

## PLANNING REPORT

# Planning and Zoning Commission

Thursday, February 5, 2026

## Monterey Square 25-093PDP

<https://dublinohiousa.gov/pzc/25-093/>

### Case Summary

Address	191 W. Bridge Street, 201 W. Bridge Street, and PIDs: 273-000205, 273-000207, 273-000209, 273-000210, 273-000211, 273-000212, 273-000213, 273-000214, 273-000215, 273-000216, 273-000217
Location	Both sides of Monterey Drive, south of its intersection with W. Bridge Street
Proposal	Construction of a mixed-use development on a 6.86-acre site.
Request	Review and approval of a Preliminary Development Plan under the provisions of Zoning Code Section 153.066.
Zoning	BSD-HTN, Bridge Street District – Historic Transition Neighborhood
Planning Recommendation	<a href="#">Approval of the Preliminary Development Plan with Conditions</a>
Next Steps	Upon review and approval of the Preliminary Development Plan, the applicant may file Final Development Plan, Preliminary Plat and Final Plat applications.
Applicant	The Wood Companies City of Dublin
Case Manager	Bassem Bitar, AICP, Deputy Director of Planning (614) 410-4635 <a href="mailto:bbitar@dublin.oh.us">bbitar@dublin.oh.us</a>



Community Planning and Development



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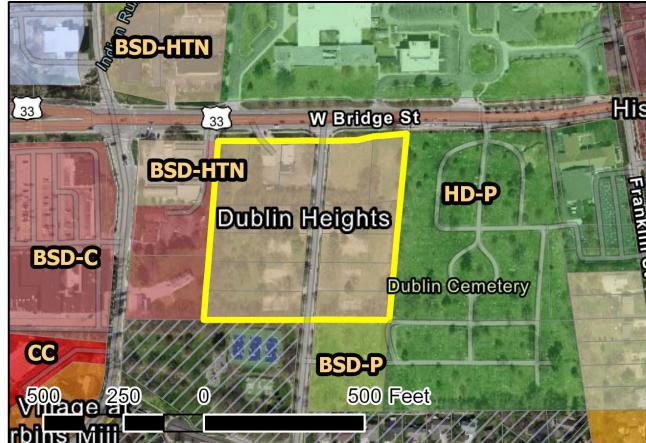
## Site Location Map

## 25-093PDP - Monterey Square



## Site Features

- 1 Shell Gas Station
- 2 City-owned Parcel
- 3 Proposed Cemetery Expansion



## 1. Request and Process

### Request

The applicant is requesting review and approval of a Preliminary Development Plan for the construction of a mixed-use development consisting of approximately 140,000 square feet of retail, office, and eating and drinking space. The uses would be distributed among several buildings on the 6.86-acre site. A "town square" is proposed on the west side of Monterey Drive, and a three-level parking structure is proposed on the east side of the site. The southeast corner of the site is proposed to be dedicated to the City for expansion of the adjacent Dublin Cemetery, and a new public street extending from Monterey Drive to the west property line is proposed to be constructed to accommodate future connectivity to Corbins Mill Drive.



### Process

The Preliminary Development Plan (PDP) is the second of three steps in the development process, as outlined below:

1. *Concept Plan (CP) – Planning & Zoning Commission (PZC) Consideration (non-binding feedback)*
2. *Preliminary Development Plan (PDP) – PZC Determination*
3. *Final Development Plan (FDP) - PZC Determination*

The purpose of the PDP is to establish a framework for the proposed development that is consistent with the requirements of the Community Plan, Bridge Street District (BSD) Special Area Plan, BSD Code, and other adopted plans, policies, and regulations. It is intended to establish the direction of the proposed development and serve as the basis for submittal of the FDP.

Preliminary and Final Plats for the development will also be considered in conjunction with the FDP and are subject to final determination by City Council.

## 2. Background

### Site Summary

The 6.86-acre site consists of 13 parcels along both sides of Monterey Drive, south of its intersection with W. Bridge Street. The individual parcels are owned by Dublin Development LLC, except the parcel at the southeast corner of W. Bridge Street and Monterey Drive, which the City of Dublin owns. A Shell gas station and convenience store currently occupy the two parcels at the southwest corner of the intersection. The rest of the site is vacant, with a previous gas station located on the city-owned parcel and previous duplexes located on the southern parcels having been demolished in recent years. Several stands of mature trees are located on the site, especially along the west and south property lines and across the rear of the parcels that front on W. Bridge Street.

The Dublin Cemetery borders the site to the east and extends along a portion of the south property line. Monterey Park is located south of the site, west of Monterey Drive. To the west of the site are a Marathon gas station, a home goods store, and a single-family residence, all fronting Corbins Mill Drive. Sells Middle School is located across W. Bridge Street to the north.

