



# BRIDGE STREET DISTRICT STREETSCAPE CHARACTER GUIDELINES

prepared by  
**MKSK**

 City of Dublin

# CONTENTS

	PAGE
1 PLACEMAKING IN DUBLIN: IMPLEMENTING A VISION.....	4
2 PRINCIPLES FOR CREATING GREAT STREETS .....	6
3 A HIERARCHY OF PLACE: STREETSCAPE CHARACTER TYPES .....	8
4 MATERIALS SPECIFICATIONS .....	20





Existing streetscapes in Dublin demonstrate a commitment to quality and design detail. New streets in the Bridge Street District will build on this tradition.

**Dublin** has a long history of investing in high quality public infrastructure. No where is this more evident than the city's roadway system. As with any community, Dublin's streets serve a critical transportation role that supports the city's economic well-being and quality of life for residents. But Dublin's streets also create an image and a feeling for travelers that helps to define the city as a place. A commitment to aesthetic character is evident throughout the city's roadways - visible in beautifully manicured landscaping, well-maintained sidewalks and bike paths, use of durable and attractive materials, and attention to detail through all stages of planning, design and construction.

This commitment to design quality will be instrumental as the City implements its vision to create a vibrant, walkable, mixed use urban core in the Bridge Street District. The Bridge Street District Plan envisioned in the Dublin Community Plan emphasizes the importance of creating a pedestrian and bicycle-friendly street network and the role of streets as part of the public realm. The Plan recognizes that great streets are spaces where people gather together and spend time. Streets in the Bridge Street District are intended to be places in their own right with their own character, much like a public park or plaza.

This document is a tool to assist the City in translating this placemaking philosophy into reality through the design and construction of new streets in the Bridge Street District. The following section (Section 2) outlines foundational principles for creating great urban streets. Section 3 outlines a hierarchy of streets in the Bridge Street District and provides recommendations for streetscape design features in each category of street. Section 3 also provides recommendations for integrating bicycle facilities and greenways into the street system. Section 4 provides material specifications for use in designing and constructing new streets according to the design guidelines.

**STREETS SHOULD BE AN  
EXTENSION OF THE PUBLIC  
OPEN SPACE NETWORK. THEY  
ARE PLACES FOR PEOPLE  
AND SHOULD BE DESIGNED TO  
ENCOURAGE PUBLIC GATHERING  
AND INTERACTION.**





**1** STREETS SHOULD BE GREAT PUBLIC SPACES AS MUCH AS FUNCTIONAL PUBLIC INFRASTRUCTURE.

**2** SIGNATURE STREETSCAPES SHOULD DEFINE THE BRIDGE STREET DISTRICT.

**3** THE PUBLIC REALM SHOULD PROVIDE VISUAL COHESION; THE PRIVATE REALM SHOULD PROVIDE VISUAL VARIETY.

**4** STREETSCAPE MATERIALS SHOULD BE AUTHENTIC, DURABLE AND INSTALLED WITH AN ATTENTION TO DETAIL.

## PUBLIC WORKS

Public works include a wide variety of infrastructure elements and facilities ranging in purpose from the purely utilitarian to the highly ornamental. Public roadways fall in the middle of this spectrum. They serve a basic utilitarian function by providing mobility and access and must be designed to meet safety and transportation service standards. But streets are also part of the built environment that people physically experience on a daily basis. The visual quality of the public right-of-way is critical. Streets within the Bridge Street District should function as an extension of the public open space system.

## GREAT STREETS

Great streets help to define great places by establishing an image and sense of identity. Streets that serve as major connections to and through the Bridge Street District provide an opportunity to “set the tone” for the district’s appearance. Streetscape elements and materials, when thoughtfully designed and installed, can be used to create a visual signature that represents a place. The use of consistent and coordinated palettes of materials, colors, textures and patterns will create a cohesive visual identity for the District.

## GREAT PLACES

Great places evolve through a combination of public and private actions that shape the physical environment. Businesses and residents will change over time, and with them, building facades, signs and outdoor spaces like patios and front yards. These changes add character that helps to create a sense of place. The design of public streets should incorporate consistent and coordinated elements, while private developments provide visual variety along the street.

## HIGH QUALITY

High quality, durable and aesthetically pleasing materials are critical to creating places where people are comfortable and enjoy spending time. Great streets should be designed like great parks, with attention to detail in all aspects of construction and installation. Quality materials like brick and stone add value to the public realm, transitions between different materials should be seamless, and no aspects of the street design should appear as an afterthought.



Great streets and memorable places can be achieved with a wide variety of design elements, as illustrated in the examples above. These guidelines outline design standards specifically developed for the Bridge Street District.



