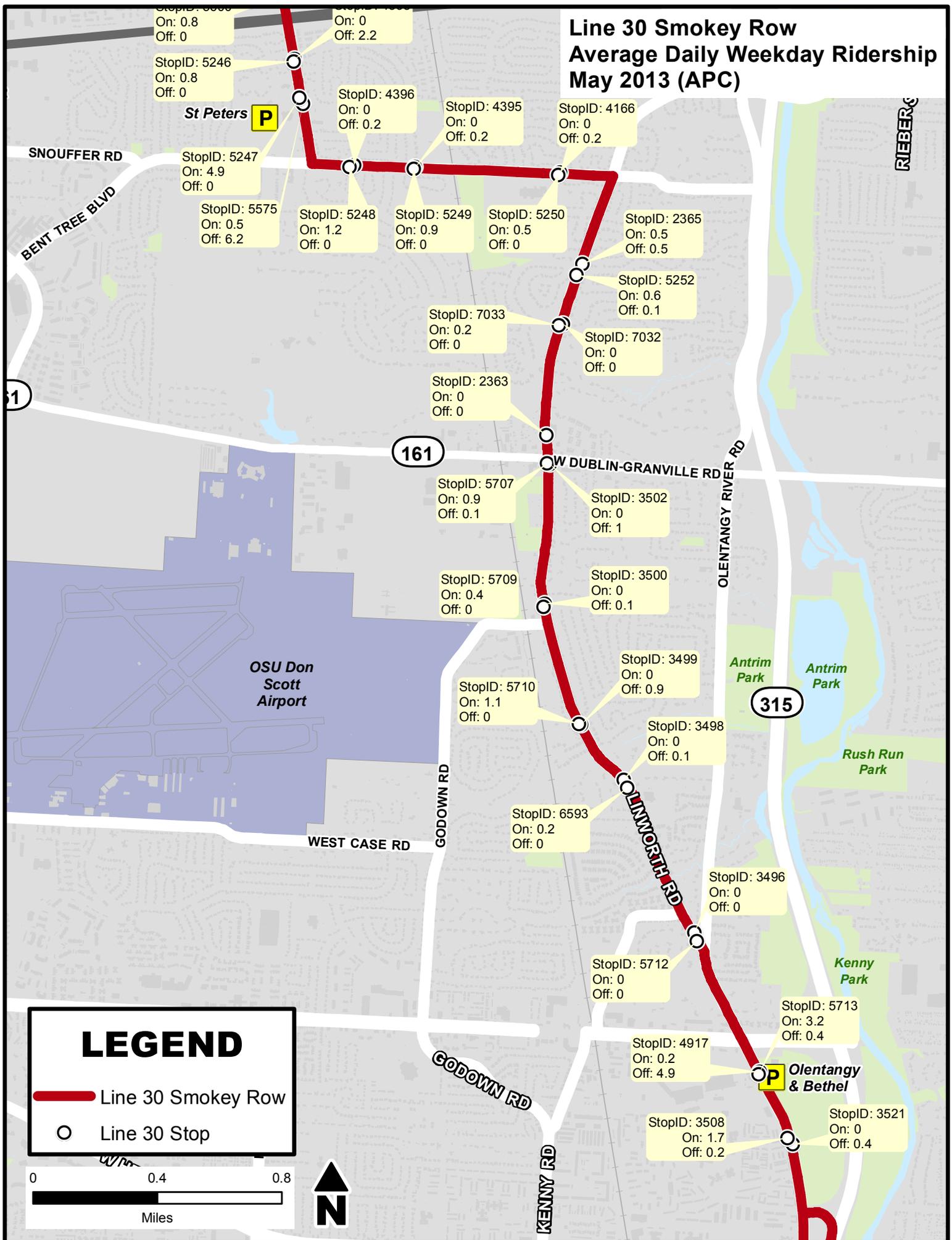


— Line 58 Alignment

Line 30 Smokey Row Average Daily Weekday Ridership May 2013 (APC)

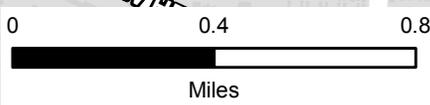


Line 30 Smokey Row Average Daily Weekday Ridership May 2013 (APC)