### History

#### *December 2023 – Case 23-127INF*

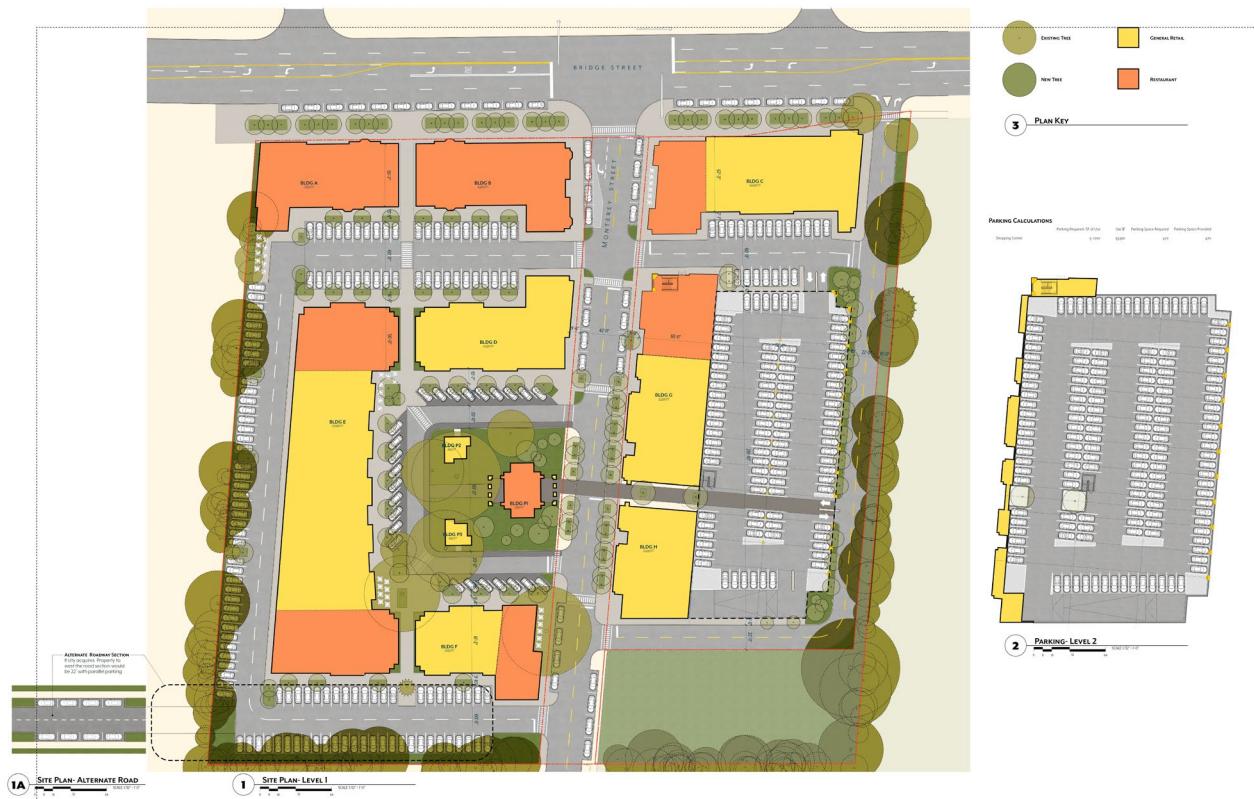
PZC held an Informal Review and provided non-binding feedback on a mixed-use proposal by Crawford Hoying that included 20,000 square feet of first-floor retail along the W. Bridge Street frontage, along with office or multi-family residential uses above. The proposal also included 40 single-family attached units to the south, organized around two new east-west streets. Open space was proposed along the east property line, including approximately one acre to be dedicated to cemetery expansion. Commission members were generally supportive of the uses and overall site layout, given the transition provided between the existing single-family detached uses to the south and the busier Bridge Street corridor to the north. Members noted the need for improvements to the design and massing of the single-family attached structures and further refinements to the traffic management details, open space location and design, and other site details.

#### *July 2024 – Case 24-086CP*

PZC reviewed a Concept Plan proposed by Crawford Hoying for a mixed-use development including 19,400 square feet of commercial space along the W. Bridge Street frontage and 53 attached single-family units to the south. A new east-west public street was proposed west of Monterey Drive to enable future connectivity to Corbins Mill Drive. Open space was proposed in the form of two pocket plazas between the commercial and residential areas, as well as a small expansion of Monterey Park. Space for cemetery expansion was shown at the southeast corner of the site. PZC recommended to City Council approval of the Concept Plan with seven conditions focusing on adding taller elements to the buildings fronting on W. Bridge Street; significantly enhancing the design of the residential buildings; refining the circulation to address emergency access and facilitate the integration of uses; enhancing the open space to include highly visible and interconnected spaces; preserving as many trees/tree lines as possible; finalizing site design details to address BSD code requirements and the site's unique setting; and coordinating with Transportation & Mobility to finalize the public street streetscape and intersection design. The project did not proceed beyond this stage.

June 2025 – Case 25-043CP

PZC reviewed and provided non-binding feedback on a Concept Plan proposed by the Wood Companies for a commercial development consisting of approximately 93,900 square feet of retail and eating and drinking space distributed among several 1-2-story buildings arranged in an urban pattern with a central square. The proposal included a two-level parking structure on the east side of the site with the first level at grade and the second level extending over the first-floor commercial space. Service drives were proposed to provide access to the parking structure as well as several surface parking spaces, including ones surrounding the central square. A drive at the southwest corner of the site was designed with the possibility of future conversion to a public street to connect to Corbins Mill Drive. On-street parking was proposed along both Monterey Drive and W. Bridge Street.



Concept Plan – 25-043CP

The Commission was generally supportive of the project including the layout and open space. Members were also appreciative of the applicant's communication with the neighbors. There were concerns raised about the lack of mix of uses with suggestions for incorporating some residential and/or office uses. Concerns were also voiced about traffic and the need for an additional access point to the west, but the Commission was not supportive of taking away park space to accommodate that access. Some suggestions included additional height on the buildings along W. Bridge Street, and a pedestrianway through Building E to break up its mass.

## Neighborhood Engagement

The applicant met with the Waterford Homeowners Association leadership and held a neighborhood meeting to introduce the project on May 22, 2025. The HOA leadership shared that a majority of attendees were generally supportive of the proposal, especially since it does

not include multi-story or high-density residential. However, they expressed concerns about cut-through business traffic into the neighborhood, noting that they felt the project would not work without a traffic signal at the intersection of Monterey Drive and W. Bridge Street. Since the Concept Plan review, the applicant has continued to communicate with the HOA and has also spoken with the owner of the residential property directly west of the development site to discuss any concerns or specific buffering needs. The updated plans include some fencing and landscaping that were requested by that property owner.