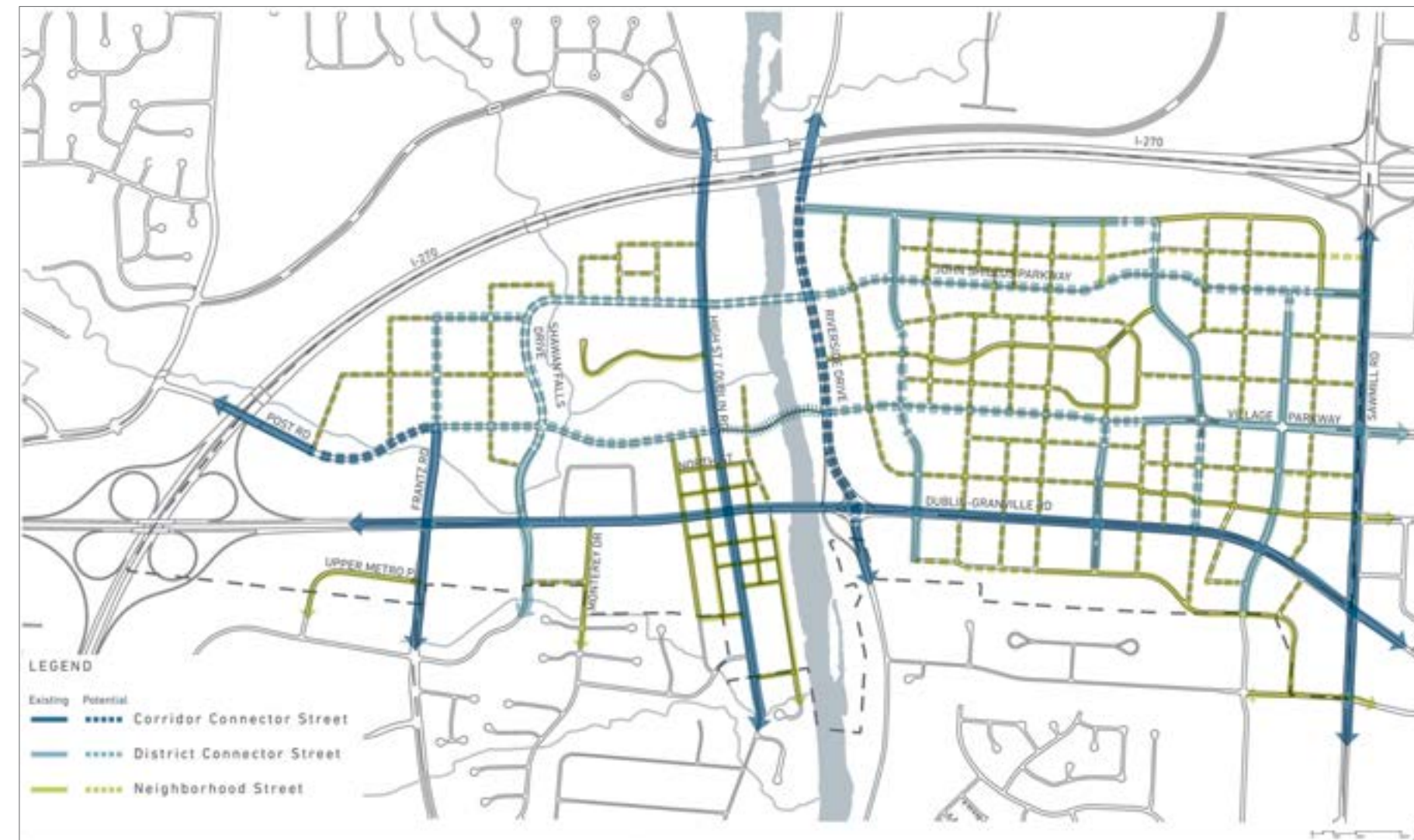
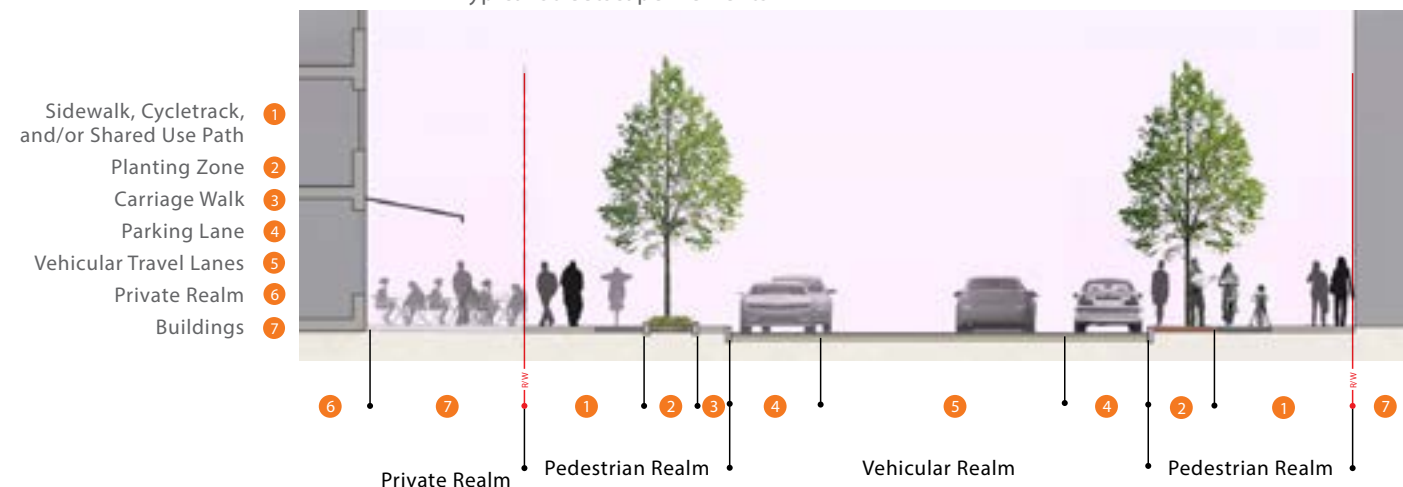
**The Street Grid** is a core element of the Bridge Street District Plan. As the district develops, the City and private property owners will partner to build new street connections, generally in the pattern illustrated in the Bridge Street District Deveopment Code. Streets in the Bridge Street District are classified into “families” that group streets with similar characteristics while accommodating different land use contexts and transportation needs. The map on the opposite page illustrates the general pattern of existing and potential streets in the District according to their street family classification. Refer to the Thoroughfare Plan within the Dublin Community Plan for a more detailed description of the street families.

