



City of Dublin

**Office of the City Manager**

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# Memo

**To:** Members of Dublin City Council  
**From:** Dana L. McDaniel, City Manager  
**Date:** October 15, 2019  
**Initiated** Jennifer M. Rauch, AICP, Interim Planning Director  
**By:** Claudia D. Husak, AICP, Senior Planner  
**Re:** Ord. 53-19  
Rezoning approximately 33.9 acres south of U.S. 33, east of Eiterman Road and north of Shier Rings Road from ID-1, Research/Office District to PUD, Planned Unit Development District (The Ohio State University Wexner Medical Center) for the construction of up to 550,000 square feet of ambulatory medical care, medical and office uses and a future hospital in two phases (Case 19-055Z/PDP)

## Summary

This Ordinance is a request for review and approval of a rezoning of 33.9 acres south of U.S. 33, east of Eiterman Road and north of Shier Rings Road from ID-1, Research/Office District to PUD, Planned Unit Development District (The Ohio State University Wexner Medical Center) for the construction of up to 550,000 square feet of ambulatory medical care, medical and office uses and a future hospital in two phases (Case 19-055Z/PDP)

## Background

The approximately 34-acre site includes portions of three parcels, all of which are owned by the City of Dublin. The largest parcel is approximately 60 acres and was annexed to the City in 2004 (Ord. 47-04) and the City of Dublin purchased the parcel for economic development purposes in 2016. The other two parcels are approximately 25 acres combined and were annexed in 1996 (Ord. 64-96) and purchased by the City in 2018. All three parcels were rezoned in 2011 from R, Rural District and R-1, Restricted Suburban Residential District to ID-1, Research/Office District as part of an on-going effort to aid in the implementation of the long-term vision of this area as an important Economic Advancement Zone and Innovation District. No development has been proposed on either parcel to date.

In 2019, City Council approved an Economic Development Agreement (Ord. 08-19) with The Ohio State University, which authorized the conveyance of this site to the University to establish a comprehensive ambulatory medical facility, consistent with the City's Economic Development Strategy to encourage development and create and preserve employment opportunities within the City.

The Planning and Zoning Commission reviewed this application on September 19, 2019 and made a recommendation of approval to City Council. On August 22, 2019, the Planning and Zoning Commission informally reviewed and commented on this proposal. The Commission complimented and welcomed the applicant for choosing this location for their proposed facility as well as the quality and layout of the proposal. Commissioners suggested the applicant provide additional paths throughout the parking lots to break up the large parking areas and provide additional opportunities to navigate to the main building entrances. Seating along walkways was encouraged closer to the drop off at main entry areas. The Commission also requested that the applicant provide more opportunities to walk or bike through the natural areas along the site boundaries.

## **Description**

### *Site*

The site is comprised of three parcels of unimproved, vacant, agricultural land. The property is rectangular and generally flat. The southeastern portion of the subject property was formerly developed with farmstead structures, but is currently vacant.

A farm drainage ditch "Cosgray Ditch" runs east/west through the center of the western parcel, then turns south between the two southern parcels. The ditch is in a City Stream Corridor Protection Zone, which the City has studied for relocation during Fall 2016 and the US Army Corps of Engineers has issued the public notice for this application on August 16, 2019.

### *Surrounding Sites*

The Avery Road Industrial Park, zoned ID-1, Research/Office District. The City of Dublin Service Center, Fleet Maintenance and the Dublin City Schools Transportation Department are located to the south across Shier Rings Road. To the east is a church in unincorporated Washington Township.

## **Proposal**

This is a proposal for a medical campus to be developed in two phases. The applicant has stated that the initial phase of the project is anticipated to include an ambulatory care facility, medical office space, and other related uses. The term ambulatory care facility will provide outpatient medical procedures, testing, and treatment in addition to traditional physician consultations. Phase I will include approximately 250,000 square feet and Phase II may expand the first phase by up to an additional 300,000 square feet to include additional medical and related facilities.

## **Community Plan**

<http://communityplan.dublinohiousa.gov/>

The Community Plan is the key policy guide for decision-making about the built and natural environments of the City of Dublin. The Community Plan text and associated maps contain detailed recommendations for future development including the appropriate location and density or intensity of residential and commercial uses; the general location and character of roads; the general location of parks, open space and public buildings among other information.

The Community Plan is a guide for City Council and the Planning and Zoning Commission as they assess the location, character, and extent of proposed public and private development in Dublin. The policies and recommendations within the Plan will be implemented over time through rezonings and subdivisions of land and the location and construction of public improvements by either the City or a developer.

Recommendations throughout the Plan are based upon a review of existing conditions and evaluation of future development scenarios for their impacts on infrastructure, roads and the fiscal health of the City. Dublin's ability to maintain high quality of services and quality of life depends on a careful review of development proposals for conformance with the Community Plan.

#### *Future Land Use*

The West Innovation District (WID) is designated as a Mixed Use Regional Center on the Future Land Use Plan. While an important district of the City, it serves a dual role as a regional economic node. The characteristics of the Mixed Use Regional Center for the WID include a mixed-use environment of integrated academic, research, office and advanced manufacturing uses that are supported by residential, retail, personal services, entertainment and open spaces as amenities.

The Community Plan shows the future land use for this site as *MUR-Research and Development*, which is an area reserved for mature innovation companies that desire the autonomy of their own site and require ample space for expansion. This sub-district offers a limited number of larger sites with ample parking, as well as visibility along U.S. 33.

#### *West Innovation District Special Area Plan*

<http://communityplan.dublinohiousa.gov/special-area-plans/west-innovation-district/>

Special Area Plans provide a more in-depth depiction of how the Land Use and Transportation Plans may be implemented in key areas of the city that are expected to undergo significant change. These elements of the Special Area Plan serve to guide decision-making regarding the appropriateness of development proposals and the infrastructure improvements necessary to support future development. Special Area Plans are conceptual and intended as general guides to indicate potential development options. The concepts are schematic only, and the actual mix of land uses, locations and configurations of buildings, parking areas, streets and access points will be determined through the public review process for individual development proposals.

In 2017, City Council approved Ordinance 69-17 for an amendment to the West Innovation District (WID) Special Area Plan as part of the City of Dublin Community Plan. The WID encompasses 1,100 acres east of Houchard Road, west of Avery Road, north of Shier-Rings Road and south of US 33/State Route 161/Post Road. Following adoption of this update to the WID Special Area plan, it is the intent of staff to initiate an amendment to the existing WID zoning to ensure consistency between the adopted plan and the zoning district.

The District is comprised of primarily undeveloped land and provides vast opportunities for walkable, mixed use, research and innovation development sites that can meet a host of business needs.

The District is designated as a *Mixed Use Regional Center* in the Plan. This designation reflects the functional role this district plays from a land use perspective. While an important district of the City, it serves a dual role as a regional economic node. Characteristics of this designation include a mixed-use environment with integrated land uses, densities that support a walkable built environment, sites that are development for a variety of corporate needs that range from the traditional suburban office uses to more walkable urban centers with higher intensity of land uses.

Appropriate land uses include office, research, medical with parking and laboratories listed as secondary uses. The Area Plan recommends a maximum density of 30,000 square feet per acre and a maximum story height of three stories. At a maximum build-out of 550,000 square feet on 33.9 acres, the proposed density is approximately 16,000 square feet per acre, which meets the Community Plan.

The Area Plan also addresses architectural character and materiality with an emphasis on modern architecture. Architectural design should not be overly restrictive in the District relative to the composition of the architectural character of buildings that would limit creativity. Glass, metal, stone, brick, concrete and wood.

### **Thoroughfare Plan**

The Thoroughfare Plan map shows the needed width of right-of-way and identifies the number of lanes needed to accommodate year 2035 development in Dublin.

The Thoroughfare Plan recommends 100 feet of right-of-way for existing Shier-Rings Road and 70 feet for Eiterman Road. The Thoroughfare Plan includes planned roadway connections throughout the WID and currently depicts Shier-Rings Road moving north toward the Ohio University campus west of Eiterman Road. The Special Area Plan also shows this layout with a street connection from existing Shier-Rings Road east of Eiterman Road through the subject site north to intersect with Eiterman to provide access to the sites between these two roadways, south of US 33.

### **Neighborhood Contact**

The HOA leadership of the Ballantrae Community as well as a few interested residents invited City staff to their recent meeting, which took place on September 5, 2019. During the meeting, staff had a chance to provide updates on the infrastructure network changes due to the roadway proposed to serve the proposed OSU facility and other adjacent sites. Based on the feedback from the earlier public meeting, staff is now moving forward with the existing Shier-Rings Road to remain in its existing location and as a way for Ballantrae residents and others to access Avery Road from the west. Staff will continue to work on finalizing the needed improvements including access arrangements for the public and private businesses along this corridor.

The applicant and the City of Dublin held a public meeting on August 8, 2019 to introduce the proposal and necessary transportation network improvements. Attendance at this meeting was

approximately 100. Residents of Ballantrae voiced concerns focusing on the potential impact of roadway network changes on their neighborhood, particularly due to a current study to terminate existing Shier-Rings Road in a cul-de-sac, east of the neighborhood. Concerns were raised that this option would cause non-neighborhood traffic to use streets in Ballantrae to travel south or east. Other concerns centered on the potential of this proposal to increase crime in the area and decrease property values. Some residents appreciated the efforts made to relocate the through movement and access to the OSU site north from existing Shier-Rings and the ability to create buffering between the medical center and the neighborhood. Some residents also requested the City study the northward extension of Baronscourt Way to intersect with relocated Shier-Rings Road.

### **Proposed Layout**

The proposal includes two site plans, one for Phase I and one for Phase II. The plans show the relocated of Shier-Rings Road as the southern site boundary. The Phase I plan shows a medical office building and the Ambulatory Care Facility. The building is proposed in the center of the site along the US 33 frontage with the main entrance located along the south façade. Parking in three distinct lots is located along the western portion of the site and in the southern portion of the site north of relocated Shier-Rings Road/University Boulevard in the Phase I plan. Drive aisles for circulation to the main entrance are indicated with walking paths connecting the parking to the front of the building. Along the southern façade, the plans show ample open space and landscape area to provide outdoor space for visitors, patients and staff. Several paths are also indicated along the northern portion of the site buffered from US 33 with trees and landscaping. In the Phase I plan, there is lawn space provided where the building and additional parking for Phase II will be located.

Both Phase I and Phase II plans show stormwater retention ponds for the site in the northwest, northeast and southeast. Access is shown off relocated Shier-Rings Road/University Boulevard with a roundabout as the main access point and secondary access in the west and east.

Phase II indicates an expansion of the parking lot east of the main entrance as well as an additional parking lot to the east between the two retention ponds. A hospital building is indicated to be attached to the Ambulatory Care Facility. Phase II continues a landscape theme along the front (south) façade to provide separation between drive aisles and to create high quality greenspaces to between the parking and the building. The access off relocated Shier-Rings Road does not change with Phase II, however there is a cross access drive aisle show for the site to the east.

### **Proposed Development Text**

#### *Zoning*

While the site is shown in the Community Plan Special WID Area Plan as Research and Development, the Zoning Code has not yet been updated to reflect this District and the site retains the current zoning of ID-1, Research/Office District. All the proposed uses included in this proposal are permitted within the ID-1 District. Due to the unique nature and needs of a medical facility of this size and associated site improvements, the applicant and staff have agreed that a rezoning from ID-1 to a Planned Unit Development District (PUD) is the appropriate mechanism to ensure that the needs of the facility can be met. In addition, the City

retains a higher level of involvement in ensuring the development will be sensitive to the surrounding area, provides for a high quality and cohesive development with appropriate development pattern, landscaping, and signs.

#### *Access*

Three driveways into this site from a new public roadway (University Boulevard) will provide vehicular access. A roundabout is planned at the main entrance and the other two driveways will be at traditional intersections. A Traffic Impact Study (TIS) has been performed by the applicant and submitted to staff for review. This study reviews the intersection control at the proposed driveways as well as several offsite intersections. Staff is working on the review of the study in coordination with the proposed public improvements to be constructed by the City. The applicant has agreed to participate in the funding of the public roadway improvements that will be built by the City. The site plan includes cross access to the parcels/developments that will be east and west of this site. This will aid in distributing the traffic onto the new roadway.

#### *Utilities/Stormwater*

This site will have access to public water for domestic and fire protection service by connecting public water main that will be constructed with the University Boulevard project. Two separate water meter vaults will be placed at the western most and eastern most site drives. This site will have access to public sanitary sewer service by connecting services to public sanitary mains that will be constructed with the University Boulevard project. This site is located within the Cosgray Sanitary Sewer shed.

In terms of stormwater management, this site has proposed the construction of three separate wet retention basins. Two of the basins are proposed along the US 33 frontage, the third is proposed to the east of the eastern most drive. These basins are designed to meet the requirements of the City of Dublin's Chapter 53 Stormwater Management Code, and the requirement of the Ohio EPA. A preliminary stormwater management report has been submitted to demonstrate compliance with these rules and regulations.

#### **Development Text**

The development text is the regulating document that outlines the development standards for the development including uses, lot requirements, and architecture and materials. The applicant has provided a development text that largely lays out similar to the existing zoning regulations for the West Innovation District as part of the Zoning Code.

#### *Uses*

The proposed text excluded a number of the uses currently permitted or conditionally permitted within the ID-1 District. It is the intent to develop this site with the ambulatory care facility/medical offices/hospital uses and, in the future, allow support services on adjacent parcels. Those prohibited uses include eating and drinking, personal services, animal services, daycare, industrial uses and residential.

Permitted uses are medical office, hospital, educational as related to post-secondary education, community education or medicine. Daycare, retail, parking structures, exercise and fitness are permitted as an accessory uses as is eating and drinking. Lastly, as currently permitted in the

ID-1 District, the proposed text allows a heliport as a permitted accessory use as long as the use complies with Zoning Code Section 153.094, which regulates heliports.

#### *Development Standards*

The proposed text requires a minimum lot size of three acres and limits lot coverage to a maximum of 75%, which exceeds the Code by 5%. The WID Special Area Plan suggests a maximum height of three stories in this portion of the District, while the Zoning Code does not limit the height of buildings in ID-1, but requires larger setbacks for taller buildings (75-foot side and rear setbacks for buildings taller than 51 feet). The proposed development text permits a height of up to 110 feet and seven stories. When questioned by residents during the Neighborhood Meeting as to the need for the height of the buildings, the applicant responded that interior space planning and efficient interior circulation mandate the height, while also allowing for a smaller building footprint. The building has been sited as far north on the site as possible to provide separation and space for buffer to the south.

Setback requirements are 100 feet from U.S. 33 for parking and pavement, with exceptions for walking paths, ground-mounted mechanical equipment that is completely screened and grass pavers required for vehicular access to any equipment. Along proposed University Boulevard pavement setbacks are required to be 25 feet and building setbacks 100 feet. Side yard setbacks are 15 feet for pavement and 50 feet for buildings. The applicant should clarify the development text that paths, outdoor amenity spaces or decorative walls are exempt from the setback requirements.

#### *Architecture*

The development text explains the unique architectural needs and building design for this type of facility as well as a facility operating under the OSU brand. As stated during the Concept Plan review, a significant amount of interior design is still to take place prior to finalizing all elevations, which will be required to be reviewed and approved by the Planning and Zoning Commission with the Final Development Plan. The applicant has provided the conceptual elevation previously shared with the Commission.

The text states that the goal of the architecture for the facility is to be high-quality with an attractive aesthetic design to meet and exceed the City's standards for quality of materials and design as a focal point in the community.

Requirements include a similar treatment of materials on all elevations with uninterrupted, blank facades prohibited if visible from the right-of-way or adjacent property. Permitted materials include brick, brick veneer, stone, stone veneer, manufactured stone, cast stone, architectural pre-cast concrete, metal, wood (including synthetic) and glass. Reflective or mirrored glass is prohibited. Other prohibited materials include prefabricated metal and untreated masonry block.

#### *Landscaping*

The text refers to the illustrative drawings and the preliminary landscape plan to illustrate the intended landscape treatment. The text requirements align with Code requirements. The Preliminary Development Plan includes details addressing the open space locations and

programming for the site. Entry walls are shown to be of a modern limestone design to reflect the heritage of the City while also indicating the desire for modern design and innovation in the District. A courtyard for a café and a respite garden are proposed between in the center of the building on the south side, while an entry court as well as an entry green are designed to enhance the arrival experience to the facility whether walking from the parking lots or using the drop-off area. Landscape buffers are shown along all side of the site. The text requires adherence to the Code for the preservation and replacements requirements.

### *Parking*

The text requires 2.25 parking spaces per 1,000 square feet of building area for this PUD, while the Zoning Code breaks out parking requirements by each individual use. Given the unique nature of this facility and the potential for various accessory uses as well as experience the applicant has with this type of facility, Staff supports this parking requirement. Additionally, it will significantly simplify the future administration of parking should uses or the size of uses change. Bicycle parking is required at one space per every 10,000 square feet of building.

### *Lighting*

The proposed text requires adherence to the Zoning Code for lighting regulations, all of which will in detail reviewed as part of the Final Development Plan. The Code addresses height and lumen levels of fixtures and permitted levels of lighting at the property line. For parking lots exceeding 150 spaced, reduced lighting levels after 10 p.m. are required.

### *Signs*

The applicant stated during the Concept Plan review as well as shared with staff, that the sign requirement for this type of facility are unique and flexibility and user experience will play an important role to the signage approach. The text requires approval of a sign plan as part of the Final Development Plan to provide additional details. The text includes seven sign types: Building Identification Signs, Ancillary Wall Signs, Window Signs, Primary Entry Monument Signs, Secondary Entry Monument Signs, Directional Signs and Regulated Signs.

- Building Identification Signs: four such signs are permitted per the development text. Per the plan, this sign type is approximately 300 square feet, which meets the maximum sign size for buildings with Interstate frontage. The text states that the final locations and dimensions of these signs (with such dimensions not to exceed those shown in the accompanying sign plan) shall be provided by the applicant with the final development plan, with the provision that one such sign shall be located on the west-facing elevation of the medical office building, another shall be located on the east-facing elevation of the same building, and the third and fourth shall be located on the south-facing and north-facing elevations of the ambulatory care facility. Each sign is permitted to be located on the uppermost story, which is the mechanical room of each building. Allowances for sign relocation are included when the building in Phase II is constructed.
- Ancillary Wall Signs: the text does not include a number permitted for additional wall signs but states that their purpose is to identify particular uses within the building, to provide addresses for such uses, and to promote efficient wayfinding into the building(s). The applicant defers the details of such signs to the Final Development Plan.



- Window Signs: window signs are permitted based on the ID-1 District allowances, which limits them to one per building or use when the use has an individual entrance.
- Primary Entry Monument Sign: The text permits one such sign either east or west of the main entrance to the facility. The sign plan shows the height of this sign to be approximately 12 feet and the overall size is 66 square feet. At the Final Development Plan Stage, the applicant will have to verify that this sign does not impede visibility for this intersection.
- Secondary Entry Monument Signs: the text permits two of these types of signs at the westernmost and easternmost access drive into the site. The sign plan shows that these signs may be five feet tall and 3'6" wide for a total size of 17.5 square feet. The sign plan indicates four locations for these signs while the text limits them to two. The applicant needs to address this discrepancy.
- Directional Signs: As is the case for these large medical facilities, directional signs primarily designed for vehicular use are an essential wayfinding element. The proposed text does not limit the number of directional signs and the plan provides for a height and size that exceeds Code. The height is shown at 7'10" with a size of 17.5 square feet. All other details are deferred until the next review by the Commission. After approval of the sign plan as part of the final development plan, changes may be approved administratively.
- Regulated Signage: The text includes this sign type due to mandated specifications pursuant to federal and/or state laws and other regulations and allows such signs shall be permitted in accordance with laws and regulations while not requiring review and approval as part of a Final Development Plan.

### **Recommendation of the Planning and Zoning Commission**

The Planning and Zoning Commission recommended approval of the Rezoning/Preliminary Development Plan to City Council on September 19, 2019 with the conditions listed below. The applicant has addressed Conditions 1 through 6 and all other conditions will be addressed at the final development plan stage.

- 1) That the development text and the sign plan be updated to address the discrepancies prior to Council review, including the correct nomenclature and permitted sizes, permitted lighting and permitted number of secondary monument signs;
- 2) That the applicant update the development text to clarify that peninsulas may count in the interior landscape requirements;
- 3) That the smaller trees shown on the plans in the interior landscape areas be permitted on the development text;
- 4) That the development text be revised to include bio-retention measures, as appropriate, in the parking lot areas to aid in stormwater management and provide for irrigation of trees; this should include the elimination of the requirement of curbed islands;
- 5) That the applicant continue to work with Engineering to demonstrate compliance with stormwater requirements as defined in Chapter 53 to the satisfaction of the City Engineer;
- 6) That the building elements and mechanical equipment areas be screened per Code in the text;
- 7) That the applicant work with staff to identify areas where new trees are appropriate to reduce the need for future removal during Phase 2 and update the text accordingly; and

- 8) That the applicant coordinate with ODOT for necessary approvals to remove any dead, invasive or hazardous vegetation along the frontage.

**Recommendation**

Staff recommends City Council approval of this Ordinance at the second reading/public hearing on November 4, 2019.

# RECORD OF ORDINANCES

**53-19**

Ordinance No. \_\_\_\_\_

Passed \_\_\_\_\_

**REZONING APPROXIMATELY 33.9 ACRES SOUTH OF U.S. 33, EAST OF EITERMAN ROAD AND NORTH OF SHIER RINGS ROAD FROM ID-1, RESEARCH/OFFICE DISTRICT TO PUD, PLANNED UNIT DEVELOPMENT DISTRICT (THE OHIO STATE UNIVERSITY WEXNER MEDICAL CENTER) FOR THE CONSTRUCTION OF UP TO 550,000 SQUARE FEET OF AMBULATORY MEDICAL CARE, MEDICAL AND OFFICE USES AND A FUTURE HOSPITAL IN TWO PHASES (CASE 19-055Z/PDP)**

**NOW, THEREFORE, BE IT ORDAINED** by the Council of the City of Dublin, \_\_\_\_\_ of its elected members concurring, that:

Section 1. The following described real estate, (see attached legal description, Exhibit A), situated in the City of Dublin, State of Ohio, is hereby rezoned PUD, Planned Unit Development District, and shall be subject to regulations and procedures contained in Ordinance No. 21-70 (Chapter 153 of the Codified Ordinances), the City of Dublin Zoning Code and amendments thereto.