### 3. City Plans and Policies

#### Envision Dublin Community Plan

The Community Plan is a key policy document used to guide decision-making regarding the future of Dublin's natural and built environment. It assists in evaluating development proposals and helps ensure that proposed development supports the community's long-term objectives.

##### *Future Land Use*

The Future Land Use (FLU) designation for this site is Mixed Use Village, which is intended to be a small-scale, pedestrian-oriented district developed with respect to the historic building context and character. The principal uses include office, retail, commercial, civic buildings, and single-family residential. Supporting uses include multi-family residential. Building height ranges from 1 to 3 stories, with massing and scale that are compatible with nearby historic structures.

##### *Bridge Street District Special Area Plan*

While the future land use plan ensures compatible and coordinated growth throughout Dublin, in key areas of the city where substantial development or redevelopment activity is likely, special area plans provide an illustrative framework for additional development design guidance. The plans are general guides and are not necessarily to be interpreted or applied literally.

The subject site is located within the Bridge Street District Special Area Plan and identified as a key site. The plan notes that it offers an opportunity to transition from the activity along W. Bridge Street to the quaintness of the Waterford Village neighborhood. It recommends that development on the site should include vertical, mixed-use buildings along W. Bridge Street at a height of two to three stories. South of the frontage, multi-family residential uses, such as townhomes, are noted as appropriate transitional uses to Monterey Park and the neighborhood. The plan also contemplates additional street connections for future development along Corbins Mill Drive.

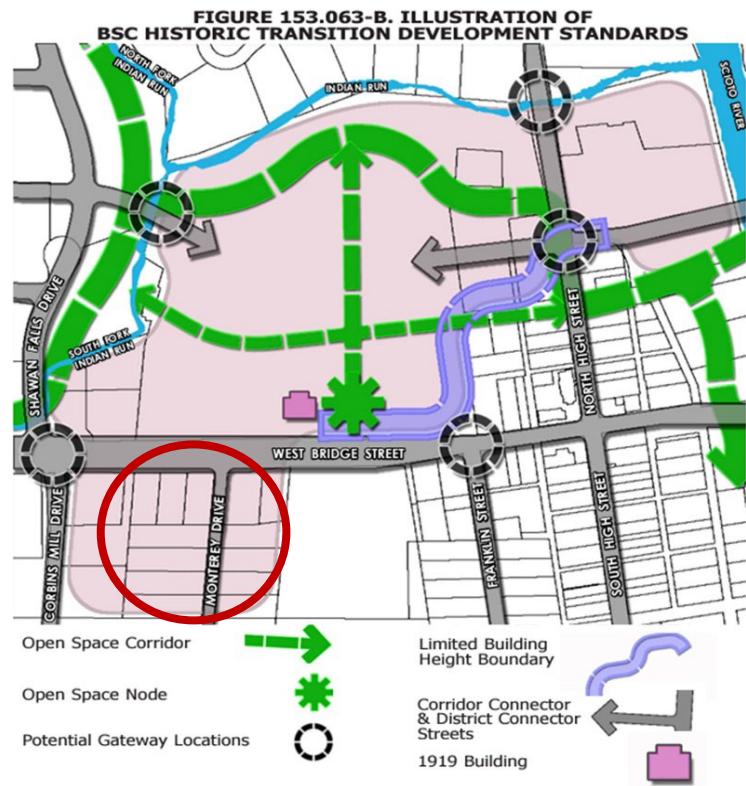


#### Bridge Street District

The Bridge Street District (BSD) Code establishes Neighborhood Districts where special attention to location and character of buildings, streets, and open spaces is important to fulfill the objectives identified in the BSD Special Area Plan within the Community Plan. Each neighborhood anticipates the conceptual layout of critical elements, including street connections, open spaces, and gateways.

### Neighborhood Standards

The site is zoned BSD-HTN, Historic Transition Neighborhood District. This district complements the HD Historic Core district by accommodating a variety of building types within a finer-grained street and block network. It also accommodates uses similar to those in the HD Historic Core district. Development allows an extension of the walkable mixed-use character of the HD Historic Core district on the larger parcels within this district. The district is subject to the specific neighborhood standards defined in the City Code. These requirements establish open space patterns and location requirements for building types, provide additional residential opportunities, and extend the small-scale commercial activities of the HD Historic Core district.



### Street Network, Lots, and Blocks

The Code provides a hierarchy of requirements for establishing a gridded street network. The Street Network Map, part of the Thoroughfare Plan, identifies three families of streets: 1) Corridor Connectors, 2) District Connectors, and 3) Neighborhood Streets. Several streets are also listed as Principal Frontage Streets, which carry additional requirements.

The site currently has access via W. Bridge Street (a Corridor Connector/ Principal Frontage Street) and Monterey Drive (a Neighborhood Street). Principal Frontage Streets (PFS) are designated to ensure a continuous, pedestrian-oriented block. Development along a PFS should include front-facing building facades and limited conflicts between pedestrian and vehicular traffic. Driveways are not permitted along a PFS without City evaluation and approval.

## 4. Project

The proposal continues to include several 1-2 story buildings arranged in a traditional walkable urban pattern with a central square. Since the Concept Plan review, the applicant has made various changes to the plan to address comments raised by PZC and staff. Notable ones include the following:

- Addition of second-floor occupied space along the W. Bridge Street frontage and the east side of Monterey Drive. Approximately 39,000 square feet of office space would occupy the upper floor areas.
- Increase in the parking structure height from two to three levels.
- Elimination of the previously proposed right-in/right-out curb cut along W. Bridge Street near the northeast corner of the site.