The streetscape character guidelines build on the street family hierarchy and introduce a complementary framework of design recommendations within three categories of streetscape types:

- Signature Streetscapes
- Standard Streetscapes
- Historic Core Streetscapes

Pages 10-15 describe how each streetscape character type applies to the planned street network and illustrate the principal design elements for each character type. The diagram below illustrates the typical streetscape elements depicted for each streetscape type. Dimensions and locations of specific elements will vary throughout the Bridge Street District depending on location and development context. Pages 16-19 describe special conditions on select streets planned to include bicycle infrastructure and/or street-side greenways.

Typical Streetscape Elements

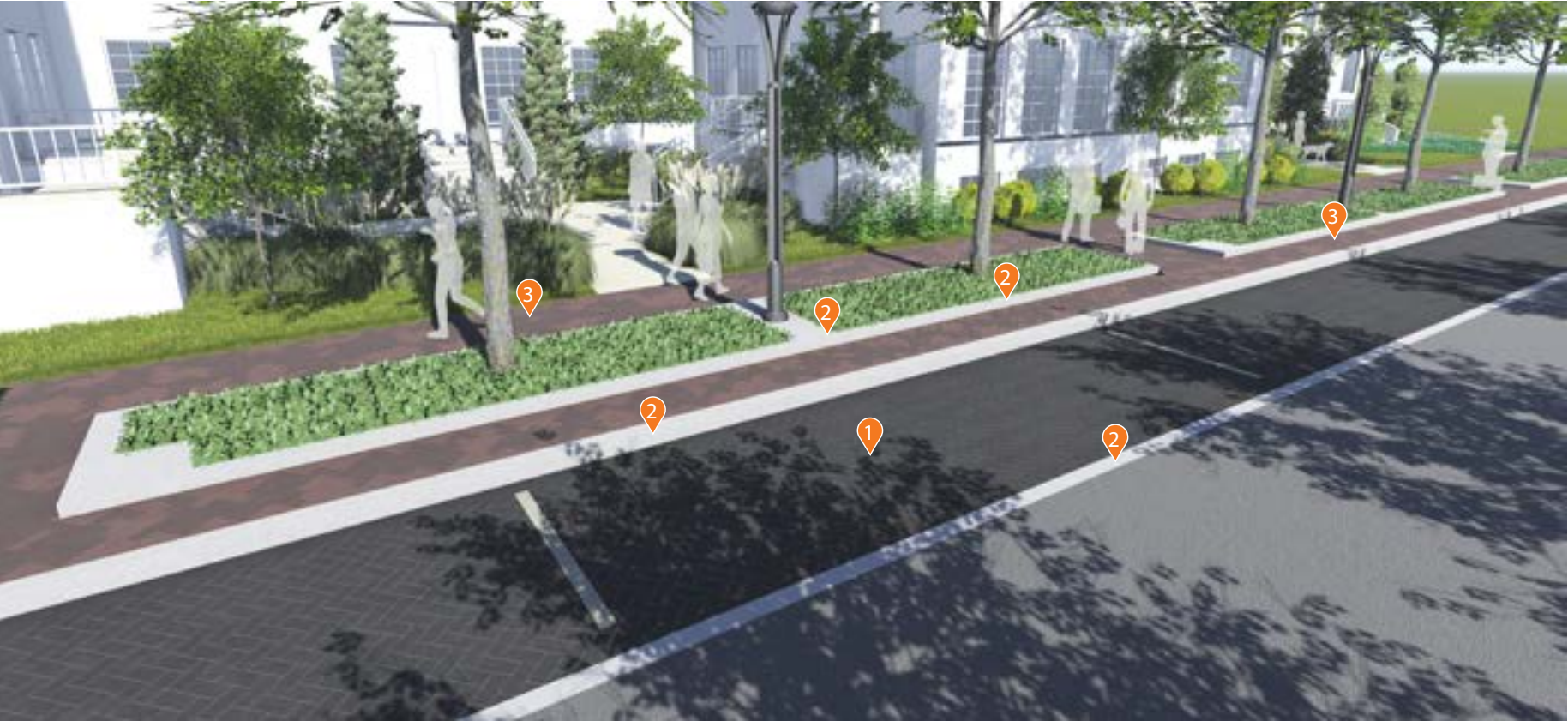


The Bridge Street District street network, classified by Street Family. Refer to the Dublin Thoroughfare Plan for more information about the Street Families.



**Signature Streetscapes** are used for the most prominent and heavily traveled streets in the Bridge Street District. They generally correspond with the Corridor Connector and District Connector Street Families identified in the Street Network Map. These streets provide major connections to and through the Bridge Street District and serve as high-visibility address corridors for new development, as envisioned in the Community Plan. Signature streetscapes set the tone for the district by establishing a high-quality, visually appealing character within the public realm.

This visual quality is expressed through the use of durable, natural materials. Brick pavers are used for sidewalks, carriage walks and parking lanes. Granite will be used for curbs, select pavement markings, and as an accent material to be coordinated with select street furnishings.

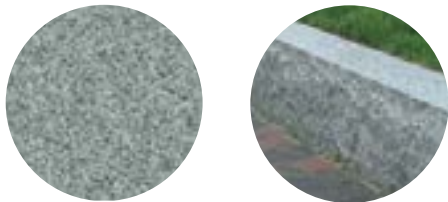


Refer to the Material Specifications Section for more detailed information about individual streetscape elements.