Section 2. The application, including the list of contiguous and affected property owners, and the recommendations of the Planning and Zoning Commission, are all incorporated into and made an official part of this Ordinance and said real estate shall be developed and used in accordance there within.

Section 3. This Ordinance shall take effect and be in force from and after the earliest period allowed by law.

Passed this \_\_\_\_\_ day of \_\_\_\_\_, 2019.

\_\_\_\_\_  
Mayor – Presiding Officer

ATTEST:

\_\_\_\_\_  
Clerk of Council



## RECORD OF DISCUSSION

# Planning & Zoning Commission

Thursday, August 22, 2019 | 6:30 pm

The Planning and Zoning Commission took the following action at this meeting:

**5. OSU Medical Campus  
19-055CP**

**6600 Shier Rings Road  
Concept Plan**

**Proposal:** A Concept Plan that is the first step for the potential rezoning of a 34-acre site from ID-1, Research Office District to Planned Unit Development District for the potential development of a medical office building and an ambulatory care center (Phase I) and a potential future hospital (Phase II).

**Location:** North side of Shier Rings Road, east of Eiterman Road, west of Avery Road, and south of US 33.

**Request:** Review of a Concept Plan under the provisions of Zoning Code Section 153.053.

**Applicant:** OSU Wexner Medical Center represented by Aaron L. Underhill, Underhill & Hodge LLC

**Planning Contact:** Claudia D. Husak, AICP, Senior Planner

**Contact Information:** 614.410.4675, [chusak@dublin.oh.us](mailto:chusak@dublin.oh.us)

**Case Information:** [www.dublinohiousa.gov/pzc/19-055](http://www.dublinohiousa.gov/pzc/19-055)

**RESULT:** The Commission reviewed and provided non-binding comments on an application for a Concept Plan. The Commission complimented and welcomed the applicant for choosing this location for their proposed facility as well as the quality and layout of the proposal. Commissioners suggested the applicant provide additional paths throughout the parking lots to break up the large parking areas and provide additional opportunities to navigate to the main building entrances. Seating along walkways was encouraged closer to the drop off at main entry areas. The Commission also requested that the applicant provide more opportunities to walk or bike through the natural areas along the site boundaries. Another comment requested more detail for the architecture of the buildings to truly create signature building within the West Innovation District. The Commission also requested additional information regarding the planned roadwork network surrounding the site.

A representative of the Ballantrae Board of Directors provided comments regarding the neighborhood's appreciation of OSU as a neighbor to their community and requested that the connectivity of the neighborhood to the improved roadway network be considered. Additional public comment centered on the connectivity of businesses along Eiterman Road to and from the north and the potential impact of an intersection with the road serving OSU and Eiterman Road and the relationship between the anticipated traffic and the facility.



5. OSU Medical Campus  
19-055CP

6600 Shier Rings Road  
Concept Plan

**MEMBERS PRESENT:**

Victoria Newell	Yes
Jane Fox	Yes
Warren Fishman	Yes
Kristina Kennedy	Yes
William Wilson	Yes
Mark Supelak	Yes
Rebecca Call	Yes

**STAFF CERTIFICATION**

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Claudia D. Husak, AICP  
Senior Planner/ Manager of Current Planning



has made her aware that most of the areas with commercial development within residential neighborhoods have not done well in the long run and have become problematic for the neighborhoods.

Mr. Cline thanked the Commission for their input.

Ms. Newell thanked the residents for their interest and involvement.

[A five-minute recess was taken.]

### **3. OSU Medical Campus, 19-055CP , Concept Plan**

Ms. Newell stated that this application is a request for review of a Concept Plan, which is the first step for the potential rezoning of a 34-acre site from ID-1, Research Office District to Planned Unit Development District for the potential development of a medical office building and an ambulatory care center (Phase I) and a potential future hospital (Phase II). The site is on the north side of Shier Rings Road, east of Eiterman Road, west of Avery Road, and south of US 33.

#### **Staff Presentation**

##### Background

Ms. Husak stated this a Concept Plan review for The Ohio State University (OSU) Wexner Medical Campus to be located in Dublin. The Concept Plan will provide informal feedback from the Commission. The applicant has filed an application for a rezoning to a PUD. The 34-acre site, currently owned by the City of Dublin, was acquired for economic development purposes. The site, located within the eastern portion of the West Innovation District and on the south side of US33, is currently comprised of three parcels. Eventually, there will be a preliminary development plat and final plat application, which will result in a parcel of the size and shape shown [image shown], and will provide access through public right-of-way. On the south side is Shier Rings Road, where the Dublin Service Center and the Dublin School Transportation site are located. The Ballantrae neighborhood is also located to the south. On the west is the Washington Township Administration Building, a church and the Sutphen Corporation. Cosgray Ditch runs through the site, and the City of Dublin has committed to working with the Army Corps of Engineers in relocating that ditch. Public notices of the project were sent out recently. The Community Plan shows the future land use for this site as RD-Research and Development. In 2017, City Council approved Ordinance 69-17 for an amendment to the West Innovation District (WID) Special Area Plan as part of the City of Dublin Community Plan. The updated plan is more inclusive in regard to land uses. It provides different amenities and opportunities for development, particularly driven by the OSU Heritage College, which is located in the northern portion of this district. To date, little development has occurred in the district. The site was zoned ID as part of the original Innovation Plan, adopted in 2011, so its current zoning is ID-1, which is a research and office zoning district. All of the uses that will be included in the PUD zoning are permitted currently in the ID-1 District. However, a PUD will provide more flexibility for the applicant and the City.

##### Proposed Site Plan

The plan for a medical campus will be developed in two phases. Phase I will include medical office space as well as an ambulatory care facility. Phase I will include approximately 250,000 square feet with parking provided in three distinct areas. The western portion of the site will remain vacant in Phase I, but will accommodate a future Phase II. The plan is to provide access to the

site along the southern boundary from existing Shier Rings Road extending west toward Eiterman Road. City Engineering is currently evaluating whether that will be a relocated Shier Rings Road and if it would have a different name. Engineering is currently reviewing the applicant's traffic impact study and evaluating the impacts of providing this access on the larger transportation network. This road change will be a City CIP project. The proposed OSU project, including the potential changes in the transportation network, was introduced to the neighborhood at a public meeting earlier in August. Significant public feedback was provided. Engineering is continuing its study of potential transportation network changes. Phase II provides for a hospital building to be attached on the west to the Phase I - Ambulatory Care Facility. Additional parking areas will be provided, as well. Internal vehicle circulation roadways and pedestrian paths will be provided. Stormwater management facilities will be located on the northern and eastern portions of the site. Cross access is contemplated for sites to develop east and west of the proposed facility. Preliminary elevations of the facility have been provided, primarily a material and massing study. Many more interior plans must be finalized. The main building will be primarily brick and glass with modern lines. Some stone, limestone and metal elements will exist on the bottom of the structure. The applicant has provided preliminary details addressing the open space locations and programming for the site. A courtyard for a café and a respite garden are proposed as well as an entry court and entry green on the south; entry walls will be of limestone; different types of treatments for the stormwater management areas will be included, as well as quiet spaces and walking paths throughout the open spaces. At the public meeting, it was noted that AEP has been working on a project to provide additional power lines through the district to Shier Rings Road, but that is not an OSU or City project. AEP is a public utility over which the City does not have jurisdiction. AEP will be hosting a public meeting in the near future. Staff has provided the following discussion questions:

- 1) Does the Commission support this proposal as a catalyst to begin establishing the vision adopted by the City as part of the WID Special Area Plan for a District that includes a mixed-use environment of integrated academic, research, office and advanced manufacturing uses, ultimately supported by residential, retail, personal services, entertainment and open spaces as amenities?
- 2) Does the Commission support the proposed layout of the facility and the site?
- 3) Is the architectural concept furthering the goal of the Community Plan for modern, innovative, sleek architecture?
- 4) Does the conceptual landscape and open space plan provide adequate variety and sufficient spaces for patients, visitors and staff?
- 5) Other considerations by the Commission.

### **Commission Questions**

Ms. Kennedy inquired if the Commission would be addressing Phase 2 at this time, as well.

Ms. Husak responded affirmatively. When the rezoning application comes before the Commission, it will have a zoning text that addresses both phases.

Ms. Call requested clarification of the location of the Cosgray Ditch, if Shier Rings Road will be realigned, or if there would be a new road, and if so, if it would have a different name.

Ms. Husak responded that, although it is currently identified as realigned Shier Rings Road, the new road does not yet have a new name. The tree line shown on the plan depicts Cosgray Ditch. Existing Shier Rings Road is on the southern portion of the site. Somewhere within this area,

there will be an intersection and a roadway running in front of the facility to the northwest; that is currently under study.

Mr. Supelak stated that this street layout is different from the master plan the Commission viewed previously. Is that because this is one of the first parcels to be substantially developed?

Ms. Husak responded it was always the intent that Shier Rings would eventually be realigned north and carry more traffic volume for the West Innovation District. Conceptually, there were also internal roadways that would connect the larger sites within the interior. This large campus facility emphasizes the need to have a roadway connection running through the district further to the east than was anticipated.

Ms. Fox stated that the Community Plan provides a holistic overview of how the district will be developed. It is important to have the Master Plan and Community Plan at hand when we review these opportunities. This plan provides some lovely uses of outdoor spaces and engaging placemaking areas, but those all appear to be on the back end of the property facing a highway, instead of on the side of the property that would become part of the greater development. Was there an engineering reason for this, such as stormwater management?

Ms. Husak responded that the City would retain possession of all the land surrounding this site. The intent is to provide a buffer between the Ballantrae community and the roadways and this use. Relocation of Cosgray Ditch will provide more green and natural areas farther south of this site. In front of the building are the more formal open spaces for the users of the facility. Trails are provided throughout the site for the patrons. The spaces south of the highway will provide setbacks and uses other than open space.

Ms. Newell stated that the packet material indicates discussions occurred regarding maintaining a straight zoning for the site or rezoning it to a PUD. Why was a PUD determined to be preferred?

Ms. Husak responded that the West Innovation District (WID) zoning has not been used for a project of this size. The only one it was tested on was the new Ohio University building. The other issue is that there is now an updated area plan for the WID that provides more flexibility in architecture and addresses uses differently. The applicant will also have a unique need for signage and parking. The existing zoning cannot accommodate those needs as well as a PUD could do.

Ms. Newell stated that a hospital and its uses are permitted in the WID, correct?

Ms. Husak responded affirmatively.

### **Applicant Presentation**

Aaron Underhill, attorney, Underhill & Hodge, 8000 Walton Parkway, New Albany, OH stated that he is representing The OSU Wexner Medical Center. With him tonight are also other representatives and consultants on behalf of the university. This is a very important and impressive project for Dublin. It will provide a great community amenity, providing services such as outpatient surgeries, physicians, testing areas and laboratories. The City has been creative in its economic development efforts. This site is the subject of a contract between the University and the City of Dublin and an associated economic development agreement. This facility is one of three similar OSU facilities currently underway. The first is on Hamilton Road and SR161 near New Albany, and another facility will be built in Powell near Home Road and Sawmill Parkway. The University looks for certain site specifications for these facilities, such as accessibility from freeways and major arterial roads and size of the site – 30-35 acres is preferred. The relocation



of Shier Rings Road is driven primarily by the size of the parcel and long-standing plans of the City. This site is an influence for the new public street, but it has also been part of a broader concept that the City has had to tie this into the broader road network and create other development opportunities in the WID. In regard to the economic development agreement (EDA), OSU will be purchasing the land from the City with no price markup; however, the City will be receiving its return on investment. Additionally, OSU will be receiving some incentives in terms of income tax rebates from employees. While OSU will be working with the City to ensure that the Shier Rings Road relocation will function properly and meet the University's needs, it is a City project. In the earlier public meeting, residents had few objections to this use or the relocated road. Their concerns were primarily focused on the existing infrastructure. OSU will participate in 32.5% of the cost of relocating Shier Rings Road to this location. OSU will share a similar financial responsibility for extending the water mainline, sanitary sewer mainline, and DubLink fiber through the site. In addition, the Avery-Shier Rings Road roundabout that is in the early planning stages of this project will receive a \$1.25 million contribution from OSU. The City will cover the remainder of its cost. In terms of economic development, this facility will generate many well-paying jobs. The EDA provides for a minimum of 350 jobs created in Phase 1, and it likely will be closer to 450 jobs. The jobs will average \$100,000 wages each, which will provide an economic benefit for the City. This is a phased project with unique components, in terms of wayfinding signage and parking for a highly programmed facility. The applicant will be returning to the Commission with a text that will address both phases of the project. Phase 2 could be an extension of the Phase 1 use, which are the medical offices and ambulatory care, or it could be a community-based hospital, similar to the Ohio Health hospital across US33. Phase 2 will be an additional extension of the existing building. There will be a lag in number of years between Phase 1 and Phase 2. The facility will look nice in the interim.

Dan Light, Executive Director of Outpatient Services, Wexner Medical Center, 6717 Brodie Blvd., Dublin, OH, stated that they have been planning for this facility for several years. Their objective is to create an outpatient care center that is focused on a comprehensive set of outpatient services. Phase 1 will provide same-day patient treatment. Their investment will be geared toward preventative services related to primary care, women's health, mental health and rehabilitation services. Having this major footprint within the City will open up many other opportunities, beyond health care, between OSU and the City. They are an academic medical center, part of the The Ohio State University. They are already in Dublin in a fragmented manner. Currently, they have less than 60,000 square feet of limited primary care access and multiple internal medicine specialties in different facilities. Operating multiple leased facilities is not very efficient. In the new facility, they will have in excess of 250,000 square feet, which will permit relocation and expansion of their existing services, but also addition of other services. Phase 1 is funded. They are beginning work on the design, and the intent is to open summer of 2022.

Amanda Hoffsis, Assistant V.P. of Planning, Architecture and Development of Real Estate, The Ohio State University, stated they are very interested in the site due to the US 33 corridor. They have been looking for a site in Dublin for some time and were unable to find one to fit this program this well, until now. This use fits well in this District. There was a need to locate the building close to US 33, as its use is consistent with other facilities in that area. It is a large building so it must be buffered from the residential community to the south and the roadway. The roadway will be pulled to the north, to the facility, and away from the residential community. The roadway leading to the facility with a drop-off area and parking are essential to the daily functions of the facility and to maintain low stress for the patients visiting the facility. It is

important to lay out the site to maintain short walks from the parking to the front door. They have put much thought into the patient flow within the building and how that will work when Phase II, the hospital, is developed. Parking in front will provide a buffer to the street, and there will be attractive landscaping. The ponds for stormwater management will also present an opportunity for beautification of the area. They have looked at ways to engage them with the sidewalks and pathways. There is an elliptical pathway, which will provide a way for physical therapy patients to use the outdoor space. Both patients and facility patrons can use the space. A large-scale green space with respite gardens will be adjacent to the dropoff at the front door and will provide a great view for the interior space of the facility. There will be two service entrances. There will be multiple ingress and egress points from the new road, but the primary entrance will lead to the front door. There will be a buffer zone between the dropoff and the parking area. There will be a limestone entry wall. Because this facility is in the West Innovation District (WID), the attempt is to maintain cleaner lines to align with the goal of a higher-tech look in the district, but with a Dublin feel. The lawn and trees will create a beautiful campus feel. An entry court will provide ease of access to the patients. The respite garden will provide a premier outdoor space nestled into the "U" of the building. There will be a café, outdoor seating, a quiet area, and an area for walking and exploring. Ms. Hoffsis shared the OSU standard sign plan and a rendering, depicting the materials and mass of the building. They are continuing to work on the internal layout of the building.

### **Public Comment**

Andrew Keeler, 5281 Brand Road, Dublin, stated that it is clear those who have spoken are very passionate about the work they are doing, balancing the needs of residents with the developers, and coming to a conclusion that is best for Dublin. The rendering shown is a work of art and clearly will provide many benefits to the City. The traffic study is diametrically opposed to what is shown on the screen. One is art; the other is science. The lengthy traffic study is thorough, but purely objective. It is the role of this Commission to weigh the objectivity with the subjectivity. What do the neighboring communities want and need, and how is this development going to affect them? Perhaps there is another alternative for the Shier Rings realignment that would be better for the residents and the University. It is important to recognize that the proposed layout does provide a buffer to the residents, which is very thoughtful, but it cuts off certain parts of the community from the remainder of the community. At the public hearing, it was clear that the residents feel disconnected already. It is important to remember that they are Dublin residents, as well. He encourages the Commission to continue listening to the residents, and before they sign off on something that was created by a computer with no subject overlay, think through the process.

Cristian Cooney, 5835 Baronscourt Way, President, Ballantrae Community Association, stated that the Board met on August 13 and unanimously agreed that they are delighted that OSU Wexner Center will become their neighbor. A concern was expressed about light pollution, which should not be very difficult to resolve. The Boards of the Ballantrae Community Association, the Villas of Ballantrae, and the Lakes of Ballantrae will be meeting on September 3 regarding the Shier Rings Road relocation. They request the City provide any available information, so that they can have a constructive meeting about the road network concerns. The residents have no issue with the relocated Shier Rings Road to support the Wexner Medical Center. Their concern is how their existing roadways will connect out of the neighborhood to it.

Richard Letizia, Facilities Engineer/Manager, Nestle Quality Assurance Center, 6625 Eiterman Road, Dublin, OH, stated that their site is located in the northern portion of the WID. They are not opposed to the Wexner Medical Center and the re-routing of Shier Rings Road, but they are concerned about transportation supporting the existing infrastructure. How will Shier Rings Road tie in on the north side? In addition to the future AEP project, there is a State project changing the US33-SR161 interchange at Post Road. There is also discussion about the expansion of Ohio University. Currently, all of their 200+ associates and their support traffic access their site from the north side. Should the Shier Rings Road access change, how much traffic will flow north to a two-lane road? They have a significant financial investment in Dublin -- \$40 million in capital projects since 2015. Nestle has been located in Dublin since 1989. They are an active member of this community and supportive of OSU, but are interested in knowing who in Dublin is holistically involved in directing the phasing and the way in which these projects will occur. They encourage caution.

### **Commission Discussion**

Ms. Kennedy inquired if the City has completed a final traffic study on this project.

Ms. Husak responded that the applicant is required to submit a traffic study with their application, and they have done so. Engineering has not yet finalized their review of the traffic study.

Ms. Kennedy inquired if the traffic study results would be brought back to the Commission.

Ms. Husak responded that the Commission's purview is the site. The traffic, engineering and public infrastructure is not part of a Planning Commission application. It is a CIP project to which the City has committed within the next few years.

Ms. Call stated that she appreciated the fact that residents from the adjacent properties have provided positive feedback regarding the project. She appreciates the presentation and the insight involved in creating a project of which OSU, the City and the residents can be proud.

Ms. Fox stated that traffic and the roadways are the major elements that have the neighbors concerned, not the facility itself. As a Council member and PZC liaison, she assures the residents that discussion will continue. Council wants a development that is good for the City but also good for the adjacent residents. She encourages the City Engineering and Planning Departments to take every resident comment to heart and ensure the best solution is found that works for all the parties. The Commission will discuss the Concept Plan tonight, but the traffic and the roadway is a separate issue. Council will commit to considering all the factors.

In regard to the Concept Plan, Ms. Fox stated that she is hopeful that the OSU Wexner Medical Center, as the first anchor in the WID, will create an environment that will draw the community to it because its site is attractive and engaging. She likes the design, particularly the section used for physical therapies. However, she noticed that the parking lots, even in Phase 2, are very large and long. She has family members who are handicapped and, as she views the dropoff area, she has a couple of suggestions. At the dropoff, between the parking lot and the front door, there is also a landscaped edge and an island. She would suggest some seating be added in the spaces between the parking and the front door for use by those who are handicapped. She would also suggest that the long parking lots be broken up with some pedestrian walkways, so pedestrians do not need to dodge between cars to reach the entrance. The walk between the vehicle and the front door would be less intimidating. She really likes the café courtyard and respite garden. People will use that space to be outside the facility for a change of air and environment. In the entry courtyard, there are concrete benches, which are cold and uninviting. Recently, with a

public park review, the City changed the concrete surfaces to wood. If we really want people to stop and sit, the surfaces should be comfortable. Finally, when this site development is completed, it should be possible to connect all the spaces with walkways.

Ms. Kennedy stated that from a design perspective, the courtyard and respite area appear to be a nod to the OSU Oval. The facility design is beautiful, and it will be a great addition to the community.

Mr. Wilson stated that this is a great facility, and he is happy they are locating it in Dublin. He would suggest the dropoff area be partially enclosed and able to accommodate several cars stopping to pick up and drop off patients. This would prevent the patients being exposed to the weather. He would also encourage more benches be provided throughout the campus. Overall, it is a great concept plan, and he welcomes them to Dublin.

Mr. Supelak thanked the facility representatives and the members of the public who spoke. He would encourage them to provide connections to City paths, as this will add dividends in the years to come. The uncertainty with the Shier Rings relocation is the main concern, but he is certain that a satisfactory solution will be identified. He agrees with the suggestion for a porte-cochere. The materiality is good and the preliminary massing is basic, but with a building like this, there is opportunity to augment the massing with fenestrations. There is opportunity to improve the architecture and make this a signature piece in Dublin.

Mr. Fishman concurred with his colleagues' comments. He welcomes them to Dublin; he is thrilled they are locating here!

Ms. Newell stated that she recently spent a large amount of time at the OSU Hospital complex with her father. She appreciates that there will be a facility closer to home. She likes the overall site layout, the respite area and the interaction on the site. Her one comment relates to the connectivity. The pathways should extend to the street frontage and provide ability to navigate safely through the parking lots. Wayfinding provided in the parking lots is critical to first-time visitors and will do much to relieve their stress. She was confused with the alignment of the roadway, but she trusts that City Engineering and Council will ensure a good road development. It would be nice if the Commission could see those details when this project comes back in order to understand better how it will fit within the overall fabric.

Ms. Newell thanked the applicant for their presentation and members of the public for their comments.