- Inclusion of a traffic signal at the intersection of W. Bridge Street and Monterey Drive based on a traffic study submitted by the applicant indicating that it is warranted. The traffic study is currently being evaluated by the City.
- Elimination of the service drive along the east side of the parking structure to help preserve some of the trees and create an 18'-wide buffer from the adjacent cemetery.
- Dedication of 10 feet of right-of-way along Monterey Drive, increasing the right-of-way width from 50 feet to 60 feet per the Envision Dublin Multi-Modal Thoroughfare Plan.
- Dedication of 60 feet of public right-of-way along the south side of the site, west of Monterey Drive to accommodate a public street with future connectivity to Corbins Mill Drive.
- Better definition of the tract to be transferred to the City for cemetery expansion. This is now fully limited to the southeast corner of the site, with acreage equal to that of the City-owned parcel at the southeast corner of W. Bridge Street and Monterey Drive that is proposed to be included in the development.
- Elimination of the service drive surrounding the town square, enabling it to be reserved for pedestrian activities.
- Offsetting some of the pedestrianways/pathways leading to the town square to create varied spaces/experiences.
- Introduction of a pedestrianway through Building E on the West side of the site, splitting it into two buildings (E1 and E2).
- Addition of an exterior stairway to the parking structure near the Monterey Drive frontage to add a focal point.

Below is a discussion of the project details.

## USE

The proposed development includes  $\pm 65,000$  square feet of ground-floor retail,  $\pm 30,500$  square feet of ground-floor eating and drinking space, and  $\pm 39,000$  square feet of second-floor office. The second floor of Building H (southernmost building on the east side of Monterey Drive) is also anticipated to house  $\pm 5,000$  square feet of eating and drinking space on the second floor.

All of these uses are permitted within the BSD-HTN district as are the proposed accessory uses, including structured and surface parking as well as outdoor seating. The uses are also consistent with those permitted in the adjacent Historic Core District to the east.

## STREETS

The proposed development would include improvements to W. Bridge Street and Monterey Drive as well as the construction of a new Neighborhood Street extending from Monterey Drive to the west property line. Based on a traffic study completed by the applicant (and under review by the City), a new traffic signal at the intersection of W. Bridge Street and Monterey Drive is included in the proposal. The other improvements to each street are outlined below.

### *W. Bridge Street*

The applicant is proposing streetscape improvements largely consistent with the Signature Streetscape details as outlined in the Bridge Street District Streetscape Character Guidelines (brick sidewalks with planting beds) although the curb material is shown as concrete and will need to be granite. On-street parking is also proposed along the street frontage. The feasibility of this parking will be subject to the findings of a separate study of the W. Bridge Street corridor, which the City is undertaking. The applicant is also proposing the dedication of



a triangular tract near the site's northeast corner for public right-of-way per the Envision Dublin Multi-Modal Thoroughfare Plan. This would make the right-of-way line more uniform. It will need to be further evaluated in coordination with the corridor study to determine of any additional right-of-way is needed.

#### Monterey Drive

As noted, the applicant is proposing to dedicate 10 feet of right-of-way to achieve the required right-of-way width of 60 feet. Striped on-street parking and street trees are proposed along both sides of the street with a mid-block crosswalk leading the town square. The sidewalks and crosswalk are proposed to be brick, while the trees are proposed to be planted in curb extensions straddling the on-street parking spaces. Concrete curbs are also proposed. This is different from the Standard Streetscape outlined in the BSD Streetscape Character Guidelines, which calls for concrete sidewalks, granite curbs, brick paver on-street parking, and tree grates or planters (with trees spaced at 40' intervals). However, it is generally similar to nearby streets in the Historic District. For that reason, staff is supportive of the proposed approach, although the design details will need to be finalized in coordination with staff.

It should be noted that the plan shows two north-bound left-turn lanes near the intersection with W. Bridge Street. These should be limited to one in order to reduce the crosswalk distance

and enhance the pedestrian experience, similar to the Riverside Drive intersection with Bridge Park Avenue and with John Shields Parkway.

#### *New Neighborhood Street*

This street is intended to eventually extend west to Corbins Mill Drive to provide connectivity per the BSD Special Area Plan and the codified BSD Street Network. The design of the north side of the street is generally consistent with the Standard Streetscape, including concrete sidewalks with tree grates and on-street parking (although concrete curbs and asphalt parking surfaces are proposed). The south side abuts Monterey Park, and further study of how the sidewalk width, connectivity to the park, and preservation of the trees in that area will need to take place in coordination with staff.

#### **LOTS AND BLOCKS**

The BSD Historic Transition Neighborhood District limits the maximum block length to 300 feet and the maximum block perimeter to 1,000 feet. The block on the west side of Monterey Drive will eventually be completed when the new neighborhood street is extended to Corbins Mill Drive, although the block length and perimeter would exceed the limits. The block on the east side is partial and cannot be completed due to the location of the cemetery and the configuration of the existing residential streets to the south. Given the existing conditions and the proposed walkable site layout, Waivers are justified and will be further detailed at the Final Development Plan stage (FDP).

#### **SITE LAYOUT**

The overall layout is consistent with the goal of this district to organize development around a fine-grained street and block network. Ideally, the layout would be flipped 180 degrees so that the parking structure is on the western end and the other buildings better relating to the Historic District and the Dublin Cemetery to the east. The applicant has indicated that several scenarios were explored to adjust the site plan accordingly but, due to the existing site dimensions, they could not achieve the town square concept while shifting the location of the parking structure. Below is a discussion of the elements of the site plan as proposed.