- 1 ENDICOTT CLAY PAVERS**  
TYPE/FINISH: Manganese Ironspot
- USE:  
On-Street Parking (Permeable)  
Sidewalks and Crosswalks



- 2 GRANITE CURB AND PAVERS**  
TYPE/FINISH: Georgia Grey
- USE:  
Street curb (vertical and flush)  
Street Pavement Markings  
Sidewalk/Planter Curb



- 3 ENDICOTT CLAY PAVERS**  
TYPE/FINISH: Dark Ironspot & Medium #46 ironspot Blend
- USE:  
Sidewalk Paver

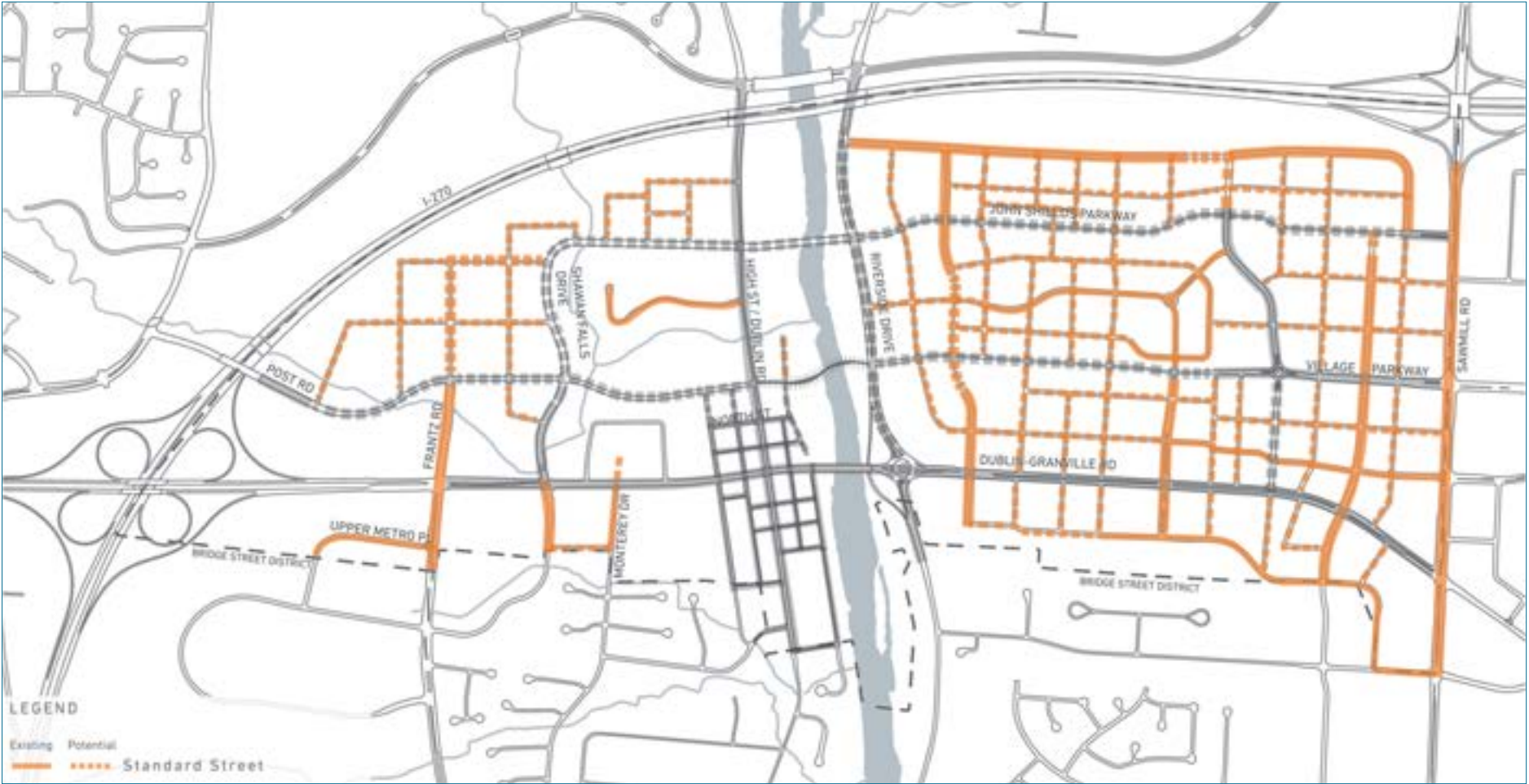




# Standard Streetscapes

generally correspond with the Neighborhood Street Family identified in the Thoroughfare Plan, but also include less prominent District Connector Streets. As the Bridge Street District develops, these streets will provide additional internal connections within neighborhoods, filling out the interconnected street grid envisioned in the plan.

The standard streetscape character type complements the design of signature streetscapes through the use of pervious brick pavers in parking lanes and granite for curbs, pavement markings and material transitions. Unlike the more prominent signature streetscapes, standard streetscapes use concrete for sidewalks rather than brick pavers. This subtle design difference reinforces the prominence and identity of the signature streets as the primary address corridors of the Bridge Street District.



Refer to the Material Specifications Section for more detailed information about individual streetscape elements.

## 1 ENDICOTT CLAY PAVERS

TYPE/FINISH: Manganese Ironspot

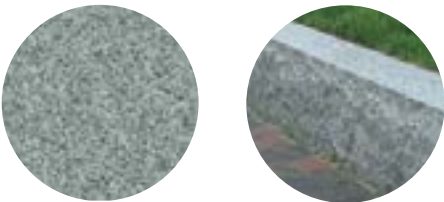
- USE:
- On-Street Parking (Permeable)
  - Sidewalks and Crosswalks



## 2 GRANITE CURB AND PAVERS

TYPE/FINISH: Georgia Grey

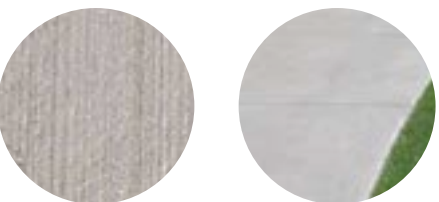
- USE:
- Street Curb (vertical and flush)
  - Street Pavement Markings
  - Sidewalk/Planter Curb



## 3 CONCRETE PAVING

TYPE/FINISH: Very Fine Broom Finish with Sawcut Joints

- USE:
- Sidewalks and Carriage Walks





**Historic Dublin** has long been recognized as the City of Dublin's cultural center and its streets have always had a distinct character from the rest of the city. This unique character will be retained as the Bridge Street District develops. The City has used special materials for streets in Historic Dublin for many years. These include the use of Belden brick pavers for sidewalks and an historic-style street lamp as opposed to the City's standard light fixture. These materials will continue to be used within the Historic core and will remain unique to Historic Dublin.