### ~~3. Enterprise Rent-A-Car, 19-062CU, Conditional Use~~

~~Ms. Newell stated that this application is a request for an approximately 4,500-square-foot vehicle rental business and car wash. The 1.49-acre site is west of Dublin Center Drive, approximately 900 feet southwest of the intersection with Sawmill Road and zoned Bridge Street District Sawmill Center Neighborhood.~~

### ~~Staff Presentation~~

~~Ms. Martin stated that this a request for a Conditional Use for a vehicle rental facility with associated car wash in an existing, 4,500-square-foot building on a 1.49-acre site within the~~



## PUBLIC MEETING COMMENTS

RE: OSU Ambulatory Center Project and  
the re-alignment of Shier Rings Rd.  
Tuesday, August 6, 2019

### Transcript of Comment Cards

1. FROM: Claudia Lundehl

*"I came upset about the road and traffic as I live in the Lakes of Ballentrae but I leave with the appreciation that there will be a green space between Shier Rings Rd. and the hospital."*

2. FROM: Tracy Loesing

*"Living right next to Shier Rings Rd. – this plan looks wonderful. The added two minutes of drive time versus less traffic is okay with me."*

3. FROM: Norb Loesing

*"I agree with the proposed re-route of Shier Rings Rd. and the cul-de-sac east of Baronscourt on Shier Rings Rd. I don't believe the cul-de-sac dividing the existing Shier Rings Rd. will cause additional traffic to flow thru Ballantrae (up Eiterman). I live along Shier Rings Rd. and most of the traffic comes from Cosgray (not thru Ballantrae). I very much like the idea of dividing the industrial part of Shier Rings Rd. (east of Baronscourt) from the residential part west of Baronscourt. Going 1/10<sup>th</sup> of a mile for the north on Eiterman to access the "new" Shier Rings Rd. is not bad!"*

4. FROM: Paula Eddy and Randy Eddy

*"Please keep "Old Shier Rings Rd." OPEN to Cosgray. Keeping it two lanes would be preferred."*

5. FROM: Mark

*"Could you consider widening Eiterman Rd. between Shier Rings Rd. and the new hospital? This would encourage drivers to use the new roads (and/or slightly longer route) rather than go through Ballantrae."*

6. FROM: Debby Ehlers

*"We want a roundabout at Eiterman and Shier Rings roads and another one at Old Shier Rings and new Shier Rings roads so old Shier Rings Rd. can stay open to Avery Rd."*

*Years ago when we moved to Ballantrae, we saw a plan for Shier Rings Rd. to be a four-lane road with a divider in the middle. Why was that plan dropped?"*

7. FROM: Lisa Roberson

*"Please help influence Avery Rd. south of Woerner Temple to Hilliard"*

8. FROM: Randy Ayers

*"Closing Shier Rings Rd. at Baronscourt and not allowing residents of Ballantrae to access Avery Rd. (at the proposed roundabout) is not reasonable. The current alignment of Shier Rings Rd. between Avery Rd. and Eiterman Rd. needs to remain open."*



9. FROM: Cristian Cooney

"Cutting off the northern collector (Shier Rings Rd.) from Avery is going to push way too much traffic down Woerner Temple. Not discussed is the load on the Avery overpass that will be the next issue the residents will raise.

10. FROM: Mileah Hamulak

*"I attended the introduction meeting of the west Innovation a couple of years ago. The main concern is the traffic it will bring to the area. That is still a concern. But I'm happy to see that those concerns were heard and the plan to move Shier Rings Rd. away from Ballantrae to keep the Innovation traffic away from the residential area. Please keep Churchman in mind to minimize traffic there because of all the condos in the area."*

11. FROM: Heidi Benner

*"Please, please, please don't close off Shier Rings Rd. to Ballantrae. Part of the beauty of our development is that we have great access to grocery, banks, churches, and SR161, etc. If this is closed and we have to head west to Eiterman Rd. to Post Rd. and back over, it takes away so much appeal. It doubles if not triples our time and makes us feel even more disconnected than we do. When people ask the best thing about Ballantrae, I always say we have great access to so much."*

12. FROM: Marlen Mathias

*"There should be a way for people coming east on Shier Rings Rd. from Darree Fields and the sports center to exit the area without going through the Ballantrae Neighborhood."*

13. FROM: Mike Salcone

*"Thank you for your presentation. I was very impressed with the plan with the exception of closing a portion of Shier Rings Rd. My suggestion would be to take a look at several other options that keep Shier Rings Rd. open to Cosgray."*

14. FROM: Kurt Loudon

*"Shier Rings Rd. needs to get to Avery Rd. Do not cut it off and place a cul-de-sac. We need to keep the route from Baronscourt Way to Avery Rd. via Shier Rings Rd."*

15. FROM: Pat Mullahey

*"Traffic flow – big problem."*

16. FROM: Lynne Herman

*"Leave an old Shier Rings Rd. open. Roundabouts that have been designed are very dangerous. The one at Bridge Park and the one leading to Eiterman. Why are we duplicating services we have – a hospital and an urgent care center with lots of medical outpatient buildings on perimeter? The roads like SR161 – East to get on I-270 all that happened was you moved the traffic congestion from Avery Rd. up to the intersection going to I-270. And you made a dangerous bridge and curve with repeated accidents. These don't lead to a lot of confidence in your services."*

17. FROM: Merry McCuen

*"#1. How will the truncated Shier Rings Rd. at Barronsway Ct. affect the fire and ambulances getting to the Lakes at Ballantrae and beyond? A lot of older people and it seems like this would slow emergency help to the people south of Barronsway Ct.*

*#2. Can you keep Shier Rings Rd. open thru to Avery and still add the new road? Seems like that would solve many of the issues /potential problems."*

18. FROM: Janell Thomas

*"Overall plan sounds good with the thoughts of separating business from neighborhoods. I understand concerns of Ballantrae neighbors about traffic. Woerner Temple wasn't mentioned as an exit to Avery from the neighborhood but it seems like a good option. Churchman isn't currently used because you can't get off of Rings Rd. onto Avery N. or Cosgray S. Both intersections need addressed ASAP. Appreciate the great job the City does. That's why we all live here."*

19. FROM: Janet McKeon

*"I leave Shier Rings Rd. alone and add your new road for hospital. Ballantrae does not want to be a cut-through for Cosgray Rd. You've protected Dublin business offices, etc. but cut out our access to town. This is all about OSU Hospital and they are ruling over all. 350 new jobs. Where are they going to live when two-bedrooms - new housing are going close to \$300,000. What are the residential of proposal for - Shier Rings Rd. (old) apartments, houses, and condos, etc."*

20. FROM: Dan Hassler

*"City Manager explanation of space concept of Shier Rings Rd. was logical and perfect. Great to see and hear that plan. I wish it were brought up earlier in the presentation. I like the Shier Rings re-alignment plan as it was presented. It will accomplish the goals of safety, access, and economic development. Thank you!"*

21. FROM: Tru Neel

- *"Eiterman/Shier Rings needs immediate attention - very dangerous. Roundabout would be good.*
- *No problem with proposed use, just roads & traffic patterns.*
- *Name of "old Shier Rings Rd?"*
- *Concern about time it will take emergency vehicles.*
- *Will city water/sewer come down old Shier Rings Rd?*
- *Definitely want to be notified of residential development details.*
- *No apartments that look like 'projects'."*

22. FROM: David Neel

*"After Shier Rings Rd. is truncated somewhere east of Eiterman, the heavy traffic back and forth between Cosgray and Avery will now have to jog to get from Cosgray to Avery. This will probably not be lessened and Shier Rings Rd. between Eiterman and Cosgray will still have heavy traffic on a way too narrow road. How can traffic be discouraged/redirected from using Shier Rings Rd? As new residences come in on Shier Rings Rd., the traffic will get worse. Think improvements to Shier Rings Rd. for future."*

23. FROM: Sue Day

*"#1. DON'T STOP SHIER RINGS RD. – especially for Fire/EMS access.*

*#2. Concern this lowers property values. People already have potential buyer that do to other areas where there is NOT a service center nearby. Now, all this?!*

*#3. Why not put access right to US33 – OSU moved Olentangy River Rd. and Cannon Dr. Can't this be done for them too?*

*#4. Getting tired of all the Dublin improvement projects with our tax dollars – we retire soon. We want to stay here but all your projects keep our tax dollars high. We won't be able to afford our own improvements. "Beautiful" – tired of hearing that with our tax dollars."*

24. FROM: Gary Jensen

*"How will you keep the community apprised of traffic options? Next hearing? You have my email address. When is next meeting?"*