#### *Required Building Zone (RBZ)*

The Historic Mixed-Use Building Type is proposed for all buildings, except for the parking structure. The required RBZ along the front property line is 0'-20', while the corner side RBZ is 0'-10'. Since W. Bridge Street is a Principal Frontage Street, it is considered the front property line, while Monterey Drive is the corner side property line. The new neighborhood street acts as another front property line. All buildings appear to meet the RBZ requirements except for Building E-1, which is separated from Monterey Drive by the town square. The building arrangement helps define that square and justifies a Waiver. Details will be finalized at FDP.

#### *Front Property Line Coverage*

The Historic Mixed-Use Building Type requires a minimum building coverage of 80% within the front property line RBZ. This is met along the W. Bridge Street frontage, but not along the new neighborhood street (72%). This is mostly due to the location of the surface parking between the building and the west property line. The closest parking space is approximately 15 feet from the right-of-way line and should be screened in accordance with the vehicular use screening requirements. Staff is supportive of an Administrative Departure at FDP, but

recommends that street walls, parallel to the street, be added to the landscape islands west of the building to provide screening.

*Setbacks*

There are no minimum side or rear yard setbacks associated with the Historic Mixed-Use Building Type. However, perimeter landscape buffer requirements apply at certain locations as further detailed in the Open Space and Landscaping section below. The Parking Structure Building Type requires 5-foot minimum side and rear yard setbacks. As proposed, the parking structure would be located 18 feet from the east (cemetery) property line, but an entrance ramp and access drive would be built along the south property line abutting the cemetery expansion area. An appropriate setback/buffer area should be provided at this location to screen the cemetery as noted in a proposed condition of approval.

*Lot Coverage*

The maximum permitted impervious lot coverage for the Historic Mixed-Use Building Type, which applies to the west side of Monterey Drive, is 85% with an additional semi-pervious coverage of 5%. The proposed impervious lot coverage is approximately 88%, which requires an Administrative Departure.

The maximum impervious lot coverage for the Parking Structure Building Type, which applies to the east side of Monterey Drive, is 80% with an additional semi-pervious coverage of 10%. The proposed impervious lot coverage is 91%, which requires a Waiver.

The final details will be verified/addressed at FDP.

**Parking**

*Location*

Three types of parking are proposed: structured (311 spaces), on-street (45 spaces), and on-site surface parking (105 spaces). The latter is proposed to be located on both sides of the internal service drives. The service drive east of Monterey Drive would dead end into the cemetery buffer, while the one on the west side would loop around the buildings framing the town square. Parking is only permitted to the rear of Historic Mixed-Use buildings. In this case, some of the parking is located along the sides of Buildings D, E-1 and E-2, so a Waiver will be required at FDP and is justified by the site layout/design.

The drive aisles are proposed to be 24 feet wide, while code allows them to be as narrow as 22 feet. The applicant has expressed preference for the wider aisles given their experience with other developments. However, due to the concern about the development's proximity to the property lines (as further noted below), staff recommends limiting the width to 22 feet unless it impedes access by emergency vehicles. It should be noted that Code requires a minimum 4-foot clear walkway area and at least 2½' between a curb and any property line, planting area, street, or building.

Several trash enclosures are shown along the service drives, although it is not clear whether adequate space for pick up vehicle maneuverability is provided. This will need to be addressed at FDP.

*Number*

The total number of proposed parking spaces (461) is less than the 648 required (based on the square footage of the individual uses), but the project could potentially be eligible for up to 30% reductions based on transit proximity, shared parking and other parking reductions as outlined in the code. This would bring the total requirement to 454 spaces. Final details, likely including a Parking Plan will be examined at FDP as would compliance with the bicycle parking requirements.

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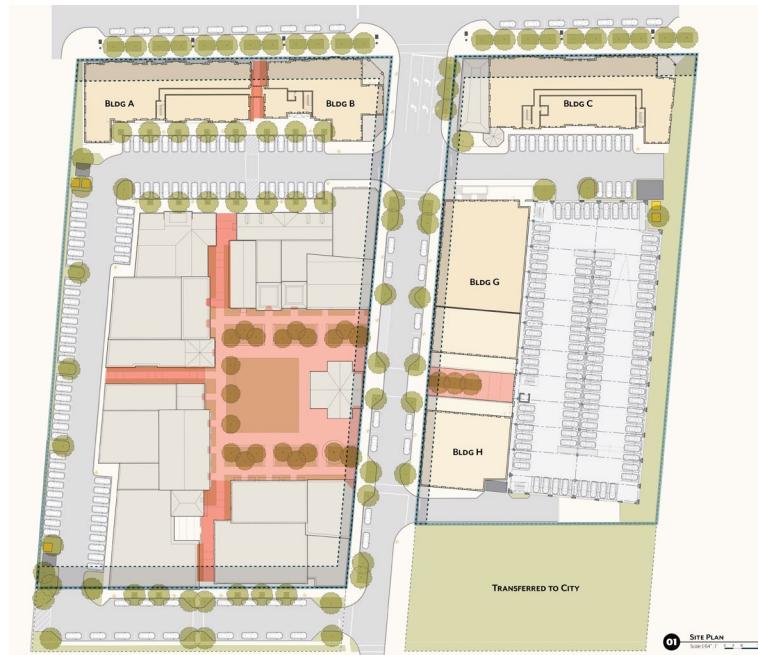
The applicant is proposing one 9'x46' loading zone along the west side of Monterey Drive. This will need to be further analyzed by City staff, along with the Washington Township Fire Department prior to FDP.