Over time, improvements to streets within Historic Dublin will provide opportunities for additional material enhancements and coordination with other streets within the Bridge Street District. The most notable of these is the potential to convert existing asphalt streets to brick paver surfaces, with granite curbs. If this option is implemented, the paver blend will coordinate with the blend used for sidewalks on signature streets, providing a visual contrast to the lighter brick used in Historic Dublin sidewalks.

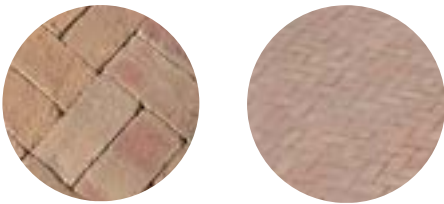


Refer to the Material Specifications Section for more detailed information about individual streetscape elements.

**1 BELDEN CLAY PAVERS**

TYPE/FINISH: Belcrest #530

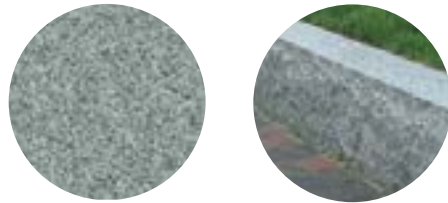
USE:  
Sidewalks  
Planter Curb



**2 GRANITE CURB AND PAVERS**

TYPE/FINISH: Georgia Grey

USE:  
Street Curb (Vertical and Flush)  
Street Pavement Markings  
Sidewalk/Planter Curb



**3 ENDICOTT CLAY PAVERS**

TYPE/FINISH: Dark Ironspot & Medium #46 Ironspot Blend

USES:  
Street Paver





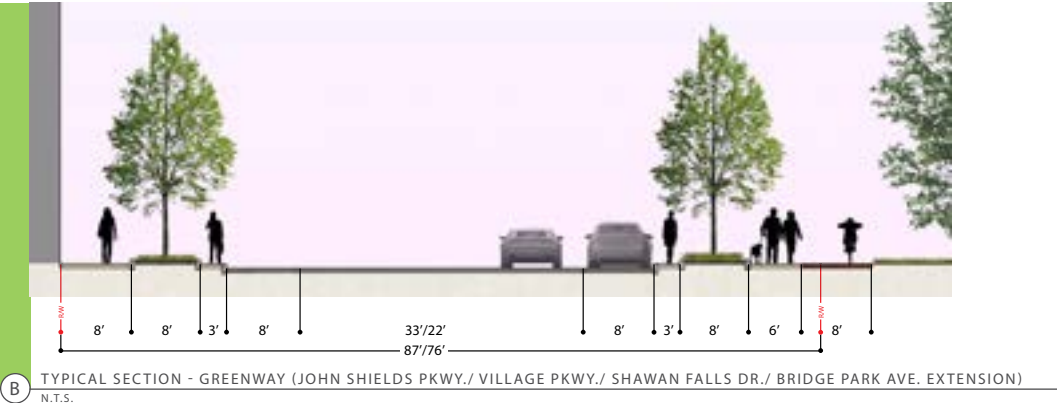
**The Bicycle Network** planned within the Bridge Street District will include a variety of on-street and off-street facilities. Within the core of the district, the bikeway network will include a central loop connecting destinations on both sides of the Scioto River with a planned bicycle/pedestrian bridge and future vehicular bridge. The central loop will include a combination of shared use paths and a cycletrack, an off-street path that provides a dedicated space for cyclists, distinct from the pedestrian sidewalk. The bicycle loop creates a special condition for certain signature streetscapes.

The location of the cycletrack or shared use path within the streetscape will vary depending on the surrounding development context. Portions of the route

will be located on street blocks with high concentrations of dining and retail establishments and high amounts of associated pedestrian traffic. In those situations, the cycletrack will be located adjacent to the streetscape planting zone, with the sidewalk directly adjacent to building facades and outdoor dining spaces, and accessible to ground floor storefronts (refer to Diagram A on the opposite page). The main portion of the loop will be located adjacent to a planned greenway system (refer to Diagram B), with a pedestrian sidewalk and cycletrack or shared use path between the streetscape planting zone and greenway. Along Riverside Drive, cyclists will use the shared path system through the planned riverside park (refer to Diagram C), providing connections between the pedestrian and vehicular bridges and beyond.



The bicycle system creates a special condition for certain streetscape designs. Refer to the diagrams on the opposite page for typical street sections.



Typical sections for select streets illustrating the location of bicycle facilities. Refer to key of typical streetscape elements on Page 8.

\* Planting Zone treatments may include tree grates, pavers or open planters. Planter widths, sidewalk widths and incorporation of curbside carriage walks will vary accordingly.



# The Greenway System

planned within the Bridge Street District, like the bikeway network, will vary in character depending on the surrounding context of adjacent development and streets. Much of the central greenway loop will run alongside the District's Signature Streets, including John Shields Parkway, Village Parkway, Bridge Park Avenue and Shawan Falls Drive.