25. FROM: Anonymous

*"I would like to suggest: widen Shier Rings Rd. into four lanes instead of making it a dead end road. As a resident of Ballantrae, living on Trafalgar Lane, we saw a "huge" increase of traffic on our street during the installation of the Cosgray roundabout. People speeding in 25mph zone. Not pleased with this entire proposal!"*

~~~~~  
**Email comments to Public Works after the meeting on 8/6/19:**

1. FROM: Donald Cuddington - via email August 7, 2019

*"I attended the hearing on Tuesday evening, August 6. This is just a suggestion that may already have been considered. Instead of building a new connector near Shier Rings and Avery, would it be possible to simply extend Baronscourt Way north (and add a stop light at the intersection of Shier Rings and Baronscourt) and then curve to the northwest toward Eiterman?*

*Also, I thought the Dublin City staff did a good job answering the questions and retaining their composure throughout. Please don't feel a need to respond to this email; just thought I'd suggest a possible solution to the traffic concerns.*

2. FROM: Marian Vordermark - via email August 11, 2019

*"It was an interesting meeting for the initial traffic pattern presentation for OSU Medical Center. As a resident of Glenealy, I travel this area frequently.*

*As I sat waiting for the signal to change facing the semi commercial area across Avery exit from southbound 33, I thought of this concept: why not re-design it like the exit directly across 33? THREE LANES--ONE DIRECT ACROSS THE STREET thru the commercial area to connect with the roundabout. That lane could be the artery out of that section. It would take the load off the Ballantrae traffic from n/b 33 & Avery commercial area. Plus, with the increased apts. along Avery in Columbus, etc., the traffic is increasing daily.*

*As a Costco shopper & Dublin Green, asl go home, I do use the quick access & exit off 33. The roundabouts existing and forthcoming direct me to this route as most convenient.*



*With conversations u have had with the Dublin residents, u know there are many who avoid the large 161 & Riverside Dr. Roundabout. This will be true with the upcoming 33/Post/Hyland roundabout to come. Residents will avoid it if there is another option. They will utilize the exit on SB 33 and exit Avery/Muirfield. I do that to avoid the high traffic area by the shopping centers and the bridge.*

*At the minimum, perhaps there is an opportunity to plan for the third lane exit to connect across Avery in the industrial area in the future. And employees of OSU may choose to enter here if the 161/Hyland-Croy roundabout is delayed.*

*Just my thoughts. Throw in a Fed Ex distribution Center? Dublin needs planners & engineers looking to the future in the region*

3. FROM: Debby Ehlers – Ballantrae Resident - via email August 10, 2019

*“After attending the Shier Rings Rd meeting this week and reflecting on everything that was said, I just wanted to reiterate what I wrote on my comment sheet. We have been Dublin residents since 1989 and have watched Dublin grow from a small town of 12,000 to a large prospering suburb. In 2006 we moved from the Dublinshire neighborhood to Ballantrae. After the move, we no longer felt like a resident but a stepchild to Dublin. Without children at home, we didn’t think that being in the Hilliard School District would be that big of a deal, but it has been. Finding out what was happening in our own city became very difficult since we no longer received the Dublin Newspaper and could not even find one to purchase. Again, we feel like Dublin is cutting us off. Years ago, we saw a Dublin plan to upgrade Shier Rings Rd from a 2 lane road to a 4 lane divided road similar to Muirfield Dr, and we were looking forward to this improvement. It appears the plan is now being disregarded, and we will be forced to drive thru more traffic to access the Avery/Muirfield Rd areas and the freeway. You indicated that the Ballantrae traffic will drive inward toward Woerner Temple, but in all likelihood you will be pushing more traffic through the OSU development instead. Please re-consider not cutting off our neighborhood and add a roundabout to your plan that will connect the old Shier Rings Rd to the new Shier Rings Rd.*

*The current intersection of Eiterman and Shier Rings is very dangerous and also needs a roundabout or light installed as soon as possible. We support the OSU development, but Ballantrae residents should not be disregarded as Dublin develops this area. It was mentioned that you were trying to move the traffic away from Ballantrae, but this logic does not hold up since Shier Rings has always been a busy road and the homeowners that built near the road should have been aware of the zoning for the vacant land near their homes.*

## Claudia Husak

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**From:** Jennifer Gibson <bgibson1@columbus.rr.com>  
**Sent:** Wednesday, August 14, 2019 12:05 PM  
**To:** Claudia Husak  
**Subject:** OSU project at Shier Rings Rd

Ms. Husak,

I am a resident at 5666 Tynecastle Loop and am writing to express my **STRONG** dissent to the proposed project on Shier Rings Road for the following reasons:

- The safety of our children. Rerouting traffic with one of only two alternatives through our neighborhood on residential streets will cause dangerous conditions for our children in grades k-5 who must (by district rule) walk to school.

- Public safety on Cosgray. The other alternative Cosgray Road is already at a heavy capacity due to the addition of the Dublin Green shopping center and exasperated by the city's failure to create a roundabout as promised at the entrance to the shopping center. Additionally, the road is subject to and barely supports tournament traffic at Darree Fields and Sports Ohio. Any further development around this road will only exacerbate the problem and create extremely dangerous capacity.

- The safety of our financial future. Our home values will be directly impacted due to the eye sore of a five story building and accompanying power lines in addition to increased traffic. This will directly impact our family's personal finances and long term financial security.

-We already have Dublin Methodist Hospital on the opposite side of 33, we don't need another huge health care facility which includes (eventually) emergency care. This is overkill, and will only decrease the property values of the homes in Ballantrae being so close to our neighborhood.

-Traffic flow is has already increased substantially with all of the building and expanding into and including Plain City, Cosgray Rd is backed up **DAILY** already with the current traffic flow.

Respectfully submitted,

Jennifer Gibson  
5666 Tynecastle Loop

## Claudia Husak

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**From:** Katie Montgomery <kmontgom80@gmail.com>  
**Sent:** Wednesday, August 14, 2019 11:22 AM  
**To:** Claudia Husak  
**Subject:** Shier Rings Road project

Dear Ms. Husak,

I am writing to express my STRONG objections to the proposed project on Shier Rings Road. My address is 5765 Trafalgar Lane in Dublin.

First and foremost, the safety of our children is at GREAT risk. Rerouting traffic with one of only two alternatives through our neighborhood on residential streets will cause DANGEROUS conditions for our children. There is no busing option for children K-5 who must walk to school (per district rule.) The roads are already heavily traveled enroute to the school due to the fact that we must take our children to school, and this will impact our children's safety greatly.

Traffic at Eiterman and Shier Rings is already congested, and there is backed up traffic nearly every day at different times of the day. I cannot image the affect the development will have on traffic on Eiterman and 161, especially at the seriously dangerous roundabout where NO ONE stops (going eastbound 161) or looks for cars coming off the roundabout onto Eiterman from 161. I almost get hit nearly every single day there.

The other alternative, Cosgray Road, is NOT a viable alternative. It is already at a heavy capacity due to the addition of the Dublin Green shopping center, and the city's failure to create a roundabout as promised at the entrance to the shopping center only feeds the problem. I avoid this path intentionally and go the "long way" because of the heavy backups I nearly always encounter, no matter the time of day. Additionally, the road is subject to and barely supports tournament traffic at Darree Fields and Sports Ohio. Any further development around this road will only exacerbate the problem and create extremely dangerous capacity. Cosgray Road is NOT a safe road. My 2 small children and I (I was also 8 months pregnant at the time) were hit by an oncoming car 18 months ago on Cosgray Road, which totaled our car. Luckily we were all unharmed, but each and every time I travel that dangerous road, I am reminded of how unsafe it is. Adding additional traffic to the Cosgray mess will undoubtedly result in loss of life. There is no question in my mind of this due to the unsafe narrow road and the speed that drivers drive on Cosgray.

We live in a beautiful neighborhood in a wonderful city, and the people who live in Ballantrae love it here. We have every desire to keep our neighborhood beautiful and for more people to enjoy what it has to offer. However, our home values will be directly impacted due to the eye sore of a five story building and accompanying power lines, not to mention the increased traffic. I am concerned about the direct impact on our family's personal finances as well as our financial security long-term. With that being said, I realize that development is necessary and I do not deem it a bad thing, but poor planning for city residents is a terrible problem that needs addressed.

I urge you to consider what this project means to the residents of Ballantrae, a beautiful DUBLIN neighborhood, and the impact it will have on our children as well as our families

Respectfully submitted,

Katie Montgomery  
5765 Trafalgar Ln  
Dublin, OH 43016  
614-783-1579

## Claudia Husak

---

**From:** Jessi Prizinsky <jessi.prizinsky@gmail.com>  
**Sent:** Wednesday, August 14, 2019 2:22 PM  
**To:** Claudia Husak  
**Subject:** City of Dublin Planning

Ms Husak,

I am a resident at 6687 Ballantrae Place and am writing to express my STRONG dissent to the proposed project on Shier Rings Road for the following reasons:

- The safety of our children. Rerouting traffic with one of only two alternatives through our neighborhood on residential streets will cause dangerous conditions for our children in grades k-5 who must (by district rule) walk to school.
- Public safety on Cosgray. The other alternative Cosgray Road is already at a heavy capacity due to the addition of the Dublin Green shopping center and exasperated by the city's failure to create a roundabout as promised at the entrance to the shopping center. Additionally, the road is subject to and barely supports tournament traffic at Darree Fields and Sports Ohio. Any further development around this road will only exacerbate the problem and create extremely dangerous capacity.
- The safety of our financial future. Our home values will be directly impacted due to the eye sore of a five story building and accompanying power lines in addition to increased traffic. This will directly impact our family's personal finances and long term financial security.

Respectfully submitted,  
Jessi Prizinsky

## Claudia Husak

---

**From:** Brian Riley <rileybc@gmail.com>  
**Sent:** Wednesday, August 14, 2019 11:18 AM  
**To:** Claudia Husak  
**Subject:** Shier Rings Road Project Concerns

Ms Husak,

I am a resident at 6719 Ballantrae Place and am writing to express my STRONG dissent to the proposed project on Shier Rings Road for the following reasons:

- The safety of our children. Rerouting traffic with one of only two alternatives through our neighborhood on residential streets will cause dangerous conditions for my children in grades k-5 who must (by district rule) walk to school.
- Public safety on Cosgray. The other alternative Cosgray Road is already at a heavy capacity due to the addition of the Dublin Green shopping center and exasperated by the city's failure to create a roundabout as promised at the entrance to the shopping center. Additionally, the road is subject to and barely supports tournament traffic at Darree Fields and Sports Ohio. Any further development around this road will only exacerbate the problem and create extremely dangerous capacity.
- The safety of our financial future. Our home values will be directly impacted due to the eye sore of a five story building and accompanying power lines in addition to increased traffic. This will directly impact our family's personal finances and long term financial security.

I am sure there are alternative plans which would mitigate these negative impacts to nearby residents like myself.

Respectfully submitted,

Brian Riley

## Claudia Husak

---

**From:** Jaime <jaimeschiavone@yahoo.com>  
**Sent:** Wednesday, August 14, 2019 11:17 AM  
**To:** Claudia Husak  
**Subject:** Ballantrae

Ms Husak,

I am a resident at 5853 Trafalgar Lane, Dublin, and I am writing to express my STRONG dissent to the proposed project on Shier Rings Road for the following reasons:

- The safety of our children. Re-routing traffic through our neighborhood on residential streets will cause dangerous conditions for our children, along with increased traffic.
- Public safety on Cosgray. The other alternative Cosgray Road is already at a heavy capacity due to the addition of the Dublin Green shopping center and exasperated by the city's failure to create a roundabout as promised at the entrance to the shopping center. Additionally, the road is subject to and barely supports tournament traffic at Darree Fields and Sports Ohio. Any further development around this road will only exacerbate the problem and create extremely dangerous capacity.
- The safety of our financial future. Our home values will be directly impacted due to the five story building and accompanying power lines, in addition to increased traffic. This will directly impact our family's personal finances and long term financial security.

Sincerely,

Jaime Schiavone

## Claudia Husak

---

**From:** Amy Swank <swank8506@gmail.com>  
**Sent:** Wednesday, August 14, 2019 11:29 AM  
**To:** Claudia Husak  
**Subject:** Shier Rings Rd Project

Ms Husak

I am a resident at 5945 Vandeleur Pl in Dublin and am writing to express my STRONG dissent to the proposed project on Shier Rings Road for the following reasons:

- The safety of our children. Rerouting traffic with one of only two alternatives through our neighborhood on residential streets will cause dangerous conditions for our children in grades k-5 who must by district rule walk to school.
- Public safety on Cosgray. The other alternative Cosgray Road is already at a heavy capacity due to the addition of the Dublin Green shopping center and exasperated by the city's failure to create a roundabout as promised at the entrance to the shopping center. Additionally, the road is subject to and barely supports tournament traffic at Darree Fields and Sports Ohio. Any further development around this road will only exacerbate the problem and create extremely dangerous capacity.
- The safety of our financial future. Our home values will be directly impacted due to the eye sore of a five story building and accompanying power lines in addition to increased traffic. This will directly impact our family's personal finances and long term financial security.

Respectfully submitted,

Amy Swank

## Claudia Husak

---

**From:** Gretchen Tinnerman <gretchen.tinnerman@gmail.com>  
**Sent:** Wednesday, August 14, 2019 12:30 PM  
**To:** Claudia Husak  
**Cc:** Husband; Jane Fox  
**Subject:** OSU Medical Campus Project

Ms Husak:

I am a resident at 6760 Trafalgar Loop (Ballantrae). My husband also owns a business in Dublin, OH that we chose to retain in Dublin due to our residential status in Dublin. I recognize the location of the proposed OSU medical campus is desirable for economic development. And I support the City's strength in economic development as a citizen. However, the way this project has been handled is very disappointing and it appears currently that the City has no concern for the Ballantrae subdivision and tax paying citizens. I am writing to express my STRONG dissent to the proposed project on Shier Rings Road for the following reasons:

- The safety of our children. Rerouting traffic with one of only two alternatives through our neighborhood on residential streets will cause dangerous conditions for our children in grades k-5 who must (by district rule) walk to school.

- It does not appear that the City has taken into account Avery Road between Rings Rd & Hayden Run Rd. I understand there are three municipalities that operate this roadway. But the additional traffic from Hilliard residents traveling to these proposed facilities will only increase traffic in what is an already dangerous road. I have a teenage driver that travels this road to Davidson every day and it simply is not safe. I have personally also blown a tire and was forced off the road due to pot holes that go unfilled and the narrow road that does not manage heavy traffic well.

- Public safety on Cosgray. The other alternative Cosgray Road is already at a heavy capacity due to the addition of the Dublin Green shopping center and exasperated by the city's failure to create a roundabout as promised at the entrance to the shopping center. Additionally, the road is subject to and barely supports tournament traffic at Darree Fields and Sports Ohio. Any further development around this road will only exacerbate the problem and create extremely dangerous capacity.

- **The safety of our financial future.** Our home values will be directly impacted due to the eye sore of a five story building and accompanying power lines in addition to increased traffic. This will directly impact our family's personal finances and long term financial security.

Respectfully submitted,

Dave & Gretchen Tinnerman

[gretchen.tinnerman@gmail.com](mailto:gretchen.tinnerman@gmail.com)

614-684-8128





**FILE COPY**

Planning | 2019

Case # 19 - 055Z/PDP

**PLANNING APPLICATION**

This is the general application form for all City of Dublin Boards and Commissions. In addition, applicants should submit a checklist with the requirements for the application type indicated below. Attach additional sheets as necessary.

**I. REVIEW REQUESTED:** The following are abbreviations for specific zoning districts within the City of Dublin with associated application types identified below:  
 PUD - Planned Unit Development  
 BSD - Bridge Street District  
 WID - West Innovation District

- Administrative Appeal
- Administrative Departure
- Amended Final Development Plan (PUD)
- Amended Final Development Plan - Sign (PUD)
- Architectural Review Board
- Building Code Appeal
- Community Plan Amendment
- Concept Plan (BSD or PUD)
- Conditional Use
- Development Plan Review (WID)
- Demolition
- Final Development Plan (BSD or PUD)
- Final Plat
- Informal Review
- Master Sign Plan
- Minor Project
- Minor Subdivision
- Non-Use (Area) Variance
- Preliminary Development Plan (BSD)
- Preliminary Plat
- Rezoning with Preliminary Development Plan (PUD)
- Site Plan Review (WID)
- Special Permit
- Standard District Rezoning
- Use Variance
- Waiver Review
- Wireless Communications Facility
- Zoning Code Amendment

**II. PROPERTY INFORMATION:** Provide information about the property including existing and proposed development.

|                                                                                                                     |                                                                               |
|---------------------------------------------------------------------------------------------------------------------|-------------------------------------------------------------------------------|
| Property Address(es):<br><b>6600 Shier-Rings Road<br/>0 Eiterman Road</b>                                           |                                                                               |
| Tax ID/Parcel Number(s)<br>(List All):<br><b>Portions of<br/>273-008174, 273-008175 &amp;<br/>274-001114</b>        | Parcel Size(s) in Acres<br>(List Each Separately):<br><b>33.923 +/- acres</b> |
| Existing Land Use/Development:<br><b>Vacant</b>                                                                     | Existing Zoning District:<br><b>ID-1 Research Office<br/>District</b>         |
| Proposed Land Use/Development:<br><b>Ambulatory medical<br/>care facility and other<br/>medical and office uses</b> | Proposed Zoning District:<br><b>PUD</b>                                       |

**III. CURRENT PROPERTY OWNER(S):** Indicate the person(s) or organization(s) who own the property proposed for development.

|                                                                                                                              |
|------------------------------------------------------------------------------------------------------------------------------|
| Name (Individual or Organization):<br><b>City of Dublin</b>                                                                  |
| Mailing Address (Street, City, State, ZIP):<br><br><b>Attn: City Manager<br/>5200 Emerald Parkway<br/>Dublin, Ohio 43017</b> |
| Email/Phone Number:<br><b>dmcdaniel@dublin.oh.us<br/>614.410.4400</b>                                                        |

For questions or more information, please contact Planning at 614.410.4600 | [www.dublinohioUSA.gov](http://www.dublinohioUSA.gov)



**IV. APPLICANT(S):** Complete this section if the person/organization representing the applicant/ property owner is different from the applicant.

Not Applicable

Name (Individual or Organization): **The Ohio University Wexner Medical Center**

Mailing Address (Street, City, State, ZIP):  
Attn: Keith Myers, VP of Planning and Real Estate, 101 Bricker Hall, 190 North Oval Mall, Columbus, Ohio 43210

Phone Number: **614.247.5968**

Email: **myers.661@osu.edu**

**V. REPRESENTATIVE(S):** Complete this section if the person/ organization representing the applicant/ property owner is different from the applicant (such as the project manager or property owner's legal council).

Not Applicable

Name (Individual or Organization): **Aaron L. Underhill, Underhill & Hodge LLC**

Mailing Address (Street, City, State, ZIP): **8000 Walton Parkway, Suite 260, New Albany, Ohio 43054**

Phone Number: **614.335.9320**

Email: **aaron@uhlfirm.com**


**VI. PROPERTY OWNER'S AUTHORIZATION OF APPLICANT(S)/ AUTHORIZED REPRESENTATIVE:** The Property Owner listed in Section III must authorize the Applicant listed in Section IV and/or the Authorized Representative listed in Section V to act on the Owner's behalf with respect to this application.

Not Applicable

I Dana L. McDaniel, the **property owner**, hereby authorize OSU / Aaron Underhill  
To act as my **representative(s)** in all matters pertaining to the processing and approval of this application, including modification to the application. I agree to be bound by all representations and agreements made by the designated representative (listed in Sections III and/or IV).

Original Signature of Property Owner (listed in Section II): 

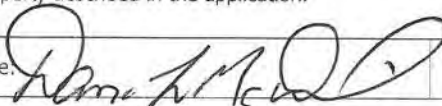
Subscribed and sworn before me this 12 day of July, 2019  
State of Ohio  
County of Franklin Notary Public Linda L. Ghik



**LINDA L. GHIK**  
NOTARY PUBLIC 12/19  
FOR THE  
STATE OF OHIO  
My Commission Expires  
May 19, 2024

**VII. AUTHORIZATION TO VISIT THE PROPERTY:** Site visits to the property by City representative are essential to process the application. The Property Owner/ Applicant/ Authorized Representative (listed in Section II), hereby authorizes City representatives to enter, photograph, and post a notice on the property described in this application. This is optional, but strongly recommended.

I Dana L. McDaniel, the **property owner or authorized representative**, hereby authorize City representatives to enter, photograph and post a notice on the property described in the application.