**STORMWATER AND UTILITIES**

The applicant is proposing underground stormwater chambers to address the applicable requirements. Additional details will need to be finalized for both public and private improvements in coordination with the City's Engineering staff prior to FDP to properly account for known capacity concerns in the existing storm sewer system.

**OPEN SPACE AND LANDSCAPING**

Code requires one square foot of publicly accessible open space per 50 square feet of commercial space. The required open space shall be located within 660 feet of the main entrance to the commercial space as measured along a pedestrian walkway. Based on the proposed commercial square footage, approximately 2,790 square feet of public open space is required. The proposed plan includes a total of 23,400 square feet of open space within the town square and a Pocket Plaza across the street. All are within the permitted distance from all building entrances. Pedestrianways providing connectivity to the town square are proposed to include additional public amenities. While the proposed public open space square footage far exceeds the required minimum, the most applicable Open Space Type for the town square is a Plaza, which requires a minimum area of .25 acres and of the total area of the square. In this instance, the area requirement is met, although any private amenities associated with Building P would have to be excluded. At approximately 2,150 square feet, the Pocket Plaza on the east side of Monterey Drive exceeds the maximum area for this Open Space Type but provides a logical amenity across from the town square. A Waiver will likely be required at FDP as the details are finalized. The proposed open space plan meets the intent of the BSD Historic Transition Neighborhood District, including connectivity to W. Bridge Street and Monterey Park via the pedestrianways.



The proposed development would necessitate the removal of a large number of trees from the site, most of which are in good condition. In order to qualify for tree replacement exemptions, the applicant will need to explore all available alternatives to retain as many healthy/desirable trees as possible. The perimeter trees along the east side (adjacent to the cemetery) are proposed to be retained as are some of the perimeter trees along the west and south sides of the site. However, given the close proximity of the proposed parking and underground stormwater facilities to the west property line, these improvements would be well within the 15' Critical Root Zone of trees on this and adjacent properties, which would be unlikely to survive the level of impact. For that reason, staff recommends that an increased buffer be provided along the west property line with details to be finalized at FDP. Similarly, the impact on the trees along the north side of Monterey Park should be further examined as the new Neighborhood Street details are completed. Proposed conditions of approval addresses these.

It should be noted that the BSD code also requires perimeter landscape buffering when a development in any BSD zone abuts property in a non-BSD zone, or when a non-residential development is adjacent to a parcel containing only single family detached building types. In this case, the cemetery to the east of the site is zoned Historic Public, so additional buffering will be required with details to be finalized at FDP.

A parcel containing a single family detached residence (zoned BSD-Commercial) also abuts the southern portion of the site's west property line. Code requires a minimum buffer width of 10 feet containing one tree per 40' of lineal boundary plus a continuous 6-foot-high planting, hedge, fence, wall or earth mound. As currently proposed, the parking lot pavement adjacent to this parcel is in very close proximity to the property line. The applicant is proposing a 6-foot-high fence and some plantings to be located on the adjacent parcel, noting that this was based on discussions with that property owner. As noted above, staff recommends an increased buffer along the west property line that could accommodate the buffering requirements.



Furthermore, staff recommends that the applicant provide a landscaped buffer along the new south property line east of Monterey Drive to better address the future cemetery expansion.

### HISTORIC MIXED-USE BUILDING TYPE

As noted, all buildings, except for the parking structure are proposed to utilize the Historic Mixed-Use Building Type, which is permitted in the BSD-HTN District. Below is an outline of the applicable standards for this building type and how the proposal corresponds to each standard. This is followed by a discussion of the findings and associated recommendations.



#### *Building Height*

The minimum permitted height for this building type is 1.5 stories, and the maximum is 2.5 stories. A half story is defined as being completely within the roof structure (of a pitched roof) with street-facing windows or in a visible basement exposed a maximum of one-half story above grade. It is intended to be occupiable. Buildings A, B, C, G, and H are 2 stories in height, thus meeting the requirements (except for the one-story western bay of Building C). Buildings D, E-1, E-2, F, and P (surrounding the town square) are one-story buildings, several of which have very high ceilings and/or false upper floors. These require consideration of a Waiver.

#### *Floor Height*

The minimum and maximum permitted ground story heights are 10 feet and 12 feet, respectively. The table below reflects the proposed story heights for each building.

<b>Building</b>	<b>A</b>	<b>B</b>	<b>C</b>	<b>D</b>	<b>E-1</b>	<b>E-2</b>	<b>F</b>	<b>G</b>	<b>H</b>	<b>P</b>
<b>Ground Story</b> 10' min - 12' max	16'*	16'*	16'	26**	16* 24**	18'* 29.3**	14'* 26**	16'	16'	14'*
<b>Upper Story</b> 9' min - 12' max	10'	12'	12'	None	8**	None	None	15'	12'	None

\* Waiver required

\*\* High ceiling and/or false upper floor – numbers vary depending on specific location within building

As can be seen, ground story height Waivers are required for all buildings, and upper story height waivers are required for Building G and possibly Building E-1 depending on how the false second floor is interpreted.

#### *Roof Type*

A combination of pitched and parapet roofs is proposed, some within the same building. The only permitted roof type for the Historic Mixed-Use Building Type is Pitched Roofs. Other roof types, including the Parapet Roof type may be approved by the Planning and Zoning Commission if determined to be architecturally appropriate. Similarly, gambrel and mansard roofs (which are proposed on some buildings) are only permitted for single family detached buildings unless otherwise determined by PZC to be architecturally appropriate. Dormers or cross gables must be incorporated when the ridge of a gambrel or mansard roof runs parallel to the street.