In addition to adjacent streets, the greenway itself should function as a public way, providing an open space alternative for recreational and destination-oriented travel by foot and bicycle. The greenway will be designed with paths along its edges and providing connections through and across the open space. The experience for those traveling along the greenway should be anchored

by major destinations, including parks and mixed use activity centers. Along the way, smaller nodes of activity and resting areas should be provided, such as street intersections that serve as connection points to the greenway from surrounding neighborhoods.

Nearly all portions of the greenway will have public street frontage, providing visual and physical access to the space and helping to define the character of major streets within the District. Some portions of the greenway will be framed by adjacent architecture. In these cases, care should be taken to ensure the open space remains inviting to the general public, and is not perceived as part of the adjacent private development. In other locations, the greenway will expand into larger parks and natural areas, providing seamless connections to the larger open space system.



The greenway loop within the Bridge Street District links the east and west sides of the District as part of the larger open space system.



Design Principles for Bridge Street District Greenways

Conceptual greenway configuration along John Shields Parkway.



Specifications are provided for select streetscape materials intended for use in Bridge Street District. Designers should consult this resource when developing streetscape plans. Additional materials will be added to this section as the City determines preferred specifications. Refer to City of Dublin Engineering for more information.

Hardscape Materials

	Page
• Light Traffic Brick Pavers .....	21
• Heavy Traffic Brick Pavers .....	22
• Permeable Brick Paver .....	23
• Granite Paver .....	24
• Granite Curb .....	25

Street & Site Furnishings

• Cree ARE-EDG Street Lamp (Valmont Davits Pole) .....	26
• Cree ARE-EDG Street Lamp (Post Top) .....	27
• Sternberg Carson City Street Lamp .....	28
• Benches .....	TBD
• Bicycle Racks .....	TBD
• Bollards .....	TBD
• Waste/Recycling Receptacles .....	TBD
• Device/Vehicle Charging Stations .....	TBD
• Signs and Wayfinding .....	TBD

Landscape/Planting Details

• Street Trees .....	TBD
• Streetscape Planting Zone Treatments .....	TBD

Light Traffic Brick Pavers

Signature streetscapes will include sidewalks constructed with a light traffic paving brick manufactured by Endicott Clay Products. Light traffic paving bricks are square cut for a clean, smooth finish and should be installed hand tight. Endicott light traffic pavers conform with the requirements of ASTM C902, ‘Standard Specification for Pedestrian and Light Traffic or Paving Brick’, Weather Class SX, Traffic Type 1, Application PX.

MANUFACTURER

Endicott Clay Products Company

QUARRY LOCATION

Fairbury, Nebraska

DIMENSIONS

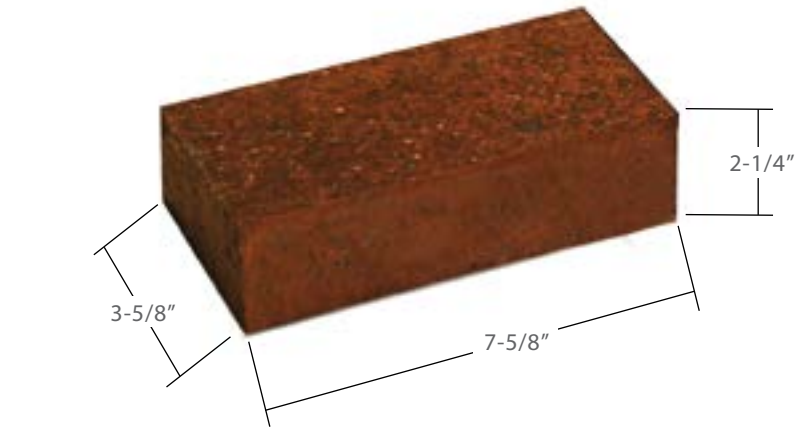
Thickness: 2-1/4 inches  
Face Size: 3-5/8 x 7-5/8 inches

COLOR

Equal mix of Dark Ironspot and Medium Ironspot #46

BRIDGE STREET DISTRICT USE

Signature Streets: Sidewalks and Carriage Walks



DARK IRONSPOT



MEDIUM IRONSPOT #46



# Heavy Traffic Brick Pavers

In locations where vehicular driveways are accessed from a signature street, the portion of the curb apron and driveway located within the public right-of-way will be constructed with a heavy vehicular paving brick, coordinated with the adjacent brick paver sidewalks. This paver unit may also be used as street pavers in the Historic Core. Endicott heavy traffic pavers conform with the requirements of ASTM C1272, ‘Standard Specifications for Heavy Vehicular Paving Brick’, Type R, Application PX.

**MANUFACTURER**  
Endicott Clay Products Company

**QUARRY LOCATION**  
Fairbury, Nebraska

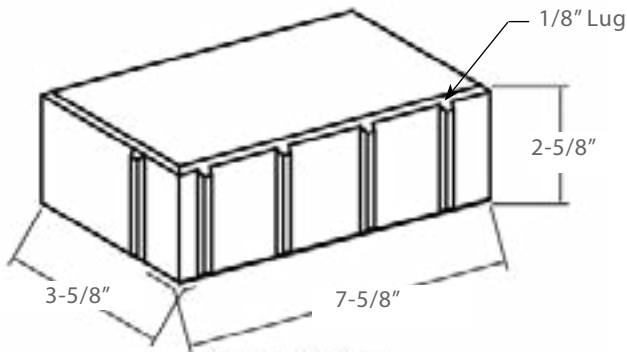
**DIMENSIONS**  
Thickness: 2-5/8 inches  
Face Size: 3-5/8 x 7-5/8 inches