Original Signature of Property Owner or Authorized Representative:  Date: 7/12/19



**VIII. APPLICANT'S AFFIDAVIT OF ACKNOWLEDGMENT:** This section must be completed with an **original signature** and **notarized**.

Original Document Attached

I Dana L. McDaniel, the **property owner** or **authorized representative**, have read and understand the contents of this application. The information contained in this application, attached exhibits and other information submitted is complete and in all respects true and correct to best of my knowledge and belief.

Original Signature of Property Owner or Authorized Representative: [Signature] Date: 7/12/19

Subscribed and sworn before me this 12 day of July, 2019  
 State of Ohio County of Franklin Notary Public Linda L. Glick



**LINDA L. GLICK**  
**NOTARY PUBLIC**  
 FOR THE  
 STATE OF OHIO  
 My Commission Expires  
**May 19, 2024**

**FOR OFFICE USE ONLY:**

|                                                                                    |                                         |
|------------------------------------------------------------------------------------|-----------------------------------------|
| Case Title: <u>POD - OSU MEDICAL CAMPUS</u>                                        | Date Received: <u>7/3/19</u>            |
| Case Number: <u>19-055Z/PDP</u>                                                    | Next Decision Due Date (If Applicable): |
| Amount Received: <u>\$4,965.00</u>                                                 | Final Date of Determination:            |
| Receipt Number: <u>16512</u>                                                       | Related Cases:                          |
| Reviewing Body (Circle One): <u>ART</u> <u>ARB</u> <u>BZA</u> <u>CC</u> <u>PZC</u> |                                         |
| Map Zone: <u>E-2</u>                                                               |                                         |
| Determination or Action:                                                           |                                         |
| Ordinance Number (If Applicable):                                                  |                                         |

**FILE COPY**

**RECEIVED**  
 JUL 03 2019  
 CITY OF DUBLIN  
 PLANNING





## Rezoning Statement

The property that is the subject of this application will constitute the OSU Medical Campus Zoning District and consists of 33.9+/- acres located to the south of and adjacent to U.S. Route 33, generally to the north of Shier-Rings Road, to the east of Eiterman Road, and to the west of Avery Road. The site is currently owned by the City of Dublin but a contract has been executed by the City and The Ohio State University (the "University"), on behalf of its Wexner Medical Center (the "Med Center"), pursuant to which the University will purchase the property with the intent to develop and operate permitted uses in accordance with the associated zoning text and preliminary development plan.

The Med Center is undertaking an initiative to expand the reach of its medical services beyond its main campus. It is evaluating and pursuing select opportunities in suburban locations around Central Ohio where it can bring comprehensive services and facilities closer to patients, and the real property that is the subject of this text has been identified as an ideal location.

Land to the north and northeast of this property is owned by Mount Carmel Health System and OhioHealth Corp., respectively. Therefore the intended use of this property, which will include an ambulatory care facility, medical office space and other related uses, will be consistent with the existing uses in the immediately surrounding areas. The term "ambulatory care facility" is expansive and is not simply defined, but can generally be described as being "a facility where outpatient medical procedures, testing, and treatment are provided in addition to traditional physician consultations."

The first phase is anticipated to include approximately 250,000 square feet of space from which these uses will operate. A second future phase could expand the first phase by up to another 300,000 square feet and would include additional medical and related facilities. Prior to the opening of the initial phase of the project the City will be relocating existing Shier-Rings Road to a location that abuts the southern boundary of this Zoning District (such new location to be referred to herein as "Relocated Shier-Rings Road").

The property that is the subject of this rezoning is part of the Economic Advancement Zone, and prior to the approval of this rezoning, the property is zoned in the Research Office (ID-1) District. The Dublin Community Plan's future land use recommendation with regard to this property is MUR – Research and Development. The development standards in the related text and the accompanying preliminary development plan further this vision and intent while accommodating the unique nature of the proposed uses and development.

Due to the unique nature of the proposed uses for this property and the arrangement of uses on the site, a planned unit development designation is being used to provide for flexibility to accommodate development that would not otherwise comply with all of the ID-1 zoning district requirements. Section 153.037(H)(1) provides, in part, that "[t]he purpose of the Research Office District is to provide for the integration of large format research and office uses in multistory buildings. Uses within the ID-1 District may include a mix of office, research, laboratory, clean manufacturing, assembly, and other uses incidental to office, technology and research uses. Higher-intensity sites in this district include those with higher freeway and major arterial visibility."

**19-055CP/Z/PDP:** affected  
**property labels**  
**ALL Labels for OSU MEDICAL**  
**CAMPUS as of 8/9/19 including**

**Addresses from 8/6/19**  
**Public Mtg Re: OSU**  
**Ambulatory Ctr and**  
**Re: road re-alignments...start**  
⇒

PAUL & CLAUDIA LUNDAHL  
6661 BANTRY CT  
DUBLIN, OH 43016

DAWN BACHERT  
378 EDINBURGH RD  
DUBLIN, OH 43017

ED & JANET MCKEON  
6669 BANTRY CT  
DUBLIN, OH 43016

RANDY AYERS  
5940 ROUNDSTONE PL  
DUBLIN, OH 43016

NOLAN & ANN BERDEN  
5917 ROUNDSTONE PL  
DUBLIN, OH 43016

RICHARD LOTIZIA  
6625 EITERMAN RD  
DUBLIN, OH 43016

KATHLEEN MALONE  
6890 FORESTHAVEN LOOP  
DUBLIN, OH 43016

Amy & Michael Porteus  
5589 Royal Dublin Drive  
DUBLIN, OH 43016

DAN HASSLER  
6676 BANTRY CT  
DUBLIN, OH 43016

PASTOR JIM STEVANUS  
FELLOWSHIP BAPTIST CHURCH OF  
DUBLIN  
6720 SHIER RINGS RD  
DUBLIN, OH 43016

JIM & KATHY ODDI  
6680 BANTRY CT  
DUBLIN, OH 43016

SANDY MEYER  
6073 SHIER LN  
DUBLIN, OH 43016

GRETCHEN TINNERMAN  
6760 TRAFALGAR LOOP  
DUBLIN, OH 43016

MILEAH HAMULAK  
6998 WIND ROSE WAY  
DUBLIN, OH 43016

MARLEN MATHIAS  
5868 BARONSCOURT WAY  
DUBLIN, OH 43016

DONALD CUDDZITE  
FELLOWSHIP BAPTIST CHURCH OF  
DUBLIN  
6720 SHIER RINGS RD  
DUBLIN, OH 43016

ANNE COLINAN DUTOIT  
5956 DUNHEATH LOOP  
DUBLIN, OH 43016

DEANNA SPEAS  
6090 SHIER LANE  
DUBLIN, OH 43016

ULRIK NYGAARD  
6591 BARONSCOURT LOOP  
DUBLIN, OH 43016

MIKE SELENE  
5904 TRAFALGAR LN  
DUBLIN, OH 43016

LUEIDONG HUDANG  
6623 BARRONSCOURT LOOP  
DUBLIN, OH 43016

YOGENDXA SHAH  
6061 BARONSCOURT WAY  
DUBLIN, OH 43016

CHRIS & TODD TAGESON  
5985 TRAFALGAR CT  
DUBLIN, OH 43016

TRUDY & DAVID NEEL  
6076 SHIER LN  
DUBLIN, OH 43016

KEN THOMAS  
6285 HAMPTON GREEN PL  
DUBLIN, OH 43016

JACK CURTIS  
6485 BALLANTRAE PL  
DUBLIN, OH 43016

PAT MULLAHEY  
6620 DALMORE LN  
DUBLIN, OH 43016

TIM GEORGE  
5895 WINSLOW CT  
DUBLIN, OH 43016

JEFF STARL  
6371 SHIER RINGS RD  
DUBLIN, OH 43016

AMANDA HOFFSIS  
1534 N. HIGH ST  
COLUMBUS, 43201

SONIA ABUZALCHM  
6784 TRAFALGAR LOOP  
DUBLIN, OH 43016

MERRY & JOEL McCUEM, TR  
6677 BANTRY CT  
DUBLIN, OH 43016

BILL & DIANE BLAKE  
6648 BANTRY CT  
DUBLIN, OH 43016

MITCH VINCENT  
6245 WESTERVILLE RD  
WESTERVILLE, OH 43081

DAN LARKIN  
6715 GLASIN CT  
DUBLIN, OH 43016

PAT BORGES  
6704 BANTRY CT  
DUBLIN, OH 43016

NORB & TRACY LOESING  
6692 BANTRY CT  
DUBLIN, OH 43016

GARY JENSEN  
6618 STRATHERN CT  
DUBLIN, OH 43016

KURT SMITH  
6692 ROUNDSTONE LOOP  
DUBLIN, OH 43016

MIKE KREBER  
5985 WILCOX PL  
DUBLIN, OH 43016

MIKE PREGENT  
7003 WATERS EDGE DR  
DUBLIN, OH 43016

JIM CHRISTOPHERSEN  
6685 STONE CIRCLE WAY  
DUBLIN, OH 43016

JOHN & DION MCCAULEY  
6939 SHIER RINGS RD  
DUBLIN, OH 43016

JOSEPH MONSKA  
6082 TUSWELL DR  
DUBLIN, OH 43016

REGINA BREMER  
6671 CARINLOUGH PL  
DUBLIN, OH 43016

NICK BRANT  
6190 ENTERPRISE CT  
DUBLIN, OH 43016

TONYA & GAYLE HOLTON  
5957 ROUNDSTONE PL  
DUBLIN, OH 43016

DEBBIE & ALAN EHLERS  
5914 VANDELEUR PL  
DUBLIN, OH 43016

CHARLES KRUMHOLTZ  
6124 GLENVILLAGE DR  
DUBLIN, OH 43016

KENNETH BECKER  
6688 BANTRY CT  
DUBLIN, OH 43016

SUSAN DAY  
6603 BARONSCOURT LOOP  
DUBLIN, OH 43016

RANDY & PAULA EDDY  
FELLOWSHIP BAPTIST CHURCH OF  
DUBLIN  
6720 SHIER RINGS RD  
DUBLIN, OH 43016

SANDRA ALLEN  
6308 VALLEY STREAM DR  
DUBLIN, OH 43017

JOE DEMAREE  
5839 BUNDON HUSH LN  
NEW ALBANY, OH 43054

LYNNE HERMAN  
5989 BARONSCOURT WAY  
DUBLIN, OH 43016

LARRY CARNS  
6644 BALLANTRAE PL  
DUBLIN, OH 43016

JEFF & KELLY BOLLINGER  
6089 SHIER LN  
DUBLIN, OH 43016

DAVE RIEGLE  
7045 WIND ROSE WAY  
DUBLIN, OH 43016

CRISTIAN COONEY  
5835 BARONSCOURT WAY  
DUBLIN, OH 43016

PAUL KNAPP  
6700 BALLANTRAE PL  
DUBLIN, OH 43016

FREEMAN MAST  
6688 ROUNDSTONE LOOP  
DUBLIN, OH 43016

TODD FREEMAN  
6776 TRAFALGAR LOOP  
DUBLIN, OH 43016

MARIAN VORDERMARK  
6220 ARDMORE WAY  
DUBLIN, OH 43016

MIKE MCCAMMON  
6708 BANTRY CT  
DUBLIN, OH 43016

KURT LOUDON  
6579 BARONSCOURT LOOP  
DUBLIN, OH 43016

KEN WAYMAN  
6693 STONE CIRCLE WAY  
DUBLIN, OH 43016

MARGIE & JERRY AEH  
6700 BANTRY CT  
DUBLIN, OH 43016

JON CARSON KUNDTZ  
P.O. BOX 96  
DUBLIN, OH 43017

KEN MILLER  
6509 BALLANTRAE PL  
DUBLIN, OH 43016

KEN & CAROL HILLER  
6752 TRAFALGAR LOOP  
DUBLIN, OH 43016

MICHAEL KING  
5884 DUNHEATH LOOP  
DUBLIN, OH 43016

COURTNEY MITCHELL  
5884 DUNHEATH LOOP  
DUBLIN, OH 43016

PETE FRIEDMAN  
6612 DALMORE LN  
DUBLIN, OH

CRYSTIN HAINS  
2982 GLENLOCH CIRCLE  
DUBLIN, OH 43017

PAULA CARTER  
6656 BANTRY CT  
DUBLIN, OH 43016

REGINA VANIK  
5936 ROUNDSTONE PL  
DUBLIN, OH 43016

TOM KREBER  
7786 BRANDON WAY DR  
DUBLIN, OH

JANELL THOMAS  
6962 WIND ROSE WAY  
DUBLIN, OH 43016

VIRGIL MATHIAS  
5868 BARONSCOURT WAY  
DUBLIN, OH 43016

JIM BALDWIN  
12675 HICKORY RIDGE  
PLAIN CITY, OH 43064

KEITH PICKELHEIMER  
7070 CLEAR WATER CT  
POWELL, OH 43065

BILL DUFORT  
5956 DUNHEATH LOOP  
DUBLIN, OH 43016

LISA & TIM ROBERSON  
7000 WATERS EDGE DR  
DUBLIN, OH 43016

HEIDI BENNER  
6627 BARONSCOURT LOOP  
DUBLIN, OH 43016

MIKE & DEBORAH SALCONE  
5904 TRAFALGAR LN  
DUBLIN, OH 43016

KIRK HENDRICKS  
6300 IRELAN PL  
DUBLIN, OH 43016

KIM HOWELL  
6712 STONE CIRCLE WAY  
DUBLIN, OH 43016

LUIS WEIL  
6772 BIRCHTON POINT DR  
DUBLIN, OH 43017



TARA ROE  
5749 TRAFALGAR LN  
DUBLIN, OH 43016

KEITH MYERS  
101 BRICKER HALL  
190 N. OVAL MALL  
COLUMBUS, OH 43210

AARON UNDERHILL  
8000 WALTON PKWY., STE. 260  
NEW ALBANY, OH 43054

BATES PROP. MGMT.  
5500 STANLEY STEAMER  
PKWY.  
DUBLIN, OH 43016

SUTPHEN REALTY LLC  
7000 COLUMBUS MARYSVILLE  
AMLIN, OH 43002

JASON HUFFMAN  
7065 SHIER RINGS ROAD  
DUBLIN, OH 43016

~~OHIO HEALTH CORP.~~  
~~1087 W. DENNISON AVE.~~  
~~COLUMBUS, OH 43204~~

MOUNT CARMEL HEALTH  
SYSTEM  
793 W. STATE ST.  
COLUMBUS, OH 43222

HISHAM AKEL  
6045 BARONSCOURT WAY  
DUBLIN, OH 43016

PAUL ROZGO  
6053 BARONSCOURT WAY  
DUBLIN, OH 43016

YOGENDRA SHAH  
6061 BARONSCOURT WAY  
DUBLIN, OH 43016

DUBLIN CITY SCHOOL DIST.  
7030 COFFMAN RD.  
DUBLIN, OH 43017

ANTHONY & JOHN GIOFFRE  
6262 EITERMAN RD.  
DUBLIN, OH 43016

WASHINGTON TOWNSHIP  
6200 EITERMAN RD.  
DUBLIN, OH 43016

IMA MOORE  
7055 SHIER RINGS RD.  
DUBLIN, OH 43016

RAKKIYAPPAN CHINNAPPAN  
6672 BANTRY CT.  
DUBLIN, OH 43016

TERESA HANSON  
6668 BANTRY CT.  
DUBLIN, OH 43016

SUSIE MILLER  
6664 BANTRY CT.  
DUBLIN, OH 43016

JAMES & JOAN DUCKWORTH  
6660 BANTRY CT.  
DUBLIN, OH 43016

PAULA CARTER & CHESTER  
GUY CASAGRANDE  
6656 BANTRY CT.  
DUBLIN, OH 43016

Stephen & Stacey Landoll  
6113 Shier L  
Dublin, OH 43016

BEATRICE BROGAN  
7025 SHIER RINGS RD  
DUBLIN, OH 43016

WILLIAM COOK  
6684 BANTRY CT.  
DUBLIN, OH 43016

ALEXANDER GUILLERIN  
KAREN SANKOVICH  
6037 BARONSCOURT WAY  
DUBLIN, OH 43016

WILLIAM BAKER  
9711 E. PIKE RD.  
CAMBRIDGE, OH 43725

33 DUBLIN INDUSTRIAL PARK  
6223 AVERY RD.  
DUBLIN, OH 43016

Rock Noah LLC  
8494 S. 700 E Ste. 200  
Sandy, UT 84070

PATRICK & LINDA  
HENTHORNE  
6655 BARONSCOURT LOOP  
DUBLIN, OH 43016

DANIEL & JODEE GALLAGHER  
6659 BARONSCOURT LOOP  
DUBLIN, OH 43016

WILLIAM & TAMARA LEASURE  
6671 BARONSCOURT LOOP  
DUBLIN, OH 43016

MUSTAFA DUGLUM  
DENANA SEJFOVIC  
6061 SHIER LN  
DUBLIN, OH 43016

RONNY GREEN  
6957 SHIER RINGS RD  
DUBLIN, OH 43016

AMERCO REAL ESTATE CO.  
6405 AVERY RD.  
DUBLIN, OH 43016

STEVEN & ROBERTA SMART,  
TR  
6681 BANTRY CT  
DUBLIN, OH 43016

RICHARD HEADLEY, TR  
6673 BANTRY CT  
DUBLIN, OH 43016

33 DUBLIN INDUSTRIAL PARK  
6500 SHIER RINGS RD  
DUBLIN, OH 43016

JOSHUA FEELY  
KIMBERLY WELLS  
6675 BARONSCOURT WAY  
DUBLIN, OH 43016

PATRICK BORGES, TR  
6704 BANTRY CT  
DUBLIN, OH 43016

JAVIER & BRIDGET RAMOS  
6652 BANTRY CT  
DUBLIN, OH 43016

JAMES RAVE  
6114 SHIER LN  
DUBLIN, OH 43016

MARK SHIER  
7026 SHIER RINGS RD  
DUBLIN, OH 43016

JOHN JANKOVIC  
6665 BANTRY CT  
DUBLIN, OH 43016

SUSAN DeSHETLER  
6696 BANTRY CT  
DUBLIN, OH 43016

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PID 274-000660  
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6071 HOLLIDAY LN  
DUBLIN, OH 43016

MICHAEL & SHARON RAMSEY  
6111 HOLLIDAY LN  
DUBLIN, OH 43016

JOHN GIBBONS  
JENNIFER MCCOY  
6074 HOLLIDAY LN  
DUBLIN, OH 43016

PROPERTY OWNER  
6095 HOLLIDAY LN  
DUBLIN, OH 43016

DENNIS & MYRA CAPLINGER  
6079 HOLLIDAY LN  
DUBLIN, OH 43016

RICHARD & BRIDGET DYGERT  
6087 HOLLIDAY LN  
DUBLIN, OH 43016

CHARLOTTE STUKEY  
6103 HOLIDAY LN  
DUBLIN, OH 43016

DAN TERRIBLE  
4915 Sealover Hollow  
Philo, OH 43771

RAY COLLEY  
6565 TANTALLON SQUARE  
DUBLIN, OH 43016

ERNIE BEZEMES  
6946 WIND ROSE WAY  
DUBLIN, OH 43016

GEORGE KONTOGIANNIS  
6956 BALLANTRAE LOOP  
DUBLIN, OH 43016

MIKE BACON  
5780 BARONSCOURT WAY  
DUBLIN, OH 43016

MATT DOWNS  
5784 BONALY CT  
DUBLIN, OH 43016

TODD KERNER  
5576 NEWTONMORE  
DUBLIN, OH 43016



## OSU MEDICAL CAMPUS ZONING DISTRICT

### PLANNED UNIT DEVELOPMENT (PUD) TEXT

As recommend for approval by the Planning and Zoning Commission and revised accordingly for City Council review as part of Ordinance 53-19

First Reading October 21, 2019  
Second Reading November 4, 2019

#### I. DISTRICT PURPOSE AND INTENT.

(A) *Purpose.* The property that is the subject of this application will constitute the OSU Medical Campus Zoning District (the “Zoning District”) and consists of 33.8+/- acres located to the south of and adjacent to U.S. Route 33, generally to the north of Shier-Rings Road, to the east of Eiterman Road, and to the west of Avery Road. On the date of this text the site is owned by the City of Dublin but a contract has been executed by the City and The Ohio State University (the “University”), on behalf of its Wexner Medical Center (the “Med Center”), pursuant to which the University will purchase the property with the intent to develop and operate permitted uses in accordance with the requirements of this text and its associated preliminary development plan.

The Med Center is undertaking an initiative to expand the reach of its medical services beyond its main campus. It is evaluating and pursuing select opportunities in suburban locations around Central Ohio where it can bring comprehensive services and facilities closer to patients. The real property that is the subject of this text has been identified as an ideal location.

The initial phase of the project is anticipated to include an ambulatory care facility, medical office space, and related uses. The term “ambulatory care facility” is expansive and is not simply defined, but can generally be described as being “a facility where outpatient medical procedures, testing, and treatment are provided in addition to traditional physician consultations”. The first phase is anticipated to include approximately 250,000 square feet of space from which these uses will operate. A second future phase could expand the first phase by up to another 300,000 square feet and would include additional medical and related facilities. Prior to the opening of the initial phase of the project the City will be relocating existing Shier-Rings Road to a location that abuts the southern boundary of this Zoning District (such new location to be referred to herein as “Relocated Shier-Rings Road”).

(B) *Conflicts.* If the requirements in this text conflict in any manner with the City of Dublin Codified Ordinances, the provisions of this text shall prevail. All matters not covered by this text shall be regulated by the requirements and standards contained in the City of Dublin Zoning Code (“Zoning Code”).

(C) *Severability*. If any provision or provisions of this text or the application thereof to any zoning lot, building or other structure or tract of land are declared by a court of competent jurisdiction to be invalid or ineffective in whole or in part or to be inapplicable to any person or situation, the effect of such decision shall be limited to the provision or provisions which are expressly stated in the decision to be invalid or ineffective to the zoning lot, building or other structure or tract of land immediately involved. All other provisions of this text shall continue to be separate and fully effective, and the application of any such provision to other persons or situations shall not be affected.

(D) *District intent*. The property that is the subject of this text is part of the Economic Advancement Zone, and prior to the approval of this text the property is zoned in the Research Office (ID-1) District. Due to the unique nature of the proposed uses in this Zoning District and the arrangement of uses on the site, a planned unit development designation is being used to provide for flexibility to accommodate development that would not otherwise comply with all of the ID-1 zoning district requirements. Section 153.037(H)(1) provides, in part, that “[t]he purpose of the Research Office District is to provide for the integration of large format research and office uses in multistory buildings. Uses within the ID-1 District may include a mix of office, research, laboratory, clean manufacturing, assembly, and other uses incidental to office, technology and research uses. Higher-intensity sites in this district include those with higher freeway and major arterial visibility.” The development standards in this text and the accompanying preliminary development plan further this vision and intent while accommodating the unique nature of the proposed uses and development. In order to provide an efficient comparison between the provisions of this text and the portions of the Zoning Code which otherwise apply to the ID-1 district, this text generally follows the format of Sections 153.038 through 153.040 of the Zoning Code. Any reference to “§” as used herein means a “section” of the Dublin Codified Ordinances.

## **II. DISTRICT USES.**

(A) Uses in this Zoning District are shown in the following table. Descriptions and characteristics of use categories which are not defined in this text can be found in Zoning Code Section § 153.002(A), Uses Definitions. Additional development requirements for particular uses are contained in division (C) of this section as referenced below within the “Use Specific Standards” column.

(1) *Permitted uses*. Uses denoted as “P” in the Table of Uses below indicate that the use is allowed by right, subject to compliance with the use specific requirements referenced in the Table of Uses and all other applicable provisions of this text and the Zoning Code, as applicable.

(2) *Conditional uses*. Uses denoted as “C” in the Table of Uses below indicate that the land use is allowed only upon approval of a conditional use permit as required by Zoning

Code § 153.236, compliance with use specific requirements referenced in the Table of Uses and all other applicable provisions of the Zoning Code.

(3) *Size or time limited uses.* Uses denoted with an “S” or “T” in the Table of Uses below indicate special limits regarding size or time duration of the use and are subject to compliance with the use specific requirements referenced in the Table of Uses and all other applicable provisions of the Zoning Code.

(4) *Table of uses.* The following table identifies principal permitted and conditional uses as well as uses which are permitted as being accessory or ancillary to a permitted or conditional use within this Zoning District. An “X” indicates that a particular use is not permitted.

**Table of Uses**

**PRINCIPAL PERMITTED USES**

|                                                                                     |                           |                                      |
|-------------------------------------------------------------------------------------|---------------------------|--------------------------------------|
| P=Permitted C=Conditional S=Size Limited T=Time Limited X= Prohibited/Not Permitted |                           |                                      |
| <b><u>PRINCIPAL USES</u></b>                                                        | <b><i>Designation</i></b> | <b><i>USE SPECIFIC STANDARDS</i></b> |
| <b>Commercial Uses</b>                                                              | <b>ID-1</b>               |                                      |
| Animal Care- General Services                                                       | X                         |                                      |
| Animal Care- Veterinary Offices                                                     | X                         |                                      |
| Animal Care- Veterinary Urgent Care and Hospitals                                   | X                         |                                      |
| Conference Center                                                                   | X                         |                                      |
| Data Center                                                                         | X                         |                                      |
| Eating and Drinking                                                                 | X                         |                                      |
| Entertainment and Recreation- Indoor                                                | X                         |                                      |
| Entertainment and Recreation- Outdoor                                               | X                         |                                      |
| Exercise and Fitness                                                                | X                         |                                      |

|                         |   |  |
|-------------------------|---|--|
| Fueling/Service Station | X |  |
| Hotel                   | X |  |
| Office- General         | P |  |

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|                                                                                |                    |                                                                                                                                               |
|--------------------------------------------------------------------------------|--------------------|-----------------------------------------------------------------------------------------------------------------------------------------------|
| P=Permitted C=Conditional S=Size Limited T=Time Limited                        |                    |                                                                                                                                               |