*Primary and Secondary Materials/Roof Materials*

The permitted primary exterior cladding materials are brick, stone and wood siding. The applicant is proposing to use brick, limestone, stone veneer, fiber cement siding/panels/trim, terracotta tiles, and stucco. No clear delineation has been made between primary and secondary materials at this time.

The permitted roof materials include dimensional asphalt composite shingles, wood shingles and shakes, metal tiles or standing seam, slate, and ceramic tiles. The proposed roof materials include asphalt shingles, standing seam metal, copper, wood shingle, and clay tile.

The use of alternative materials is at the discretion of PZC.

*Façade Transparency*

The minimum street façade transparency is 40% on the ground story (storefronts) and 20% on upper stories (windows). For non-street façades, the minimum transparency is 15%. Blank wall limitations also apply. These requirements appear to be met in various but not all locations and will require further evaluation at FDP. Significant expanses of blank walls are proposed on the west elevations of Buildings E-1 and E-1. The applicant is proposing to use vines on the exterior of the ground story blank walls to soften their appearance and has provided a character image reflecting the intent. Additional blank walls are proposed on the south elevations of Buildings E-2, and F which face the new neighborhood street.

*Mid-Building Pedestrianways*

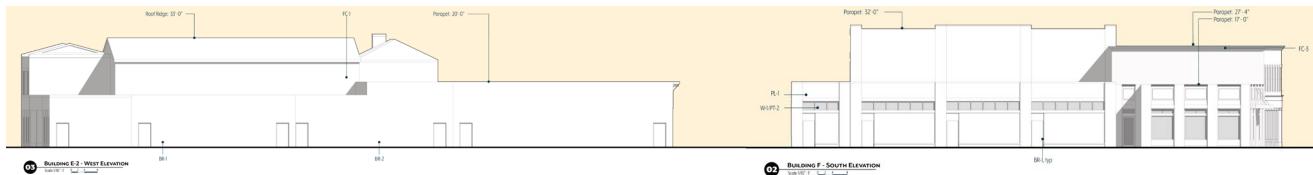
For Historic Mixed-Use buildings greater than 150 feet in length, mid building pedestrianways are required. Building A is 161 feet, building C is 200 feet, building E-2 is 191 feet, and Building G is 191 feet in length. All other buildings are less than 150 feet long. The code standard is based on the development pattern in the Historic District. In this case, an Administrative Departure for Building A and a Waiver for Building G seem to be warranted, but pedestrianways should be considered for Buildings C and E-2.

*Other*

There are several other standards that will need to be further evaluated at FDP and might require consideration of Waivers, such as entrance design, articulation of stories on street facades, windows, shutters, awnings and canopies.

*Discussion and Recommendations*

The proposed architectural theme is complementary to the nearby Historic District and promotes walkability and small public gatherings. While parapet roofs are not common in the Historic District, their use in this case appears to complement the architectural theme when incorporated at the appropriate locations. As noted above, various Waivers are requested, especially ones pertaining to building and story heights. In certain cases, the massing and rooflines of the buildings appear too complicated relative to the interior layout. Some of these could be mitigated as the design evolves, especially if the priority is placed on the human scale/pedestrian experience. However, concerns remain about the buildings surrounding the town square. While the proposed height helps create enclosure, it results in some exaggerated facades, faux second story space, and additional blank walls along the south and west sides of the buildings.



For those reasons, staff recommends the following:

- That the applicant consider adding a true upper story above at least some of the buildings around the town square. Ideally, a small residential component would be added to the mix of uses to help further activate the space and add a level of safety at night. Parking needs would be offset by the difference in peak hour use. Alternatively, additional office space, or even "vanilla box" space for future use could be incorporated.
- That the buildings' massing, roof forms, and facades be simplified to further complement the interior layout, while addressing the applicable standards.
- That the architectural details be further refined to reflect traditional design patterns.
- That fenestration or further articulation be added to the blank walls.
- That the applicant continue to coordinate with staff on addressing these conditions and finalizing all other architectural details and materials at FDP.

It should be noted that there might be an opportunity to introduce the Loft Building Type into the mix, especially on the east side of Monterey Drive. This requires a two-story minimum height but allows parapet roofs and permits greater story heights that are more consistent with the ones proposed by the applicant. Buildings G, H, and the parking structure could potentially be classified as one Loft Building.

### PARKING STRUCTURE BUILDING TYPE

The parking structure is proposed to include brick facades with regular vertical and horizontal articulation. It appears to meet the applicable building standards except that, at 305 feet in length, it is slightly over the 300-foot limit. The specific design details will be finalized at FDP, but staff recommends that additional design elements that provide a sensitive backdrop to the cemetery be added to the east and south sides. At this time, the applicant is proposing to add vines to the east façade.

### Signage

The applicant will submit a Master Sign Plan application at a later date.

### LIGHTING

The application materials include a preliminary lighting plan. The details will be further discussed at FDP. Light fixture selection within the public right-of-way will need to be coordinated with the City.