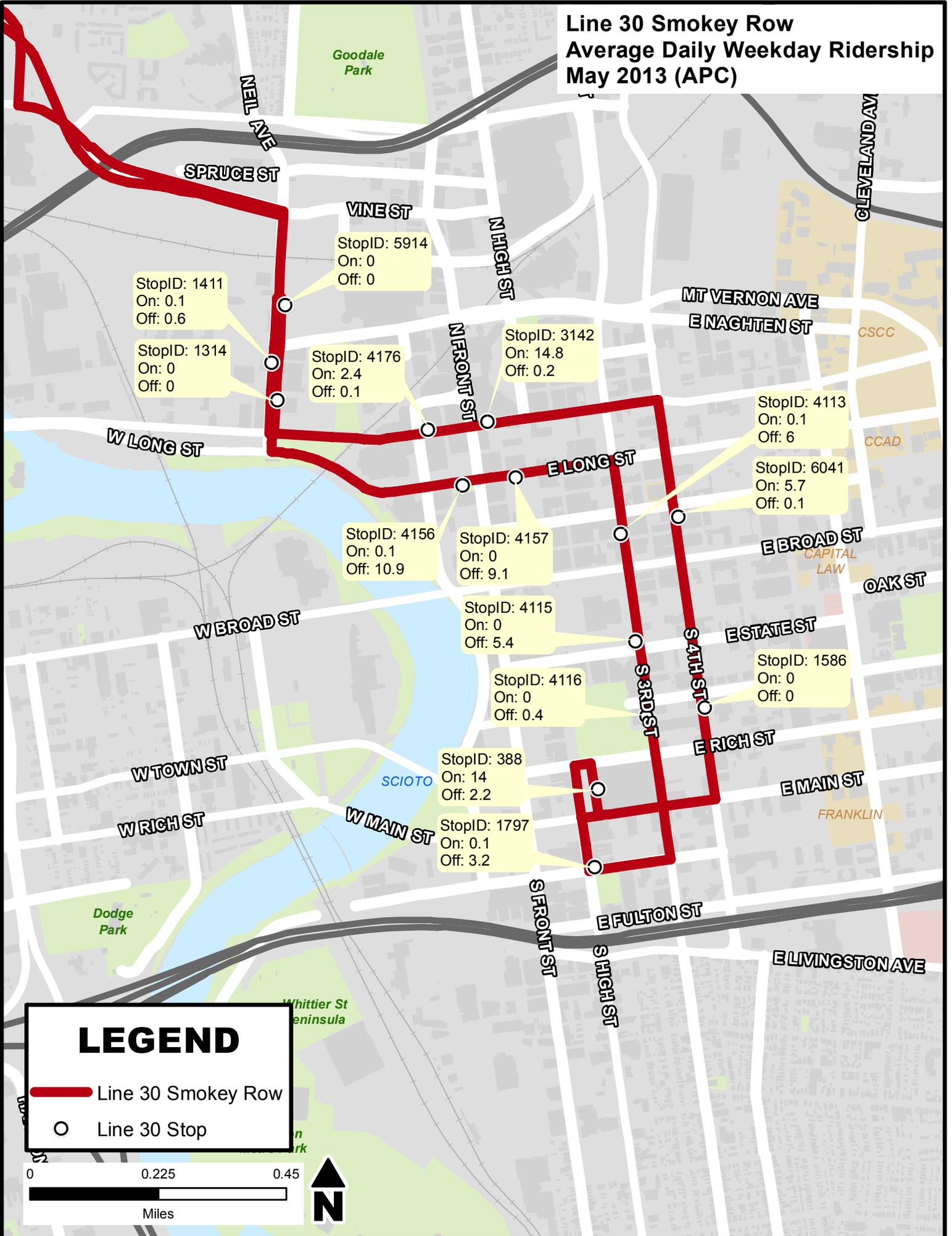


LEGEND

- Line 30 Smokey Row
- Line 30 Stop



**Line 30 Smokey Row
Average Daily Weekday Ridership
May 2013 (APC)**



LEGEND

- Line 30 Smokey Row
- Line 30 Stop