| <b><u>PRINCIPAL</u></b><br><b>USES</b>                                         | <b>Designation</b> | <b>USE SPECIFIC STANDARDS</b>                                                                                                                 |
| <b>Commercial Uses (Cont.)</b>                                                 |                    |                                                                                                                                               |
| Office- Call Centers                                                           | X                  |                                                                                                                                               |
| Office- Flex                                                                   | X                  |                                                                                                                                               |
| Office- Medical                                                                | P                  |                                                                                                                                               |
| Parking Structures                                                             | P                  | 153.038(C)(7)                                                                                                                                 |
| Personal, Repair and Rental Services                                           | X                  |                                                                                                                                               |
| Retail                                                                         | X                  |                                                                                                                                               |
| Transportation- Park-and-Ride                                                  | P                  | 153.038(C)(9)                                                                                                                                 |
| <b>Civic, Public &amp; Institutional Uses</b>                                  |                    |                                                                                                                                               |
| Ambulatory care facilities (as such term is defined in Section I of this text) | P                  |                                                                                                                                               |
| Day Care                                                                       | X                  |                                                                                                                                               |
| Educational Facility                                                           | P                  | 153.038(C)(11), except that such programs and/or uses shall have a defined mission relating to post-secondary education, community education, |

|                                        |   |                 |
|----------------------------------------|---|-----------------|
|                                        |   | and/or medicine |
| High School                            | X |                 |
| Government Services- General           | X |                 |
| Government Services- Safety            | X |                 |
| Government Services- Service           | X |                 |
| Hospital                               | P |                 |
| Library, Museum or Gallery             | X |                 |
| Religious or Public Assembly           | X |                 |
| Parks and Open Space                   | P |                 |
| Transportation - Transit Station       | P |                 |
| <b>Industrial Uses</b>                 |   |                 |
| Construction and Contract Service      | X |                 |
| Manufacturing and Assembly             | X |                 |
| Mini-Storage                           | X |                 |
| Research and Development               | P |                 |
| Utilities- District Energy Plant       | X |                 |
| Utilities- Renewable Energy Facilities | P | 153.038(C)(17)  |

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|                                    |   |  |
|------------------------------------|---|--|
| <b>Industrial Uses (Cont.)</b>     |   |  |
| Utilities- Electric Substation     | X |  |
| Utilities- Essential Services      | P |  |
| Utilities- Wireless Communications | X |  |



|                              |   |  |
|------------------------------|---|--|
| Warehousing and Distribution | X |  |
| <b>Residential Uses</b>      |   |  |
| Dwelling- Single-Family      | X |  |
| Dwelling- Townhome           | X |  |
| Dwelling- Live-Work          | X |  |
| Dwelling- Multiple Family    | X |  |

**ACCESSORY USES**

|                                                                                    |                           |                                                                                                                                               |
|------------------------------------------------------------------------------------|---------------------------|-----------------------------------------------------------------------------------------------------------------------------------------------|
| P=Permitted C=Conditional S=Size Limited T=Time Limited X=Prohibited/Not Permitted |                           |                                                                                                                                               |
| <b><u>ACCESSORY USES</u></b>                                                       | <b><u>Designation</u></b> | <b><u>USE SPECIFIC STANDARDS</u></b>                                                                                                          |
| <b>Commercial Uses</b>                                                             |                           |                                                                                                                                               |
| Bicycle Facilities                                                                 | P                         | 153.038(C)(23), except that there shall be no requirement for locating bicycle parking spaces within a certain distance of building entrances |
| Child Day Care                                                                     | P                         | The second sentence of 153.038(C)(10) shall apply to this use, the second sentence of this same section shall not.                            |
| Community Activity                                                                 | P,T                       | 153.038(C)(24)                                                                                                                                |
| Construction Trailer/Office                                                        | P,T                       | 153.038(C)(25)                                                                                                                                |
| Corporate Residences                                                               | X                         |                                                                                                                                               |
| Drive-In/Drive-Thru                                                                | X                         |                                                                                                                                               |

|                                      |   |                                                                                                                                                                                                                            |
|--------------------------------------|---|----------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|
| Dwelling Rental or Sales Office      | X |                                                                                                                                                                                                                            |
| Eating and Drinking                  | P | Not permitted as a stand-alone use. This use shall only be permitted within a building containing a principal permitted use.                                                                                               |
| Entertainment and Recreation- Indoor | X |                                                                                                                                                                                                                            |
| Exercise and Fitness                 | P | Not permitted as a stand-alone use. This use shall only be permitted within a building containing a principal permitted use. This use shall not exceed 10% of the gross floor area of the building from which it operates. |

|                                      |      |                                                                                                                                                                                                                            |
|--------------------------------------|------|----------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|
| <b>Commercial Uses (Cont.)</b>       |      |                                                                                                                                                                                                                            |
| Helipad/Heliport                     | P    | 153.038(C)(28)                                                                                                                                                                                                             |
| Home Occupation                      | X    |                                                                                                                                                                                                                            |
| Outdoor Display/Seasonal Sales       | X    |                                                                                                                                                                                                                            |
| Parking Structures                   | P    | 153.038(C)(7)                                                                                                                                                                                                              |
| Personal, Repair and Rental Services | X    |                                                                                                                                                                                                                            |
| Retail                               | P, S | Not permitted as a stand-alone use. This use shall only be permitted within a building containing a principal permitted use. This use shall not exceed 10% of the gross floor area of the building from which it operates. |
| Transportation - Park and Ride       | P    | 153.038(C)(9)                                                                                                                                                                                                              |
| Transportation -                     | P    |                                                                                                                                                                                                                            |

|                                      |                            |                |
|--------------------------------------|----------------------------|----------------|
| Transit Stop                         |                            |                |
| Utilities - Essential Services       | P                          |                |
| Utilities - Renewable Energy         | P                          | 153.038(C)(31) |
| Utilities - Renewable Wind Equipment | C                          | 153.038(C)(31) |
| Utilities - Wireless Communications  | See Dublin Code Chapter 99 |                |
| Vehicle Charging Stations            | P                          |                |
| Warehousing and Distribution         | X                          |                |

(B) Similar uses or uses not addressed.

(1) In those situations where a use is not specifically addressed but could reasonably be interpreted as similar in character to a use listed in this Zoning District, the Director may determine that the use is similar to the uses in the Zoning District, as a permitted or accessory use.

(2) The Director shall base the decision on a finding that the proposed use satisfies all of the following:

- (a) Is consistent with the intent of this section and with the development intent for this Zoning District.
- (b) Will not impair the present or potential use of other properties within the Zoning District in the vicinity.
- (c) The operation, scale and characteristics of the proposed use are no greater than those listed in this text, in terms of aesthetics, traffic generated, noise, potential nuisances, and other impacts related to community health, safety and welfare.

- (d) Will not adversely affect the Economic Advancement Zone elements of the Community Plan.
- (3) Once a finding of similar use is made, it shall comply with all applicable Code provisions, review and approval requirements, and Zoning District regulations that apply to that use.
- (4) The Director's determination shall be in writing and sent to the applicant. The applicant may either appeal the decision to the Board of Zoning Appeals in accordance with the provisions of the Zoning Code, or submit an application for an amendment to this Zoning District.

### III. DEVELOPMENT STANDARDS.

(A) Lot requirements. The following requirements for arrangement and development of land and buildings shall be met in this Zoning District.

(1) Intensity of use. Lot size shall be sufficient to provide the yard spaces required by the following provisions:

(a) Minimum lot size. The minimum size of lots within this Zoning District shall be 3 acres.

(b) Maximum lot coverage. Lot coverage for structures and impervious surfaces shall not exceed 75%.

(c) Building height. The maximum building height for the occupiable above-grade portions of buildings shall not exceed 110 feet and shall not exceed 7 stories in height. Mechanical equipment which is screened in accordance with the requirements of this text shall be permitted to be installed on buildings to a height necessary to accommodate the equipment and screening. Where such mechanical equipment is fully enclosed (i.e., so that such enclosure has a roof and is accessible using one or more doors that are accessible while standing on the roof), the enclosure and mechanical equipment shall count as a building story.

(2) Setback requirements. Placement of structures and improvements shall provide sufficient separation to the adjacent site or use according to the following provisions. Setbacks shall be provided as necessary to accommodate any additional site requirements such as landscaping, mounding and buffering.

(a) Lot width. Lots shall be a minimum of 60 feet in width at the public right-of-way.

(b) Front yards. The required minimum setbacks for principal and accessory structures shall be as follows:

(i) U.S. Route 33: The minimum required setback for pavement and buildings shall be 100 feet from the right-of-way for U.S. Route 33 as it exists on the date that this text becomes legally effective. Ground-mounted mechanical equipment which is completely screened from off-site view by a wall or fencing shall be permitted to encroach up to 25 feet into the minimum required setback, in addition to grass pavers that provide a route of vehicular access to serve such equipment. In addition, walking paths, benches, gardens, and other landscaping shall be permitted within the minimum required setback from U.S. Route 33. No parking shall be permitted to be constructed between U.S. Route 33 and any front plane of the primary building in this Zoning District which runs generally parallel to U.S. Route 33.

(ii) Relocated Shier-Rings Road: The minimum required setbacks from the right-of-way of Relocated Shier-Rings Road shall be 25 feet for pavement and 100 feet for buildings.

(iii) Eastern and Western Perimeter Boundaries: The minimum required setback for pavement shall be 15 feet and the minimum required setback for buildings shall be 50 feet from each of the eastern and western boundary lines of this Zoning District.

(iv) Internal Parcel Lines: There shall be a zero setback requirement from all parcel lines which are located internally within this Zoning District.

(v) Elimination of Setbacks: In the event that a parcel located within this Zoning District and an adjacent parcel located outside of this Zoning District to the east or west (a) come under common ownership or control and (b) are zoned to allow compatible uses, then any minimum pavement and building setbacks set forth in this text which apply to the eastern or western perimeter boundaries of this Zoning District (as applicable) shall no longer apply with respect to these parcels.

(vi) Minimum Building Separation: There shall be no minimum required separation distance between buildings. It is anticipated that the second phase of building construction in this Zoning District may consist of an expansion of the building that is constructed during the first phase of development therein.

(B) Additional outdoor requirements. The following requirements for outdoor storage, operations and service areas shall be met in this Zoning District:

(1) Outdoor operations. Uses shall operate entirely within an enclosed structure, unless it is determined by the Director that proposed outdoor operations are harmonious to the surrounding area and are adequately screened.

(2) Outdoor storage. Exterior storage shall not be permitted in this Zoning District.

(3) Off-site impacts. Uses must be conducted in a manner harmonious with the surrounding area and comply with minimum requirements as set forth by the provisions of § 153.076 of the Dublin Codified Ordinances.

(4) Service areas. Overhead doors shall be located to the side or rear of structures to minimize visibility from U.S. Route 33. Open service areas and loading docks shall be screened by walls a minimum of 6 feet in height, but not greater than 12 feet. Walls, fences or landscape screening shall have 100% opacity to effectively conceal service and loading operations from an adjoining public right-of-way and from any residential zoning district or a residential Zoning District of a planned development district as listed in § 153.016. Compliance with the provisions of § 153.133(C) shall also be required.

(5) All storage areas and structures for outdoor mechanical equipment must be screened so as not to be visible from the street and from pedestrian circulation areas, or recessed within the footprint of the building.

(C) Architectural requirements.

(1) Building Design:

(a) Intent and Character: The ambulatory care facility is part of a larger initiative for the Med Center and therefore architecture for the building from which it will operate is tied closely to the Med Center's branding. The University's design vision for buildings in the locations where it is constructing outside of the University's main campus requires similarities from facility-to-facility in order for patients to identify the branding of this type of use for the Med Center. Moreover, the services and operations within these facilities drive the buildings' footprints, and causes the need for an "inside-out" design process. The Med Center will continue to program the services and operation of the facility throughout the City's review of the preliminary development plan. This may result in the Med Center requesting some modifications to architecture at the time it files a final development plan application.

In addition, it is anticipated that the buildings which are approved as part of the preliminary development plan application will be constructed in two or more phases. The applicant may elect and shall be permitted to file a final development plan application which pertains to only the initial phase of construction and development. To the extent that such final development plan illustrates the locations and sizes of later phases of development for purposes of

context, then the developer and/or the City may stipulate this point and in such a circumstance the approval of the first phase shall not be deemed to be approval of any later phase. In this circumstance later phases of the building shall be required to be reviewed and approved as part of a separate final development plan application when the applicant is prepared to move ahead with any such phase.

(b) Design Intent: The Med Center has a proven track record of designing high-quality buildings with attractive aesthetic designs and which are efficient in terms of minimizing their environmental impacts and serving patients. The design of the ambulatory care facility and buildings with other permitted uses will achieve these same goals. Buildings shall be designed to meet or exceed the City's standards in terms of quality of materials and design and shall recognize this facility as a focal point in the community.

Medical buildings are necessarily large and typically include long walls that together form spaces for offices, exam rooms, laboratories, and other related uses. The goal for the development of the structures in this Zoning District is to balance the practical needs of these buildings with the desire to provide exterior designs that are attractive. Architecture by its nature is a subjective medium, meaning that the adoption of strict objective standards in all instances may not provide the best means for achieving appropriate design. In recognition of this fact, the standards set forth herein provide requirements for designing buildings in an effort to set expectations for the quality of architecture that will be expected for these structures. On the other hand, these standards are meant to allow for some flexibility to encourage innovative design, provided that the spirit and intent of these provisions are met.

(2) Design Requirements:

(a) Level of Façade Finish: Buildings shall be required to employ a comparable use of materials on all elevations. All elevations of a building shall receive similar treatment in terms of style, materials, and design so that no elevation is of a lesser visual character than any other.

(b) Quality: Architectural design for all portions of a building or structure that are visible from a public right-of-way or adjacent property shall meet the community standard in terms of quality while taking into account the unique nature of the use(s) that will be found therein.

(c) Long Façade Requirements: Uninterrupted blank wall facades shall be prohibited to the extent that they are visible from a public right-of-way or adjacent property. Design variations on long exterior walls shall be employed in order to create visual interest. Examples of such design variations include, but are not limited to, the use of offsets, recesses and/or projections, banding, windows,

and/or reveals; scoring of building facades; color changes; texture or material changes; and variety in building height.

(d) Use of Design Elements: The use of one or more architectural or design elements may be used to soften the aesthetics of the building, such as but not limited to canopies, porticos, overhangs, arches, outdoor patios, community spaces, or similar devices.