## 5. Plan Review

Preliminary Development Plan	
Criteria	Review
1. The PDP shall be consistent with the approved CP, the record established by the required reviewing body, the associated staff report, and the Director's recommendation.	<b>Criterion Met:</b> The proposal is consistent with the Concept Plan and addresses several of the items raised during that review.
2. The development is consistent with the Community Plan, BSD Special Area Plan, BSD Design Guidelines, other adopted city plans, and related policies.	<b>Criterion Met:</b> The proposed development is consistent with all applicable plans.
3. The proposed land uses align with all applicable requirements and use specific standards of 153.059 Uses.	<b>Criterion Met:</b> The proposed uses align with the applicable requirements and specific use standards.
4. The proposed buildings are appropriately sited and scaled to create a cohesive development character that complements the surrounding environment, and conforms to the requirements of 153.062 Building Types and 153.065 Site Development Standards.	<b>Criterion Met with Waivers and Conditions:</b> The proposed buildings are appropriately sited to create a walkable district with a central public square. Some of the standards are not completely met and will require some modifications and/or Waivers that will be finalized at FDP.
5. The proposed lots and blocks conform to the requirements of 153.060 Lots and Blocks.	<b>Criterion Met with Waivers:</b> Given the site's existing constraints, including the location of the Dublin Cemetery to the east, the block length and perimeter requirements cannot be met and only a partial block is feasible on the east side of Monterey Drive. This is mitigated by the proposed walkable organization of the site.
6. The proposed street types conform to the requirements and standards of 153.061 Street Types, including the general pattern of streets, blocks, and development reflected on the BSD Street Network Map and the conceptual locations of access points to surrounding streets to avoid adverse impacts on surrounding	<b>Criterion Met with Conditions:</b> The proposal is generally consistent with the applicable standards and the BSD Street Network Map. The traffic study recommending the addition of a traffic signal at the intersection of W. Bridge Street and Monterey Drive is being evaluated by the City. The streetscape details for all streets will need to be finalized in coordination with the City.

neighborhoods and traffic infrastructure.

7. The proposed design of the internal circulation system, driveways, and any connections to the public realm provide for safe and efficient access for pedestrians, bicyclists, vehicles, and emergency services.
8. The proposed design of buildings conforms to the BSD Code and is consistent with the BSD Design Guidelines, while integrating with nearby development.
9. The proposed open spaces are appropriately sited and designed to conserve or enhance natural features as appropriate, enhance the community both within and outside the proposed development, and conform to the requirements of 153.064 Open Spaces.
10. The scale and design of the proposed development allows for the adequate provision of services currently furnished by or that may be required by the city or other public agency including, but not limited to, fire and police protection, public water and sanitary sewage services, recreational activities, traffic control, waste management, and administrative services.
11. The proposed development conforms to the requirements of 153.063 Neighborhood Standards, as applicable.

**Criterion Met with Conditions:** The internal circulation and driveways are appropriate as is the connectivity to the public realm for all modes of transportation. Some of the details will need to be finalized at FDP with input from the Washington Township Fire Department.

**Criterion Met with Waivers and Conditions:** The overall design of the buildings is consistent with the intent of the BSD-Historic Transition Neighborhood, but various Waivers are requested and several details will need to be refined/finalized at FDP.

**Criterion Met with Conditions:** The proposed town square has the potential of providing a meaningful public space in the community, and the proposed pedestrian pathways would provide connectivity to Monterey Park and the adjacent cemetery. Additional buffering and tree preservation should be incorporated into the plan.

**Criterion Met with Conditions:** The proposed development allows for the provision of services to the site and adjacent sites. The applicant should continue to work with the City and the Washington Township Fire Department to finalize the related details.

**Criterion Met with Waivers and Conditions:** The development meets the intent of the Historic Transition Neighborhood District standards. Some elements of the proposal require refinements and/or Waivers, and the applicant should continue to coordinate with the City as the details are finalized.

12. The proposed development provides adequate stormwater management systems and facilities that comply with the applicable regulations of this code and any other applicable design criteria or regulations as adopted by the city or required by other government entities.

**Criterion Met with Conditions:** The applicant should continue to work with the City's Engineering staff on the design and configuration of the stormwater detention needed for the site for both the public and private improvements to properly account for known capacity concerns in the existing storm sewer system.

13. The proposed development can be adequately serviced by existing and/or planned public or private infrastructure consistent with the city's most recently adopted capital improvements program.

**Criterion Met:** The development can be adequately serviced.

14. If the development is to be implemented in phases, each phase has adequate infrastructure to serve the development independently without the need for further phased improvements.

**Not Applicable:** The development consists of a single phase.

15. The proposed development demonstrates consistency with the recommendations, principles, and intent of all applicable design standards and guidelines, including but not limited to buildings, open spaces, and streetscapes.

**Criterion Met with Conditions:** The proposed development is consistent with the Historic Transition Neighborhood standards and the BSD Special Area Plan. Various details will need to be refined/finalized at FDP.

## Recommendation

**Planning Recommendation:** Approval of Preliminary Development Plan with the following conditions:

- 1) The applicant shall continue to coordinate with City staff on finalizing the streetscape details for all streets, including the W. Bridge Street/Monterey Drive intersection design and number of left-turn lanes, and the treatment along the south side of the new Neighborhood Street.
- 2) The drive aisle width shall be reduced to 22 feet unless emergency vehicle or service access is impeded.
- 3) The width of the buffer along the west side of the site shall be increased to allow screening of the adjacent single family residential property and preservation of trees on both sides of the property line.
- 4) A buffer shall be provided along the south side of the site east of Monterey Drive to meet the required setback and provide screening for the cemetery expansion.
- 5) The applicant shall continue to work with the City Engineering staff on the design and configuration of the stormwater detention needed for the site for both public and private

improvements to properly account for known capacity concerns in the existing storm sewer system.

- 6) The applicant shall explore all available alternatives to retain as many healthy/desirable trees as possible.
- 7) The applicant shall consider adding a true upper story above at least some of the buildings around the town square.
- 8) The buildings' massing, roof forms, facades, and design details shall be refined to further complement the interior layout and reflect traditional design patterns.
- 9) Fenestration or further articulation shall be added to all blank walls.
- 10) Additional design elements that provide a sensitive backdrop to the cemetery shall be added to the east and south facades of the parking structure.