**SPACER LUGS**  
1/8 inch

**EDGES**  
Chamfered

**COLOR**  
Equal mix of Dark Ironspot and Medium Ironspot #46  
Manganese Ironspot

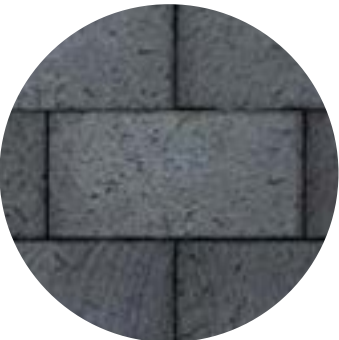
**BRIDGE STREET DISTRICT USE**  
Signature Streetscapes: Sidewalks and Carriage Walks  
Signature Streetscapes: Crosswalks (Manganese Ironspot)  
Historic Core Streetscapes: Street Paver



DARK IRONSPOT



MEDIUM IRONSPOT #46



MANGANESE IRONSPOT

# Permeable Brick Paver

On-street parallel parking lanes will be constructed with heavy vehicular paving bricks, designed and installed to allow for stormwater infiltration. Endicott permeable pavers are manufactured to accommodate heavy vehicular traffic and conform with the requirements of ASTM C1272, ‘Standard Specification for Heavy Vehicular Paving Brick’, Type F, Application PX.

**MANUFACTURER**  
Endicott Clay Products Company

**QUARRY LOCATION**  
Fairbury, Nebraska

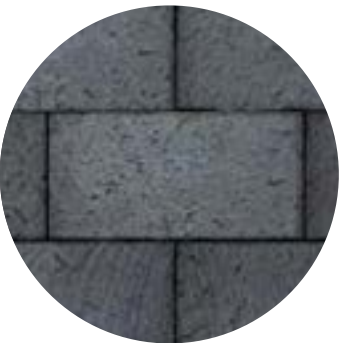
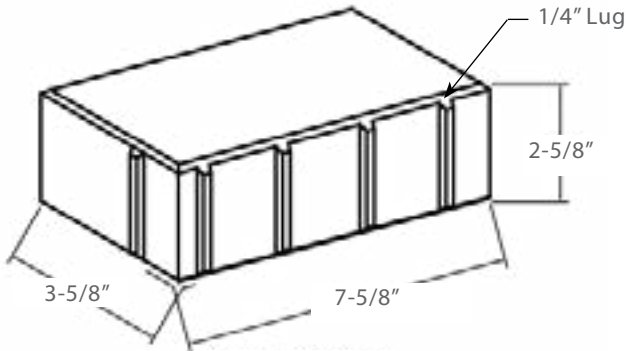
**DIMENSIONS**  
Thickness: 2-5/8 inches  
Face Size: 3-5/8 x 7-5/8 inches

**SPACER LUGS**  
1/4 inch

**EDGES**  
Chamfered

**COLOR**  
Manganese Ironspot

**BRIDGE STREET DISTRICT USE**  
Signature Streetscapes: Permeable on-street parking



MANGANESE IRONSPOT





# Granite Paver

Granite pavers will be used on all new streets in the Bridge Street District for pavement markings and base material at utilities, signs, light poles and other vertical elements in the streetscape.

MANUFACTURER  
North Carolina Granite Corporation

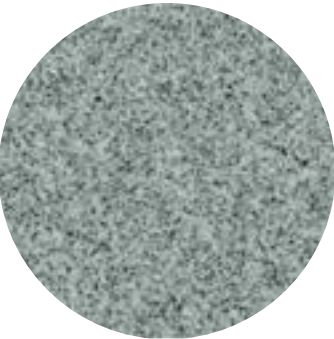
QUARRY LOCATION  
Elberton, Georgia

DIMENSIONS (vary)  
Thickness: 2-1/4 inches  
Face Sizes: 5-5/8 x 23-5/8 inches  
3-5/8 x 7-5/8 inches  
17-5/8 x 17-5/8 inches

COLOR  
Top: Thermal  
Sides: Sawn  
Bottom: Sawn

COLOR  
Georgia Gray (Mt. Airy alternate)

BRIDGE STREET DISTRICT USE  
Signature Streetscapes: Pavement markings and trim at utilities, signs, light poles and other vertical elements in sidewalks and carriage walks



GEORGIA GRAY GRANITE

# Granite Curb

Granite curbs (vertical installation) and bands (flush installation) will be used on all new streets in the Bridge Street District. Flush bands will serve as an edge restraint between clay pavers and asphalt surfaces. Vertical planter curbs will be used as an edge surrounding streetscape planting beds between the carriage walk and sidewalk on signature streets.

MANUFACTURER  
North Carolina Granite Corporation

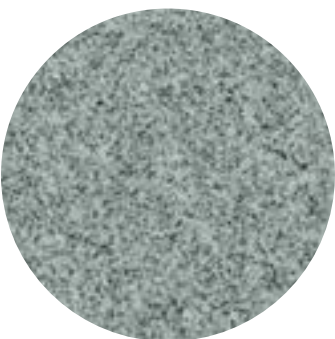
QUARRY LOCATION  
Elberton, Georgia

DIMENSIONS (VARY)  
18 x 6 inches  
15 x 6 inches  
12 x 6 inches

FINISH  
Top: Thermal  
Front/Back: Split  
Sides: Sawn  
Bottom: Sawn

COLOR  
Georgia Gray (Mt. Airy alternate)

BRIDGE STREET DISTRICT USE  
Signature Streetscapes: Street curbs, flush bands, planter curbs  
Standard Streetsscapes: Street curbs, flush bands  
Historic Core Streetscapes: Street curbs



GEORGIA GRAY GRANITE



# Cree ARE-EDG Street Lamp (Valmont Davits Pole)

The Cree Edge Series luminaire will be used in combination with a 30-foot tall Valmont pole for multi-lane streets in the Bridge Street District (not including Historic Dublin streets). Spacing and photometrics will vary by location.