(e) Use of Landscaping: Landscaping shall be utilized where appropriate to enhance the aesthetics of the building and to lessen its visual impact when viewed from public rights-of-way.

(f) Primary Building Entrances: Primary entrances to buildings shall be made sufficiently prominent that they can be easily identified from a distance.

(g) Screening of Building Elements: Elements such as meter boxes, utility conduits, roof and wall projections such as vent and exhaust pipes, mechanical equipment, and trash containers and dumpsters shall be designed, located, or screened in accordance with City Code.

(h) Roofs: All roof types shall be permitted (including, without limitation, flat roofs), provided that each roof design shall be compatible with the style and design of the building.

(3) Materials:

(a) Exterior wall finishes: Permitted primary building materials shall include brick, brick veneer, stone, stone veneer, manufactured stone, cast stone, architectural precast concrete, metal, wood (including synthetic wood products), and/or glass (except that reflective or mirrored glass shall be prohibited).

(b) Prohibited Materials: Prefabricated metal buildings and untreated masonry block structures are prohibited.

(4) Phasing: The east-facing façade of the building to be constructed as part of the first phase of development with this Zoning District will serve as an interior building wall once a building addition is constructed as part of the second phase of development. In recognition of this fact, the east-facing façade of the building constructed during the first phase may have a different level of finish than the remainder of that building. The University shall present details of this interim façade condition for review and approval as part of a final development plan.



#### IV. SITE DEVELOPMENT REQUIREMENTS.

The following requirements for site development shall be met in this Zoning District.

(A) Fences. Fences within this Zoning District will comply with §§ 153.078 to 153.083.

(B) Landscape requirements. Landscaping in this Zoning District shall be provided in accordance with this text and the preliminary development plan, as may be modified as part of a final development plan. All other applicable requirements of §§ 153.134 to 153.148 shall be met, except that variances as contemplated in §153.138(B) shall be heard by the Planning Commission.

(1) Intent. Landscaping should emphasize a naturalized appearance and blend with the design of public rights-of-way, while demarking areas of more formal design where people will gather and use space.

(2) Character elements. Site landscaping shall comply with the general character and design components set forth in the preliminary development plan, with final details for landscaping to be provided in the final development plan.

(3) Property perimeter and parking lot buffering. The property perimeter and parking lot buffering requirements in this Zoning District are illustrated in plans which accompany this text. Requirements in this regard within this Zoning District shall be substantially consistent with these plans, as approved as part of a final development plan. The provisions of this paragraph and the referenced plans shall apply to the exclusion of relevant provisions of the Zoning Code.

(4) Use of mounding. Integration of mounding as one form of screening should focus on landforms of varying width and height that will achieve a more natural and less "engineered" appearance.

(5) Interior landscape requirements. All landscape islands must include a minimum width of ten feet from back to back of curb. All islands must be a minimum of 180 square feet in area. Parking lot islands shall be designed in a manner so as to be generally distributed evenly throughout paved parking areas without being required in the interior of service courts and loading dock areas. Pensinsulas may be used to meet and shall be counted toward the minimum interior landscaping requirements for this Zoning District. Bio-retention measures, where appropriate within parking lot areas, shall be encouraged in order to aid stormwater management and provided irrigation for trees.

(6) Street trees. One, two-inch caliper street tree shall be provided for each 35-foot interval of public or private street frontage and along entry driveways exceeding 200 feet in length. To achieve the character of the EAZ, trees shall be planted in informal clusters along public arterials and collectors as defined in the EAZ Plan. Entry drives and local streets may include more uniform spacing. Street trees along Relocated Shier-Rings

Road/University Boulevard shall be installed by the City as part of its construction thereof using specifications, species, and standards as determined by the City.

(7) Open space plantings. One deciduous tree must be planted for each 2,000 square feet of open space (areas not part of lot coverage). Trees must have a minimum caliper of two inches at the time of planting. Groupings or clusters of trees are preferred, planted at intervals necessary to ensure their survival.

(8) Parking lot plantings. To help reduce excessive heat build-up and emissions from large areas of hard surfacing, landscape areas must be provided within parking lots designed for a capacity of 40 cars or more. Landscape islands with a surface area equal to 5% of the area of the paved surface (including all parking spaces, interior drives, loading docks, drop-off/pick-up lanes, and access drives beyond the right-of-way) shall be provided in all parking lots. The required landscape areas shall meet the following requirements:

(a) All required landscaped areas shall consist of curbed islands (such curbs shall not be required in areas where bio-retention measures are provided) or peninsulas that are surrounded on at least two sides by pavement. Landscaping on the perimeter of the parking lot shall not be counted toward meeting this requirement.

(b) A minimum of one broad leaf/deciduous tree shall be provided for every 300 square feet of landscaped area required. These trees shall be a minimum of 1.75 inches in caliper.

(c) The landscape areas may be located in "dead corners" or in landscape islands so as to break up expanses of parking spaces and to contribute to orderly circulation of pedestrian or vehicular traffic in the parking area.

(C) Tree preservation. Sites shall also comply with the procedures for §§ 153.140 to 153.149. Notwithstanding the foregoing, trees installed as part of the screening of the eastern faced of the building within the first phase of development in this Zoning District which are later removed to accommodate the second building phase shall not be subject to the requirements of these sections. Reasonable efforts shall be made by the developer in the final development plan for the first phase of development in this Zoning District to locate trees in a manner that reduces the need to remove them with any later phase of development. Removal of trees within the right-of-way of U.S. Route 33 shall be permitted with the prior approval of the Ohio Department of Transportation.

(D) Open space and greenways.

(1) Outdoor plazas, squares or greens, and/or public art shall be provided in this Zoning District. Reasonable efforts shall be made to provide these amenities in various locations in order to provide visitors, employees, and patients with various opportunities to sit,

walk, and congregate outdoors. Character images for such amenities accompany this text.

(2) Open space areas shall be large enough and of proper dimensions so as to constitute a useable area, with adequate access, through easements or other similar arrangements.

(3) The inclusion of the following amenities are permitted and encouraged within an open space where appropriate to the principal use: public art, sidewalk cafes, food kiosks, ornamental fountains, drinking fountains, waterfalls, sculpture, arbors or trellises, planted beds, clock pedestals, transit shelters, awnings (not including canvas materials), canopies and similar structures.

(4) Any permanent open space developed in conjunction with a building or site must be equipped for congregation and recreational opportunities by users of the building and surrounding areas.

(E) *Utilities.* All utility lines including water supply, sanitary sewer service, electricity, telephone and gas, and their connections or feeder lines shall be placed underground. Unless otherwise dictated by the needs of the building, all utility connections shall be kept to the rear or the side of the building, out of view or screened.

(F) *Parking requirements.*

(1) Parking shall comply with §§ 153.200 to 153.212, unless specifically provided for herein.

(2) No parking shall be permitted between the front of a building and U.S. Route 33. All parking shall be located to the side or rear of a building. Loading areas for buildings shall be located on the side or rear of a building (with the rear of the building to be considered to be the building facade that faces southward), provided that the screening requirements of this text are met.

(3) Sidewalks shall be provided on-site to link parking to public sidewalks and to the entrances of the buildings.

(4) Parking and maneuvering areas shall meet the minimum requirements of the parking space dimensions set forth in §153.040(F)(5).

(5) Parking in this Zoning District shall be provided at the minimum rate of 2.25 spaces per 1,000 square feet of building area.

(6) The requirements of §153.204 shall not apply to this Zoning District.

(G) *Bicycle parking.*

(1) One bicycle parking space shall be required for every 10,000 square feet of building floor area constructed, or fraction thereof.

(2) Bicycle racks shall be installed according to the dimensional requirements set forth by the bike rack manufacturer and the latest edition of the APBP Bicycle Parking Guidelines, or similarly acceptable industry publication.

(H) Loading areas.

(1) Location. Loading/unloading areas and service areas shall be permitted in the locations shown in the preliminary development plan.

(2) All service vehicles must be maintained within appropriately screened areas and shall not be placed forward of the structure.

(I) Parking structures. If provided, parking structures shall meet the requirements of §153.040(J).

(J) Signs. Signs shall comply with §§ 153.150 through 153.164, unless specifically provided for in the following section. In the case of conflicts, the provisions of this text will prevail.

(1) Intent. Signage needs for the types of facilities that are to be developed in this Zoning District are unique and require flexibility, not in an effort to deviate from the community standard but instead to properly and adequately identify uses, users and tenants, and to promote efficient wayfinding. Character images of anticipated types of wall signage are set forth below for the limited purpose of setting expectations for these signs ahead of specific signage that will be provided for approval as part of a final development plan. In addition, a conceptual sign package is being provided as part of the preliminary development plan. A master sign plan shall be filed as part of a final development plan for review and approval by the Planning Commission.

(2) Types. The following signs shall be permitted in this Zoning District:

(a) Building Identification Signs. Four “Building Identification Signs” (see accompanying conceptual sign plan, Sign Type #5) shall be permitted. The maximum permitted dimensions for these signs are illustrated in the accompanying sign plan. The final locations and dimensions of these signs (with such dimensions not to exceed those shown in the accompanying sign plan) shall be provided by the applicant in a final development plan. One such sign shall be located on the west-facing elevation of the medical office building that is to be constructed as part of the first phase of development in this Zoning District, another shall be located on the east-facing elevation of the same building, and the third and fourth shall be located on the south-facing and north-facing elevations of the ambulatory care facility that is to be constructed during the first phase of development. The purpose of each of the signs shall be to identify the University and/or the Med Center and shall include graphic text and a logo. Each sign shall

be mounted to the “penthouse” on each building, which is intended to consist of a fully enclosed mechanical screening area with access provided from the roof by at least one pedestrian door. The penthouse for the medical office building consist of the sixth floor of that building, the penthouse for the ambulatory care facility consist of the third floor of that building, and the penthouse for the hospital shall consist of the seventh floor of that building. At such time as the second phase of building is constructed in this Zoning District, the east-facing sign located on the penthouse of the medical office building shall be relocated to the east-facing façade of the penthouse on the portion of the building constructed as part of the second phase.

(b) Ancillary Wall Signs. Additional wall signs (“Ancillary Wall Signs”) shall be permitted along the southern facades of the first and second building phases in order to identify particular uses within the building, to provide addresses for such uses, and to promote efficient wayfinding into the building(s). The permissible maximum dimensions of this type of sign shall be established in a final development plan along with general requirements for locations, and it is the intent that once they are established the applicant may relocate, remove, or replace these signs in accordance with the approved dimensions and requirements with administrative approval. In addition, building-mounted street address numbers may be located on the building in locations approved as part of a final development plan (see accompanying conceptual sign plan, Sign Type #4).

(c) Window Signs. Window signs shall be permitted in the same manner and with same requirements as would apply to this site if it were located in the ID-1 zoning district.

(d) Primary Entry Monument Sign. One “Primary Entry Monument Sign” (see accompanying conceptual sign plan, Sign Type #1) shall be permitted to be located on the east or west side of the primary central entry drive from Relocated Shier-Rings Road/University Boulevard. The maximum permitted dimensions for this sign type are illustrated in the accompanying sign plan. The final location and dimensions of this sign (with such dimensions not to exceed those shown in the accompanying sign plan) shall be provided by the applicant in a final development plan.

(e) Secondary Entry Signs. Four “Secondary Entry Signs” (see accompanying conceptual sign plan, Sign Type #2) shall be permitted to be located within this Zoning District. These shall be monument signs. One such sign shall be permitted on the east or west side of the westernmost access drive into the Zoning

District from Relocated Shier-Rings Road/University Boulevard, and the another such sign shall be permitted on the east or west side of the easternmost access drive into the Zoning District from Relocated Shier-Rings Road/University Boulevard. One Secondary Entry Sign shall be permitted on the east and west boundary lines of the Zoning District where vehicular cross access is provided to adjacent properties. The maximum permitted dimensions for these signs are illustrated in the accompanying sign plan. The final locations and dimensions of these signs (with such dimensions not to exceed those shown in the accompanying sign plan) shall be provided by the applicant in a final development plan.

(f) *Directional Signs*. “Directional Signs” shall be permitted internally within this Zoning District for purposes of directing vehicular and pedestrian traffic within the property to the different uses contained therein. The permissible maximum dimensions of this type of sign shall be established in a final development plan along with general requirements for locations, and it is the intent that once they are established the applicant may relocate, remove, or replace these signs in accordance with the approved dimensions and requirements by obtaining administrative approval. An example of one type of directional signage is shown as a “Tall Vehicular Directional Sign” in the accompanying conceptual sign plan, Sign Type #3.

(g) *Regulated Signage*. In addition to the signage permitted above, the uses in this Zoning District may be required to install and maintain certain types of signage with mandated specifications pursuant to federal and/or state laws and other regulations. Such signage shall be permitted in accordance with such laws and regulations but shall not be required to be reviewed and approved as part of a final development plan. Regardless of whether or not such signs require a permit from the City, prior to installation the applicant shall provide written documentation detailing the locations, dimensions, and specifications for these signs to the City for informational purposes and record-keeping.

(i) *Other Signage*. Any signage that is permitted by the Dublin Zoning Code but is not addressed in this text shall be permitted in this Zoning District in accordance with Dublin City Code.

(3) *Multi-tenant signs*. No more than three tenant panels may be provided on one monument sign. Tenant panels must include a frame to visually separate the individual panels.

(4) *Secondary image*. Secondary images for signs may be designed up to 30% of the maximum permitted area of the sign face.

(5) Colors. Signs shall be limited to the use of four colors. Colors for trademarked logos shall be permitted; logos shall be considered as one of the four permitted colors.

(K) Site lighting. The lighting requirements of § 153.149 shall be met.

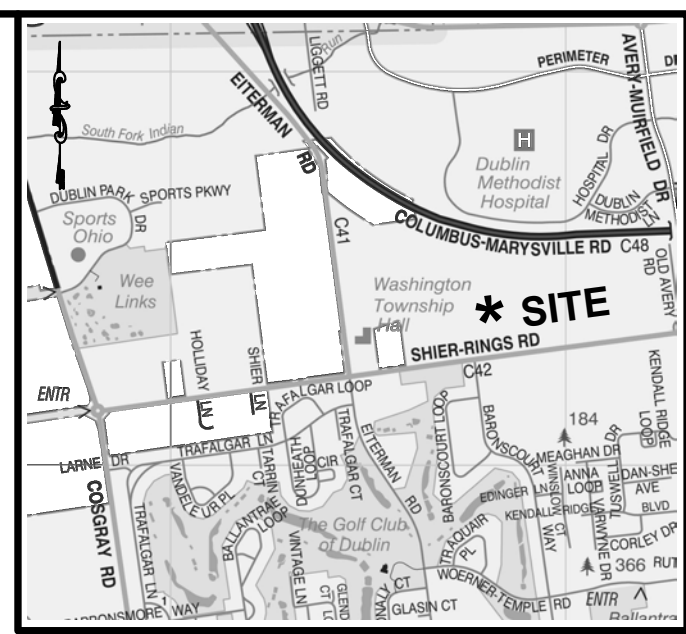
(L) Circulation and access.

(1) Right-of-way dedication. No right-of-way dedications shall be required as a result of development in this Zoning District.

(2) Site access. Vehicular access to and from the Zoning District shall be provided from three different access points along Relocated Shier-Rings Road with permissible turn movements shown on the preliminary development plan.

(3) Cross Access. Cross access for vehicular and pedestrian traffic shall be provided at the eastern and western boundaries of the Zoning District as generally shown in the preliminary development plan and as more particularly identified and approved in a final development plan, subject to a cross access easement being executed between the University and the City which also allows vehicular and pedestrian traffic to use adjacent parcels for purposes of ingress and egress. One secondary entry monument sign shall be permitted near each of the points where cross access drives intersection with the eastern and western boundary lines of this Zoning District if it meets the requirements of this text and is included in a final development plan.

Z:\18-0003-114\DWG\PRODUCTION DRAWINGS\PRE DEVELOPMENT PLAN\003-114\_PDP\_Site Plan.dwg Site Plan Aug 29, 2019 - 7:08:15am rflower



**VICINITY MAP**  
SCALE: NTS

**SITE STATISTICS & REQUIREMENTS**

**SOIL:** BROWN MOTTLED GRAY LEAN CLAY, HSG GROUP D  
BROWN CLAY-SILT GLACIAL TILL, HSG GROUP D

**ZONING DISTRICT:** ID-1: RESEARCH OFFICE DISTRICT  
**PROPOSED USE:** AMBULATORY CARE FACILITY  
**PROPOSED ZONING:** PUD

**TOTAL SITE AREA:** 33.873 AC.

**PARKING CALCULATIONS (PHASE 1):**  
HANDICAPPED SPACES: 25 SPACES  
SURFACE SPACES: 799  
TOTAL SPACES: 824

**PARKING CALCULATIONS (PHASE 2):**  
HANDICAPPED SPACES: 0 SPACES  
SURFACE SPACES: 408  
TOTAL SPACES: 408

**SETBACKS**  
U.S. ROUTE 33: 100' PAVEMENT & BUILDING  
RELOCATED SHIER-RINGS ROAD: 25' PAVEMENT / 100' BUILDING  
EASTERN PERIMETER BOUNDARY: 15' PAVEMENT / 50' BUILDING  
WESTERN PERIMETER BOUNDARY: 15' PAVEMENT / 50' BUILDING  
INTERNAL PARCEL: 0' PAVEMENT & BUILDING  
(EXCEPT AS NOTED FOR PRIVATE DRIVES)

**MIN. BUILDING SEPARATION:** 0'  
**MAX. BUILDING HEIGHT:** 110'

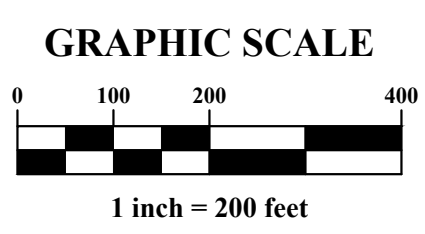
**SITE COVERAGE PROVIDED (PHASE 1):** 38.0%  
**STRUCTURE AREA:** 1,954 ACRES (85,117 S.F.)  
**SIDEWALK AREA:** 0.934 ACRES (40,686 S.F.)  
**PAVEMENT AREA:** 9,218 ACRES (401,537 S.F.)  
**TEMP. PAVEMENT AREA:** 0.760 ACRES (33,106 S.F.)

**ADDITIONAL SITE COVERAGE FOR PHASE 2 NOT TO EXCEED A TOTAL OF 75%**

**MAX. LOT COVERAGE:** 75%  
**TOTAL BUILDING AREA (PHASE 1):** 12,866 ACRES (560,443 S.F.)  
**TOTAL ALLOWABLE BUILDING AREA:** 25,405 ACRES (1,106,631 S.F.)

**INDEX OF DRAWINGS**

| SHEET TITLE          | SHEET NUMBER |
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| STORM SEWER PROFILES | 12           |
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| STORM SEWER PROFILES | 15           |



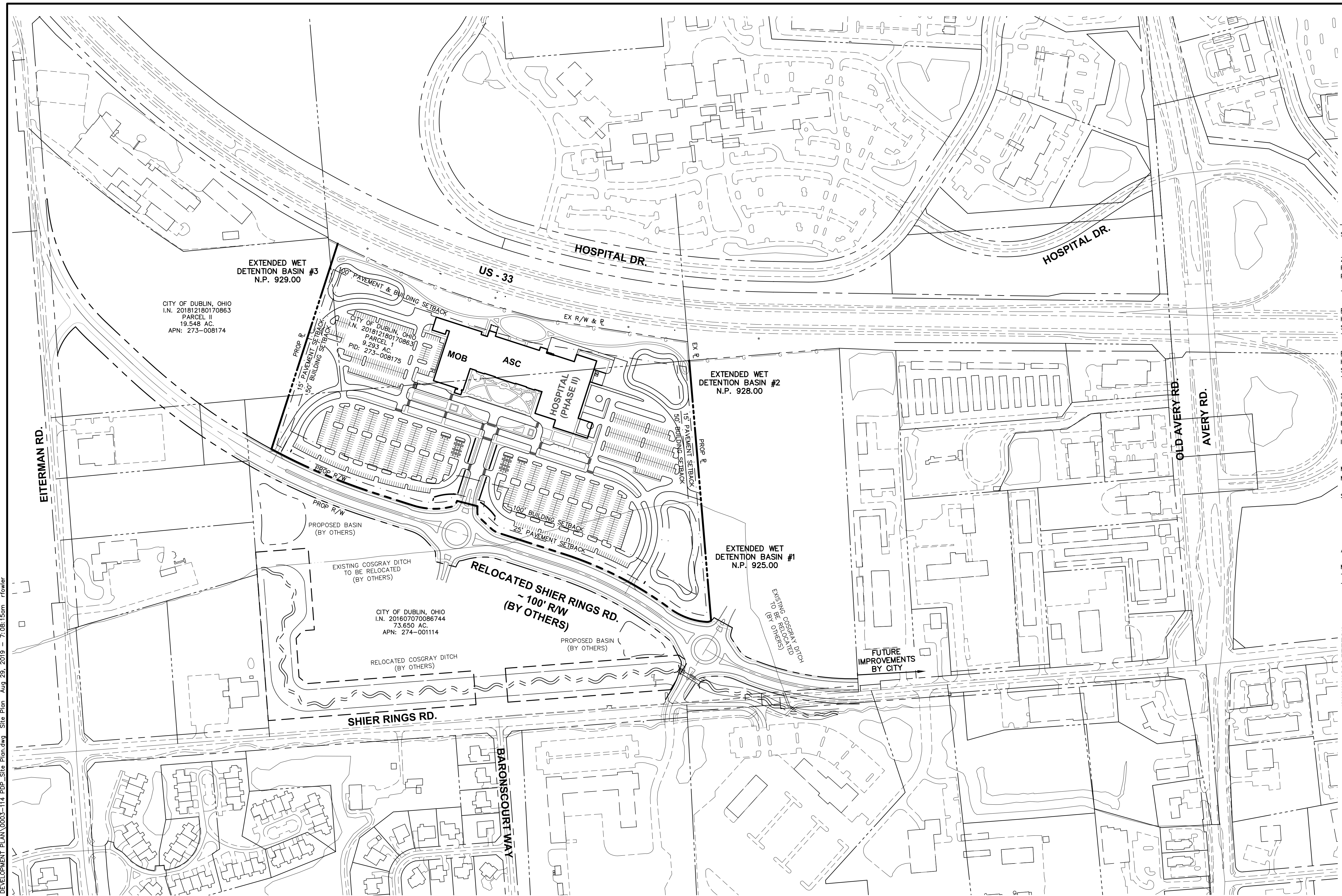
**CITY OF DUBLIN, OHIO  
PRELIMINARY DEVELOPMENT PLAN  
SITE PLAN  
FOR  
OSU MEDICAL CAMPUS PUD**

PLAN PREPARED BY:

**ADVANCED  
CIVIL DESIGN**  
ENGINEERS SURVEYORS

422 Beecher Road  
Gahanna, Ohio 43230  
ph 614.428.7750  
fax 614.428.7755

SCALE: 1" = 200'  
DATE: 08/29/2019 SHEET 1 / 15



EXTENDED WET  
DETENTION BASIN #3  
N.P. 929.00

CITY OF DUBLIN, OHIO  
I.N. 201812180170863  
PARCEL II  
19.548 AC.  
APN: 273-008174

CITY OF DUBLIN, OHIO  
I.N. 201812180170863  
PARCEL I  
9.293 AC.  
PID: 273-008175

EXTENDED WET  
DETENTION BASIN #2  
N.P. 928.00

EXTENDED WET  
DETENTION BASIN #1  
N.P. 925.00

CITY OF DUBLIN, OHIO  
I.N. 201607070086744  
73.650 AC.  
APN: 274-001114

FUTURE  
IMPROVEMENTS  
BY CITY



GENERAL NOTES

- CITY OF COLUMBUS AND OHIO DEPARTMENT OF TRANSPORTATION CONSTRUCTION AND MATERIAL SPECIFICATIONS, CURRENT EDITIONS, AND ANY SUPPLEMENTS THERETO (HEREAFTER REFERRED TO AS STANDARD SPECIFICATIONS), SHALL GOVERN ALL CONSTRUCTION ITEMS UNLESS OTHERWISE NOTED. IF A CONFLICT BETWEEN SPECIFICATIONS IS FOUND, THE MORE STRICT SPECIFICATION AS DECIDED BY THE CITY ENGINEER. ITEM NUMBERS LISTED REFER TO CITY OF COLUMBUS ITEM NUMBERS UNLESS OTHERWISE NOTED.
- THE CITY ENGINEER WILL NOT BE RESPONSIBLE FOR MEANS, METHODS, PROCEDURES, TECHNIQUES, OR SEQUENCES OF CONSTRUCTION THAT ARE NOT SPECIFIED HEREIN. THE CITY ENGINEER WILL NOT BE RESPONSIBLE FOR SAFETY ON THE WORK SITE, OR FOR FAILURE BY THE CONTRACTOR TO PERFORM WORK ACCORDING TO CONTRACT DOCUMENTS.
- THE DEVELOPER OR CONTRACTOR SHALL BE RESPONSIBLE TO OBTAIN ALL NECESSARY PERMITS INCLUDING BUT NOT LIMITED TO OHIO EPA PERMITS TO INSTALL (PTI) AND NOTICES OF INTENT (NOI), BUILDING PERMITS, ETC.
- THE CONTRACTOR SHALL NOTIFY THE CITY OF DUBLIN DIVISION OF ENGINEERING IN WRITING AT LEAST 3 WORKING DAYS PRIOR TO BEGINNING CONSTRUCTION.
- THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR COMPLYING WITH ALL FEDERAL, STATE AND LOCAL SAFETY REQUIREMENTS INCLUDING THE OCCUPATIONAL SAFETY AND HEALTH ACT OF 1970, THE CONTRACTOR SHALL EXERCISE PRECAUTION ALWAYS FOR THE PROTECTION OF PERSONS (INCLUDING EMPLOYEES) AND PROPERTY. IT SHALL ALSO BE THE SOLE RESPONSIBILITY OF THE CONTRACTOR TO INITIATE, MAINTAIN AND SUPERVISE ALL SAFETY REQUIREMENTS, PRECAUTIONS AND PROGRAMS IN CONNECTION WITH THE WORK, INCLUDING THE REQUIREMENTS FOR CONFINED SPACES PER 29 CFR 1910.146.
- FOLLOWING COMPLETION OF CONSTRUCTION OF THE SITE IMPROVEMENTS AND BEFORE REQUESTING OCCUPANCY, A PROOF SURVEY SHALL BE PROVIDED TO THE DIVISION OF ENGINEERING THAT DOCUMENTS "AS-BUILT" ELEVATIONS, DIMENSIONS, SLOPES AND ALIGNMENTS OF ALL ELEMENTS OF THIS PROJECT. THE PROOF SURVEY SHALL BE PREPARED, SIGNED AND SUBMITTED BY THE PROFESSIONAL ENGINEER WHO SEALED THE CONSTRUCTIONS DRAWINGS.
- THE CONTRACTOR SHALL RESTRICT CONSTRUCTION ACTIVITY TO PUBLIC RIGHT-OF-WAY AND AREAS DEFINED AS PERMANENT AND/OR TEMPORARY CONSTRUCTION EASEMENTS, UNLESS OTHERWISE AUTHORIZED BY THE CITY ENGINEER.
- THE CONTRACTOR SHALL CAREFULLY PRESERVE BENCHMARKS, PROPERTY CORNERS, REFERENCE POINTS, STAKES AND OTHER SURVEY REFERENCE MONUMENTS OR MARKERS. IN CASES OF WILLFUL OR CARELESS DESTRUCTION, THE CONTRACTOR SHALL BE RESPONSIBLE FOR RESTORATIONS. RESETTLE OF MARKERS SHALL BE PERFORMED BY AN OHIO PROFESSIONAL SURVEYOR AS APPROVED BY THE CITY ENGINEER.