MANUFACTURER  
Cree (Luminaire), Valmont (Pole)

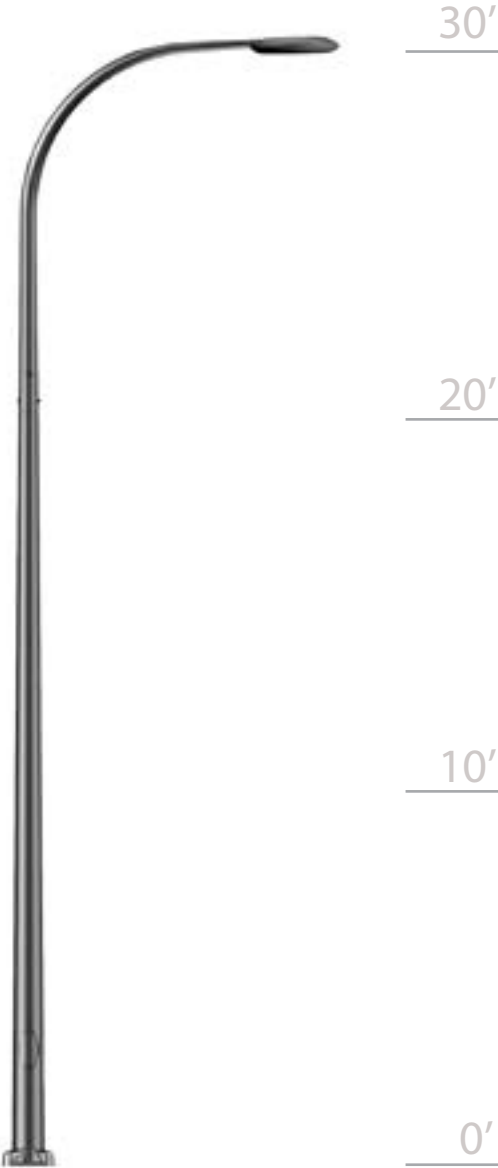
LUMINAIRE  
ARE-EDG Model  
LED  
Edge Area Square

POLE  
Davits Model  
Round, tapered aluminum pole

COLOR/FINISH  
Black, powder-coated

HEIGHT  
30 feet

BRIDGE STREET DISTRICT USE  
Multi-lane Streets



# Cree ARE-EDG Street Lamp (Post Top)

The Cree Edge Series post-top style luminaire will be used in combination with a 16-foot tall pole for two-lane streets in the Bridge Street District (not including Historic Dublin streets). Spacing and photometrics will vary by location.

MANUFACTURER  
Cree (Luminaire), Valmont (Pole)

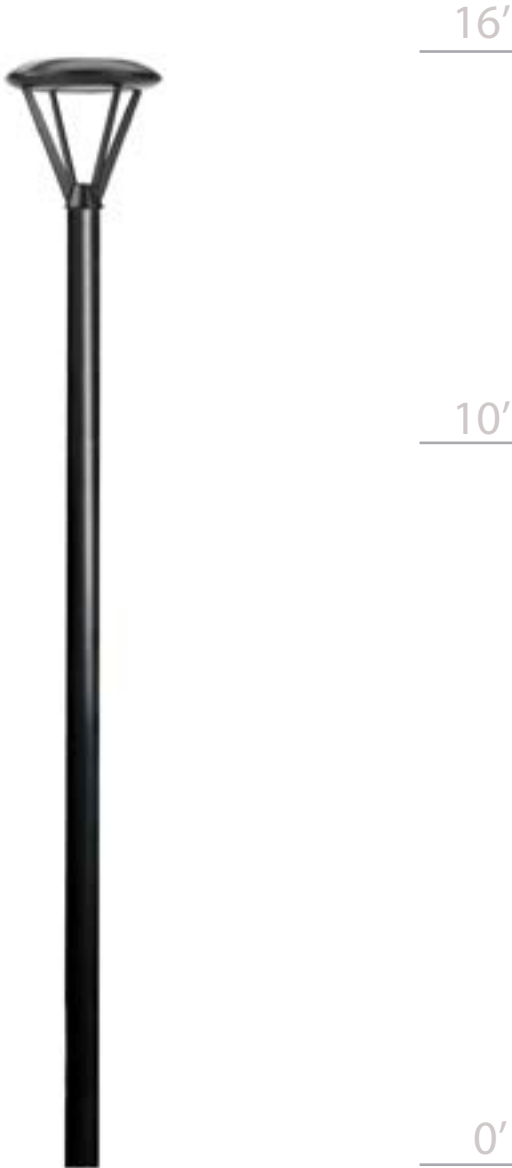
LUMINAIRE  
ARE-EDG Post Top Model  
LED  
Edge Area Round

POLE  
Round, tapered aluminum pole

COLOR/FINISH  
Black, powder-coated

HEIGHT  
16 feet

BRIDGE STREET DISTRICT USE  
Two-lane Streets





# Sternberg Carson City Street Lamp

The Sternberg traditional-styled street lamp is used on streets in Historic Dublin. Spacing and photometrics vary by location.

MANUFACTURER  
Sternberg

LUMINAIRE  
Carson City Model  
LED

POLE  
Round, tapered aluminum with pedestal base

COLOR/FINISH  
Black, powder-coated

HEIGHT  
12 feet

BRIDGE STREET DISTRICT USE  
Historic Core Streets



12'

0'

Additional Material Specifications to be added