- NON-RUBBER TIRED VEHICLES SHALL NOT BE MOVED ON OR ACROSS PUBLIC STREETS OR HIGHWAYS WITHOUT THE WRITTEN PERMISSION OF THE CITY ENGINEER.
- THE CONTRACTOR SHALL RESTORE ALL DISTURBED AREAS TO EQUAL OR BETTER CONDITION THAN EXISTED BEFORE CONSTRUCTION. DRAINAGE DITCHES OR WATERCOURSES THAT ARE DISTURBED BY CONSTRUCTION SHALL BE RESTORED TO THE GRADES AND CROSS-SECTIONS THAT EXISTED BEFORE CONSTRUCTION.
- TRACKING OR SPILLING MUD, DIRT OR DEBRIS UPON STREETS, RESIDENTIAL OR COMMERCIAL DRIVES, SIDEWALKS OR BIKE PATHS IS PROHIBITED ACCORDING TO SECTION 97.38 OF THE DUBLIN MUNICIPAL CODE. ANY SUCH OCCURRENCE SHALL BE CLEANED UP IMMEDIATELY BY THE CONTRACTOR AT NO COST TO THE CITY. IF THE CONTRACTOR FAILS TO REMOVE SAID MUD, DIRT, DEBRIS, OR SPILLAGE, THE CITY RESERVES THE RIGHT TO REMOVE THESE MATERIALS AND CLEAN AFFECTED AREAS, THE COST OF WHICH SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
- DISPOSAL OF EXCESS EXCAVATION WITHIN SPECIAL FLOOD HAZARD AREAS (100-YEAR FLOODPLAIN) IS NOT PERMITTED.
- ALL SIGNS, LANDSCAPING, STRUCTURES OR OTHER APPURTENANCES WITHIN RIGHT-OF-WAY DISTURBED OR DAMAGED DURING CONSTRUCTION SHALL BE REPLACED OR REPAIRED TO THE SATISFACTION OF THE CITY ENGINEER. THE COST OF THIS WORK SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
- ALL FIELD TILE BROKEN OR ENCOUNTERED DURING EXCAVATION SHALL BE REPLACED OR REPAIRED AND CONNECTED TO THE PUBLIC STORM SEWER SYSTEM AS DIRECTED BY THE CITY ENGINEER. THE COST OF THIS WORK SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
- ALL PRECAST CONCRETE PRODUCTS SHALL BE INSPECTED AT THE LOCATION OF MANUFACTURE. APPROVED PRECAST CONCRETE PRODUCTS WILL BE STAMPED OR HAVE SUCH IDENTIFICATION NOTING THAT INSPECTION HAS BEEN CONDUCTED BY THE CITY OF COLUMBUS. PRECAST CONCRETE PRODUCTS WITHOUT PROOF OF INSPECTION SHALL NOT BE APPROVED FOR INSTALLATION.
- BACKFILL WITHIN A 1:1 INFLUENCE LINE OF EXISTING STRUCTURES (HOUSES, GARAGES, ETC.) OR PUBLIC INFRASTRUCTURE (PAVEMENT, CURBS, SIDEWALKS, BIKE PATHS, ETC.) SHALL BE COMPACTED GRANULAR BACKFILL ACCORDING TO ITEM 912 OF THE STANDARD SPECIFICATIONS OR FLOWABLE GRADE TYPE II ACCORDING TO ITEM 613. ITEM 911 OF THE STANDARD SPECIFICATIONS SHALL BE USED ELSEWHERE.
- THE CONTRACTOR SHALL SUBMIT A COPY OF THE APPROVED CONSTRUCTION DRAWINGS AND A LIST OF PROPOSED PRECAST CONCRETE PRODUCT MANUFACTURERS TO THE CITY OF COLUMBUS CONSTRUCTION INSPECTION DIVISION BEFORE COMMENCING CONSTRUCTION.

SEND THE INFORMATION TO THE FOLLOWING ADDRESS:

CONSTRUCTION INSPECTION DIVISION  
CITY OF COLUMBUS  
1800 EAST 17TH AVENUE  
COLUMBUS, OHIO 43219

SEND A COPY OF THE TRANSMITTAL LETTER TO THE FOLLOWING ADDRESS:

DIVISION OF ENGINEERING  
CITY OF DUBLIN  
5800 SHIER RINGS ROAD  
DUBLIN, OHIO 43016

- ALL TRENCHES WITHIN PUBLIC RIGHT-OF-WAY SHALL BE BACKFILLED ACCORDING TO THE APPROVED CONSTRUCTION DRAWINGS OR SECURELY PLATED DURING NONWORKING HOURS. TRENCHES OUTSIDE THESE AREAS SHALL BE BACKFILLED OR SHALL BE PROTECTED BY APPROVED TEMPORARY FENCING OR BARRICADES DURING NONWORKING HOURS. CLEAN UP SHALL FOLLOW CLOSELY BEHIND THE TRENCHING OPERATION.
- ALL TREES WITHIN THE CONSTRUCTION AREA NOT SPECIFICALLY DESIGNATED FOR REMOVAL SHALL BE PRESERVED, WHETHER SHOWN OR NOT SHOWN ON THE APPROVED CONSTRUCTION DRAWINGS. TREES TO BE PRESERVED SHALL BE PROTECTED WITH HIGH VISIBILITY FENCING PLACED A MINIMUM 15 FEET FROM THE TREE TRUNK. TREES 6 INCHES OR GREATER AT DBH (DIAMETER BREAST HEIGHT) MUST BE PROTECTED WITH FENCING PLACED AT THE CRITICAL ROOT ZONE OR 15 FEET, WHICHEVER IS GREATER. TREES NOT INDICATED ON THE APPROVED CONSTRUCTION DRAWINGS FOR REMOVAL MAY NOT BE REMOVED WITHOUT PRIOR APPROVAL OF THE DIVISION OF ENGINEERING.
- CONDUIT MUST BE DIRECTIONALLY BORED ACROSS STREETS INSTEAD OF OPEN CUT, UNLESS SPECIFICALLY APPROVED BY THE CITY ENGINEER. PAVEMENT REPLACEMENT SHALL BE NOT PERMITTED. PERMITS TO CONSTRUCT IN THE RIGHT-OF-WAY OF EXISTING STREETS MUST BE OBTAINED FROM THE CITY OF DUBLIN DIVISION OF ENGINEERING BEFORE COMMENCING CONSTRUCTION. SHOULD OPEN CUTTING OF EXISTING PAVEMENT BE PERMITTED, CONTROLLED DENSITY BACKFILL (TYPE II) SHALL BE USED IN PLACE OF COMPACTED GRANULAR BACKFILL, ACCORDING TO ITEM 613 OF THE STANDARD SPECIFICATIONS.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE CONDITION OF TRENCHES WITHIN THE RIGHT-OF-WAY OF EXISTING STREETS AND PUBLIC EASEMENTS FOR A PERIOD OF ONE YEAR FROM THE FINAL ACCEPTANCE OF THE WORK, AND SHALL MAKE ANY NECESSARY REPAIRS AT NO COST TO THE CITY.
- PAVEMENTS SHALL BE CUT IN NEAT STRAIGHT LINES THE FULL DEPTH OF THE EXISTING PAVEMENT OR AS REQUIRED BY THE CITY ENGINEER. PAVEMENT REPLACEMENT SHALL BE CONDUCTED ACCORDING TO CITY OF COLUMBUS STANDARD DRAWING 1441 AND APPLICABLE CITY OF DUBLIN STANDARD DRAWINGS. THE REPLACEMENT OF DRIVEWAYS, HANDICAPPED RAMPS, SIDEWALKS, BIKE PATHS, PARKING LOT PAVEMENT, ETC. SHALL BE PROVIDED ACCORDING TO THE APPROVED CONSTRUCTION DRAWINGS AND CITY OF DUBLIN STANDARD CONSTRUCTION DRAWINGS.
- TREE TRIMMING WITHIN THE CONSTRUCTION ZONE IS TO BE COMPLETED BY A CERTIFIED ARBORIST AT THE COMPLETION OF THE PROJECT. THE ARBORIST IS TO RETURN AND TRIM ANY BROKEN BRANCHES AS NEEDED.
- ANY MODIFICATION TO THE WORK SHOWN ON THE DRAWINGS MUST HAVE PRIOR WRITTEN APPROVAL BY THE CITY ENGINEER, CITY OF DUBLIN.
- ALL INLETS SHALL BE CHANNELIZED.

- PARK AREAS SHALL BE FINE-GRADED AND SEEDED WITH THE FOLLOWING MIXTURE:  
IMPROVED KENTUCKY BLUEGRASS: 40% OF WEIGHT (2 VARIETIES IN EQUAL PARTS)  
IMPROVED PERENNIAL RYE: 60% OF WEIGHT (2 VARIETIES IN EQUAL PARTS)  
GERMINATION RATE: 85% APPLICATION RATE: 7 LBS PER 1000 SQ FT OR AS DIRECTED BY THE DIVISION OF PARKS AND RECREATION, CITY OF DUBLIN, OHIO.
- TRAFFIC CONTROL AND OTHER REGULATORY SIGNS SHALL BE TYPE S WITH A SQUARE POST ANCHOR BASE INSTALLATION AND MEET ALL REQUIREMENTS OF ODOT TC-41.20 AND APPLICABLE CITY OF DUBLIN SPECIFICATIONS.
- STREET SIGNS SHALL MEET ALL CITY OF DUBLIN SPECIFICATIONS WITH LETTERING COLORED IN WHITE DISPLAYED OVER A BROWN BACKGROUND. SIGN TUBING SHALL BE BROWN IN COLOR AND CONFORM WITH THE TYPE S, SQUARE POST ANCHOR BASE INSTALLATION REQUIREMENTS OF ODOT TC-41.20.
- UTILITIES
- THE FOLLOWING UTILITIES ARE KNOWN TO BE LOCATED WITHIN THE LIMITS OF THIS PROJECT:  
**GAS**  
COLUMBIA GAS OF OHIO, INC.  
1600 DUBLIN ROAD  
COLUMBUS, OHIO 43215  
(614) 481-1000  
**ELECTRIC**  
AMERICAN ELECTRIC POWER  
850 HUGH CENTER DRIVE  
GAHANNA, OHIO 43230  
(614) 883-6817  
**COMMUNICATION SPECTRUM**  
1015 OLENTANGY RIVER ROAD  
COLUMBUS, OHIO 43212  
(614) 481-5000  
**FRONTIER COMMUNICATIONS**  
(800) 982-8772  
**AT&T**  
111 N. 4TH STREET  
COLUMBUS, OHIO 43215  
(614) 223-5780  
**DUBLINK**  
CITY OF DUBLIN  
6555 SHIER-RINGS ROAD  
DUBLIN, OHIO 43016  
(614) 410-4750  
**SANITARY STORM WATER**  
CITY OF DUBLIN  
6555 SHIER-RINGS ROAD  
DUBLIN, OHIO 43016  
(614) 410-4750  
**CITY OF COLUMBUS**  
DIVISION OF POWER  
3368 INDIANOLA AVENUE  
COLUMBUS, OHIO 43214  
(614) 645-7360  
**CITY OF COLUMBUS**  
DIVISION OF SEWERS AND DRAINS  
1250 FAIRWOOD AVENUE  
COLUMBUS, OHIO 43206  
(614) 645-7102  
**CITY OF COLUMBUS**  
DIVISION OF WATER  
910 DUBLIN ROAD  
COLUMBUS, OHIO 43215  
(614) 645-8276
- THE CONTRACTOR SHALL GIVE NOTICE OF INTENT TO CONSTRUCT TO OHIO UTILITIES PROTECTION SERVICE (TELEPHONE NUMBER 800-362-2764), PRODUCER'S UNDERGROUND PROTECTION SERVICE (TELEPHONE NUMBER 614-587-0486), AND TO OWNERS OF UNDERGROUND UTILITIES THAT ARE NOT MEMBERS OF A REGISTERED UNDERGROUND PROTECTION SERVICE. NOTICE SHALL BE GIVEN AT LEAST 2 WORKING DAYS BEFORE START OF CONSTRUCTION.
- THE IDENTITY AND LOCATIONS OF EXISTING UNDERGROUND UTILITIES IN THE CONSTRUCTION AREA HAVE BEEN SHOWN ON THE APPROVED CONSTRUCTION DRAWINGS AS ACCURATELY AS PROVIDED BY THE OWNER OF THE UNDERGROUND UTILITY. THE CITY OF DUBLIN AND THE CITY ENGINEER ASSUMES NO RESPONSIBILITY FOR THE ACCURACY OF THE LOCATIONS OR DEPTHS OF UNDERGROUND UTILITIES SHOWN ON THE APPROVED CONSTRUCTION DRAWINGS. IF DAMAGE IS CAUSED, THE CONTRACTOR SHALL BE RESPONSIBLE FOR REPAIR OF THE SAME AND FOR ANY RESULTING CONTINGENT DAMAGE.
- LOCATION, SUPPORT, PROTECTION AND RESTORATION OF ALL EXISTING UTILITIES AND APPURTENANCES, WHETHER SHOWN OR NOT SHOWN ON THE APPROVED CONSTRUCTION DRAWINGS, SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
- WHEN UNKNOWN OR INCORRECTLY LOCATED UNDERGROUND UTILITIES ARE ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE OWNER AND THE CITY ENGINEER.
- PUBLIC STREET LIGHTING MAY BE IN THE VICINITY OF THIS PROJECT. CONTACT THE CITY OF DUBLIN, DIVISION OF ENGINEERING AT 410-4637, TWO DAYS PRIOR TO BEGINNING WORK.
- TRAFFIC CONTROL
- TRAFFIC CONTROL SHALL BE FURNISHED, ERECTED, MAINTAINED, AND REMOVED BY THE CONTRACTOR ACCORDING TO OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (OMUTCD), CURRENT EDITION.
- ALL TRAFFIC LANES OF PUBLIC ROADWAYS SHALL BE FULLY OPEN TO TRAFFIC FROM 7:00 AM TO 9:00 AM AND FROM 4:00 PM TO 8:00 PM UNLESS AUTHORIZED DIFFERENTLY BY THE CITY ENGINEER. AT ALL OTHER HOURS THE CONTRACTOR SHALL MAINTAIN MINIMUM ONE-LANE TWO-WAY TRAFFIC. UNIFORMED, OFF-DUTY POLICE OFFICERS SHALL REPLACE FLAGMEN DESIGNATED BY THE OMUTCD, AND SHALL BE PRESENT WHENEVER ONE-LANE, TWO-WAY TRAFFIC CONTROL IS IN EFFECT. POLICE CRUISERS MAY BE REQUIRED AS DIRECTED BY THE CITY ENGINEER.
- IF THE CITY ENGINEER DETERMINES THAT THE CONTRACTOR IS NOT PROVIDING PROPER PROVISIONS FOR TRAFFIC CONTROL, THE CITY ENGINEER SHALL ASSIGN UNIFORMED, OFF-DUTY POLICE OFFICERS TO THE PROJECT AT NO COST TO THE CITY.
- STEADY-BURNING, TYPE "C" LIGHTS SHALL BE REQUIRED ON ALL BARRICADES, DRUMS, AND SIMILAR TRAFFIC CONTROL DEVICES IN USE AT NIGHT.
- ACCESS FROM PUBLIC ROADWAYS TO ALL ADJOINING PROPERTIES FOR EXISTING RESIDENTS OR BUSINESSES SHALL BE MAINTAINED THROUGHOUT THE DURATION OF THE PROJECT FOR MAIL, PUBLIC WATER AND SANITARY SEWER SERVICE, AND EMERGENCY VEHICLES. THE CONTRACTOR SHALL PROVIDE A TRAFFIC CONTROL PLAN DETAILING THE PROPOSED MAINTENANCE OF TRAFFIC PROCEDURES. THE TRAFFIC CONTROL PLAN MUST INCORPORATE ANY TRAFFIC CONTROL DETAILS CONTAINED HEREIN. THE TRAFFIC CONTROL PLAN PROPOSED BY THE CONTRACTOR MUST BE APPROVED BY THE CITY ENGINEER PRIOR TO CONSTRUCTION.
- EROSION AND SEDIMENT CONTROL
- THE CONTRACTOR OR DEVELOPER IS RESPONSIBLE FOR SUBMITTING A NOTICE OF INTENT (NOI) TO BE REVIEWED AND APPROVED BY THE OHIO EPA. THE NOI MUST BE SUBMITTED TO EPA 45 DAYS PRIOR TO THE START OF CONSTRUCTION AND MAY ENTITILE COVERAGE UNDER THE OHIO EPA GENERAL PERMIT FOR STORMWATER DISCHARGES ASSOCIATED WITH CONSTRUCTION ACTIVITY. A PROJECT LOCATION MAP MUST BE SUBMITTED WITH THE NOI. A SEDIMENT AND EROSION CONTROL PLAN MUST BE SUBMITTED TO THE CITY ENGINEER FOR APPROVAL IF A SEDIMENT AND EROSION CONTROL PLAN HAS NOT ALREADY BEEN INCLUDED WITH THE APPROVED CONSTRUCTION DRAWINGS. THIS PLAN MUST BE MADE AVAILABLE AT THE PROJECT SITE AT ALL TIMES. THE DESIGN OF EROSION CONTROL SYSTEMS SHALL FOLLOW THE REQUIREMENTS OF OHIO EPA, ITEM 207 OF OHIO DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS, AND THE CITY ENGINEER. AN INDIVIDUAL NPDES STORMWATER DISCHARGE PERMIT MAY BE REQUIRED. THE CONTRACTOR SHALL BE CONSIDERED THE PERMITTEE.
- THE CONTRACTOR SHALL PROVIDE SEDIMENT CONTROL AT ALL POINTS WHERE STORM WATER RUNOFF LEAVES THE PROJECT, INCLUDING WATERWAYS, OVERLAND SHEET FLOW, AND STORM SEWERS.
- ACCEPTED METHODS OF PROVIDING EROSION/SEDIMENT CONTROL INCLUDE BUT ARE NOT LIMITED TO: SEDIMENT BASINS, SILT FILTER FENCE, AGGREGATE CHECK DAMS, AND TEMPORARY GROUND COVER. HAY OR STRAW BALES ARE NOT PERMITTED.

- THE CONTRACTOR SHALL PROVIDE ADEQUATE DRAINAGE OF THE WORK AREA AT ALL TIMES CONSISTENT WITH EROSION CONTROL PRACTICES.
- DISTURBED AREAS THAT WILL REMAIN UNWORKED FOR 30 DAYS OR MORE SHALL BE SEEDED OR PROTECTED WITHIN SEVEN CALENDAR DAYS OF THE DISTURBANCE. OTHER SEDIMENT CONTROLS THAT ARE INSTALLED SHALL BE MAINTAINED UNTIL VEGETATIVE GROWTH HAS BEEN ESTABLISHED. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE REMOVAL OF ALL TEMPORARY SEDIMENT DEVICES AT THE CONCLUSION OF CONSTRUCTION BUT NOT BEFORE GROWTH OF PERMANENT GROUND COVER.
- BLASTING (IF PERMITTED)
- THE CONTRACTOR MUST OBTAIN A BLASTING PERMIT FROM WASHINGTON TOWNSHIP FIRE DEPARTMENT PRIOR TO BLASTING FOR ROCK EXCAVATION. THE CONTRACTOR SHALL SUBMIT BLASTING REPORTS UPON COMPLETION OF BLASTING TO THE CITY ENGINEER, THE OWNER, AND THE OWNER'S ENGINEER. TOP OF ROCK ELEVATIONS SHALL BE SHOWN ON "AS-BUILT" CONSTRUCTION DRAWINGS.
- SANITARY SEWERS
- CONNECTIONS TO THE SANITARY SEWER WILL BE PERMITTED UPON RECEIVING AN OEPA PERMIT TO INSTALL (PTI), AND UPON RECEIVING A SATISFACTORY LETTER FROM THE DESIGN ENGINEER STATING THAT THE PROJECT HAS BEEN CONSTRUCTED AS PER THE PLANS, AND ALL OF THE CONDITIONS OF THE PTI HAVE BEEN MET. THE DEVELOPER IS RESPONSIBLE FOR OBTAINING ALL REQUIRED OHIO EPA APPROVALS AND PAYING REVIEW FEES.
- SANITARY SEWAGE COLLECTION SYSTEMS SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE RULES, REGULATIONS, STANDARDS AND SPECIFICATIONS OF THE CITY OF DUBLIN, OHIO EPA, OHIO DEPARTMENT OF HEALTH AND THE CURRENT EDITION OF THE GREAT LAKES-UPPER MISSISSIPPI RIVER BOARD (TEN STATES) - RECOMMENDED STANDARDS FOR WASTEWATER FACILITIES.
- THE MINIMUM REQUIREMENTS FOR SANITARY SEWER PIPE WITH DIAMETERS 8-INCHES TO 10-INCHES SHALL BE PVC SEWER PIPE ASTM D3034, SDR 35, PIPE FOR 6-INCH DIAMETER HOUSE SERVICE LINES SHALL BE PVC PIPE ASTM D3034, SDR 35. PVC PIPE SHALL NOT BE USED AT DEPTHS GREATER THAN 28 FEET. PIPE MATERIALS AND RELATED STRUCTURES SHALL BE SHOP TESTED IN ACCORDANCE WITH CITY OF COLUMBUS CONSTRUCTION INSPECTION DIVISION QUALITY CONTROL REQUIREMENTS.
- THE MINIMUM REQUIREMENTS FOR SANITARY SEWER PIPES WITH DIAMETERS 12-INCHES TO 30-INCHES SHALL BE PVC SEWER PIPE, ASTM D3034, SDR 35 OR SANITITE HP PIPE, ASTM F2764. SANITARY SEWER PIPES WITH DIAMETERS 30-INCHES TO 60-INCHES SHALL BE PVC SEWER PIPE, ASTM D3034, SDR 35 OR SANITITE HP PIPE, ASTM F2764.
- ALL IN-LINE WYE AND TEE CONNECTIONS IN CONCRETE SEWERS, 18-INCH DIAMETER AND LARGER, SHALL BE EITHER KOR-N-TEE OR KOR-N-SEAL CONNECTIONS CONFORMING TO THE MANUFACTURER'S RECOMMENDATIONS.
- GRANULAR BACKFILL SHALL BE COMPACTED GRANULAR MATERIAL ACCORDING TO ITEM 912 OF THE STANDARD SPECIFICATIONS OR CONTROLLED DENSITY BACKFILL ACCORDING TO ITEM 613, TYPE II OF THE STANDARD SPECIFICATIONS AS DIRECTED BY THE CITY ENGINEER.
- ALL MANHOLE LIDS SHALL BE PROVIDED WITH CONTINUOUS SELF-SEALING GASKETS. THE APPROVED CONSTRUCTION DRAWINGS SHALL SHOW WHERE BOLT-DOWN LIDS ARE REQUIRED. SANITARY SEWER MANHOLES SHALL BE PLACED BETWEEN THE PIPES CROSSING AND APPROVED BY THE CITY ENGINEER AND CONFORM TO THE CITY OF DUBLIN. SANITARY MANHOLE STANDARD DRAWING. MANHOLE LIDS SHALL INCLUDE CITY OF DUBLIN LOGO.
- ALL PVC SEWER PIPES SHALL BE DEFLECTION TESTED NO LESS THAN 60 DAYS AFTER COMPLETION OF BACKFILLING OPERATIONS. ALL OTHER REQUIREMENTS SHALL BE ACCORDING TO ITEM 901.21 OF THE STANDARD SPECIFICATIONS.
- TEMPORARY BULKHEADS SHALL BE PLACED IN PIPES AT LOCATIONS SHOWN ON THE APPROVED CONSTRUCTION DRAWINGS AND SHALL REMAIN IN PLACE UNTIL THE PERMIT TO INSTALL (PTI) HAS BEEN ISSUED BY THE OEPA AND THE SEWERS HAVE BEEN APPROVED FOR USE BY THE CITY ENGINEER. THE COST FOR FURNISHING, INSTALLING, MAINTAINING, AND REMOVING BULKHEADS SHALL BE INCLUDED IN THE CONTRACT UNIT BID PRICE FOR THE VARIOUS SANITARY SEWER ITEMS.
- ALL SANITARY SEWERS INCLUDING SANITARY SEWER SERVICE LINES SHALL BE SUBJECTED TO AND PASS INFILTRATION OR EFFLUENT TESTS ACCORDING TO ITEM 901 OF THE STANDARD SPECIFICATIONS AND MUST BE APPROVED FOR USE BY THE CITY ENGINEER BEFORE ANY SERVICE CONNECTIONS ARE TAPPED INTO SEWERS.
- FOR SANITARY SEWER INFILTRATION, LEAKAGE THROUGH JOINTS SHALL NOT EXCEED 100 GALLONS PER INCH OF TRIBUTARY SEWER DIAMETER PER 24 HOURS PER MILE OF LENGTH OR THE COMPUTED EQUIVALENT. ALL SANITARY SEWERS SHALL BE TESTED.
- AT THE DETERMINATION OF THE CITY ENGINEER, THE CONTRACTOR MAY BE REQUIRED TO PERFORM A TV INSPECTION OF THE SANITARY SEWER SYSTEM PRIOR TO FINAL ACCEPTANCE BY THE CITY. THIS WORK SHALL BE COMPLETED BY THE CONTRACTOR AT HIS EXPENSE.
- VISIBLE LEAKS OR OTHER DEFECTS OBSERVED OR DISCOVERED DURING TV INSPECTION SHALL BE REPAIRED TO THE SATISFACTION OF THE ENGINEER.
- ROOF DRAINS, FOUNDATION DRAINS, FIELD TILE OR OTHER CLEAN WATER CONNECTIONS TO THE SANITARY SEWER SYSTEM ARE STRICTLY PROHIBITED ACCORDING TO SECTION 51.23 OF THE DUBLIN CODE OF ORDINANCES.
- ALL WATER LINES SHALL BE LOCATED AT LEAST 10 FEET HORIZONTALLY AND 18 INCHES VERTICALLY FROM SANITARY SEWERS AND STORM SEWERS, TO THE GREATEST EXTENT PRACTICABLE. WHERE SANITARY SEWERS CROSS WATERMAINS OR OTHER SEWERS OR OTHER UTILITIES, TRENCH BACKFILL SHALL BE PLACED BETWEEN THE PIPES CROSSING AND SHALL BE COMPACTED GRANULAR MATERIAL ACCORDING TO ITEM 912 OF THE STANDARD SPECIFICATIONS. IN THE EVENT THAT A WATER LINE MUST CROSS WITHIN 18 INCHES OF A SANITARY SEWER, THE SANITARY SEWER SHALL BE CONCRETE ENCASED OR CONSIST OF DUCTILE IRON PIPE MATERIAL.
- SERVICE RISERS SHALL BE INSTALLED WHERE THE DEPTH FROM WYES TO PROPOSED GROUND ELEVATION EXCEEDS 10 FEET. TOPS OF RISERS SHALL BE NO LESS THAN 9 FEET BELOW PROPOSED GROUND ELEVATION IF BASEMENT SERVICE IS INTENDED.
- WHERE SERVICE RISERS ARE NOT INSTALLED, A MINIMUM 5-FOOT LENGTH OF SANITARY SEWER SERVICE PIPE OF THE SAME SIZE AS THE WYE OPENING SHALL BE INSTALLED.
- THE CONTRACTOR SHALL FURNISH AND PLACE, AS DIRECTED, APPROVED WYE POLES MADE OF 2 INCHES X 2 INCHES LUMBER AT ALL WYE LOCATIONS, ENDS OF EXTENDED SERVICES, OR AT THE END OF EACH RISER WHERE RISERS ARE REQUIRED. WYE POLES SHALL BE VISIBLE BEFORE ACCEPTANCE BY THE CITY. THE COST OF THESE POLES SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR THE VARIOUS SEWER ITEMS.
- EXISTING SANITARY SEWER FLOWS SHALL BE MAINTAINED AT ALL TIMES. COSTS FOR PUMPING AND BYPASSING SHALL BE INCLUDED IN THE CONTRACTOR'S UNIT PRICE BID FOR THE RELATED ITEMS.
- THE CONTRACTOR SHALL FURNISH ALL MATERIAL, EQUIPMENT, AND LABOR TO MAKE CONNECTIONS TO EXISTING MANHOLES. THE SEWER PIPE TO MANHOLE CONNECTIONS FOR ALL SANITARY SEWERS SHALL BE FLEXIBLE AND WATER TIGHT. ALL HOLES SHALL BE NEATLY CORED. THE SEWER PIPE BARREL AT THE SPRINGLINE SHALL NOT EXTEND MORE THAN 1 INCH BEYOND THE INSIDE FACE OF THE MANHOLE. TO MAINTAIN FLEXIBILITY IN THE CONNECTION, A 1-INCH SPACE SHALL BE LEFT BETWEEN THE END OF THE PIPE INSIDE THE MANHOLE AND THE CHANNEL. THIS SPACE SHALL BE FILLED WITH A WATERPROOF FLEXIBLE JOINT FILLER. ANY METAL THAT IS USED SHALL BE TYPE 300 SERIES STAINLESS STEEL. THE CONNECTION MAY BE ANY OF THE FOLLOWING TYPES:  
A. RUBBER SLEEVE WITH STAINLESS STEEL BANDING.  
1) KOR-N-SEAL AS MANUFACTURED BY NATIONAL POLLUTION CONTROL SYSTEMS, INC.  
2) LOCK JOINT FLEXIBLE MANHOLE SLEEVE AS MANUFACTURED BY INTERFACE CORPORATION.  
3) OR EQUAL AS APPROVED BY THE CITY ENGINEER.  
B. RUBBER GASKET COMPRESSION.  
1) PRESS WEDGE II AS MANUFACTURED BY PRESS-SEAL GASKET CORPORATION.  
2) DURA SEAL III AS MANUFACTURED BY DURA TECH, INC.  
3) LINK-SEAL AS MANUFACTURED BY THUNDERLINK CORPORATION.  
4) OR EQUAL AS APPROVED BY THE CITY ENGINEER.
- THE COST FOR THIS WORK ALONG WITH A NEW CHANNELIZED BASE FOR THE MANHOLE SHALL BE INCLUDED IN THE UNIT BID PRICE FOR THE RELATED ITEMS OF WORK.
- WATER LINE
- ALL WATER LINE MATERIALS SHALL BE PROVIDED AND INSTALLED ACCORDING TO CURRENT SPECIFICATIONS OF THE CITY OF COLUMBUS DIVISION OF WATER.
- ALL PUBLIC WATER PIPE WITH A DIAMETER 3 INCHES TO 8 INCHES SHALL BE DUCTILE IRON, CLASS 53. PUBLIC WATER PIPE 12 INCHES IN DIAMETER OR LARGER SHALL BE DUCTILE IRON, CLASS 53. PUBLIC WATER PIPE 20 INCHES IN DIAMETER OR LARGER MAY BE PRESTRESSED CONCRETE PIPE. PRIVATE WATER PIPE SHALL MEET THE APPROVAL OF THE CITY OF COLUMBUS DIVISION OF WATER PRIOR TO APPROVAL OF THE CONSTRUCTION DRAWINGS.

- ONLY FIRE HYDRANTS CONFORMING TO CITY OF COLUMBUS STANDARDS WILL BE APPROVED FOR USE.
- PUBLIC WATER LINES SHALL BE DISINFECTED BY THE CITY OF COLUMBUS DIVISION OF WATER. REQUESTS FOR WATER LINE CHLORINATION SHALL BE MADE THROUGH THE CITY OF DUBLIN DIVISION OF ENGINEERING. THE COST FOR CHLORINATION SHALL BE PAID FOR BY THE CONTRACTOR.
- ALL WATER LINES SHALL BE DISINFECTED ACCORDING TO ITEM 801.15 OF THE STANDARD SPECIFICATIONS. SPECIAL ATTENTION IS DIRECTED TO APPLICABLE SECTIONS OF AMERICAN WATER WORKS ASSOCIATION SPECIFICATION C-651, PARTICULARLY FOR FLUSHING (SECTION 5) AND FOR CHLORINATING VALVES AND FIRE HYDRANTS (SECTION 7). PRESSURE TESTING SHALL BE PERFORMED IN ACCORDANCE WITH SECTION 801.14 OF THE CITY OF COLUMBUS CONSTRUCTION AND MATERIAL SPECIFICATIONS. WHEN WATER LINES ARE READY FOR DISINFECTION, THE CITY OF DUBLIN SHALL SUBMIT TWO (2) SETS OF "AS-BUILT" PLANS, AND A LETTER STATING THAT THE WATER LINES HAVE BEEN PRESSURE TESTED AND NEED TO BE DISINFECTED, TO THE CITY OF COLUMBUS, DIVISION OF WATER. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL COSTS ASSOCIATED WITH THE DISINFECTION OF WATER SERVICE. INTERRUPTION OF WATER SERVICE SHALL BE PERFORMED IN ACCORDANCE WITH SECTION 801.14 OF THE CITY OF COLUMBUS CONSTRUCTION AND MATERIAL SPECIFICATIONS.
- THE CONTRACTOR SHALL PAINT ALL FIRE HYDRANTS ACCORDING TO CITY OF DUBLIN STANDARDS. THE COST OF PAINTING FIRE HYDRANTS SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR FIRE HYDRANTS.
- NO WATER TAPS OR SERVICE CONNECTIONS (E.G., TO CURB STOPS OR METER PITS) MAY BE ISSUED UNTIL ADJACENT PUBLIC WATER LINES SERVING THE CONSTRUCTION SITE HAVE BEEN DISINFECTED BY THE CITY OF COLUMBUS DIVISION OF WATER AND HAVE BEEN ACCEPTED BY THE CITY ENGINEER. A TAP PERMIT FOR EACH WATER SERVICE MUST BE OBTAINED FROM THE CITY OF DUBLIN AND THE CITY OF COLUMBUS DIVISION OF WATER BEFORE MAKING ANY TAPS INTO PUBLIC WATER LINES.
- THE CONTRACTOR SHALL NOTIFY THE CITY OF COLUMBUS DIVISION OF WATER AT 645-7788 AND THE CITY OF DUBLIN DIVISION OF ENGINEERING AT LEAST 24 HOURS BEFORE TAPPING INTO EXISTING WATER LINES.
- ALL WATER MAIN STATIONING SHALL BE BASED ON STREET CENTERLINE STATIONING.
- ALL BENDS, JOINT DEFLECTIONS AND FITTINGS SHALL BE BACKED WITH CONCRETE PER CITY OF COLUMBUS STANDARDS.
- THE CONTRACTOR SHALL GIVE WRITTEN NOTICE TO ALL AFFECTED PROPERTY OWNERS AT LEAST 1 WORKING DAY BUT NOT MORE THAN 3 WORKING DAYS PRIOR TO ANY TEMPORARY INTERRUPTION OF WATER SERVICE. INTERRUPTION OF WATER SERVICE SHALL BE MINIMIZED AND MUST BE APPROVED BY THE CITY ENGINEER.
- WATER METERS SHALL BE INSTALLED INSIDE PROPOSED STRUCTURES UNLESS A METER PIT INSTALLATION IS APPROVED BY THE CITY OF COLUMBUS DIVISION OF WATER. METER PITS MUST CONFORM TO STANDARD DRAWINGS L-7103, A&B FOR 5/8" THROUGH 1" METERS OR L-6317, A, B, C&D FOR 1-1/2" OR LARGER METERS.
- WATER LINES TO BE INSTALLED IN EMBANKMENT AREAS SHALL BE PLACED AFTER THE EMBANKMENT HAS BEEN PLACED AND COMPACTED ACCORDING TO THE STANDARD SPECIFICATIONS.
- CURB STOP BOXES SHALL BE LOCATED AT LEAST 1 FOOT INSIDE THE RIGHT-OF-WAY AND SET AT FINISHED GRADE.
- IF THE TOP OF THE OPERATING NUT OF ANY VALVE IS GREATER THAN 36 INCHES BELOW FINISHED GRADE, AN EXTENSION STEM SHALL BE FURNISHED TO BRING THE TOP OF THE OPERATING NUT TO WITHIN 24 INCHES OF FINISHED GRADE ELEVATION.
- ALL WATER LINES SHALL BE PLACED AT A MINIMUM DEPTH OF 4 FEET MEASURED FROM TOP OF FINISHED GRADE TO TOP OF WATER LINE. WATER LINES SHALL BE SET DEEPER AT ALL POINTS WHERE NECESSARY TO CLEAR EXISTING OR PROPOSED UTILITY LINES OR OTHER UNDERGROUND RESTRICTIONS BY A MINIMUM OF 18 INCHES.
- TWO 3/4 INCH TAPS SHALL BE INSTALLED WITHIN 2 FEET OF THE END OF THE LINE ON ALL DEAD-END WATER LINES.
- STORM SEWER
- ALL STORM WATER DETENTION AND RETENTION AREAS AND MAJOR FLOOD ROUTING SWALES SHALL BE CONSTRUCTED TO FINISH GRADE AND HYDRO-SEEDED AND HYDRO-MULCHED ACCORDING TO ITEMS 203 AND 659 OF THE STANDARD SPECIFICATIONS.
- WHERE PRIVATE STORM SEWERS CONNECT TO PUBLIC STORM SEWERS, THE LAST RUN OF PRIVATE STORM SEWER CONNECTING TO THE PUBLIC STORM SEWER SHALL BE REINFORCED CONCRETE PIPE CONFORMING TO ASTM DESIGNATION C76, WALL B, CLASS IV FOR PIPE DIAMETERS 12 INCHES TO 15 INCHES, CLASS III FOR 18 INCHES TO 24 INCH PIPES, AND 27 INCHES AND LARGER PIPE SHALL BE CLASS II, UNLESS OTHERWISE SHOWN ON THE APPROVED CONSTRUCTION DRAWINGS. INSPECTION IS REQUIRED BY THE CITY OF DUBLIN'S DIVISION OF ENGINEERING.
- GRANULAR BACKFILL SHALL BE COMPACTED GRANULAR MATERIAL ACCORDING TO ITEM 912 OF THE STANDARD SPECIFICATIONS OR CONTROLLED DENSITY BACKFILL ACCORDING TO ITEM 613, TYPE II OF THE STANDARD SPECIFICATIONS AS DIRECTED BY THE CITY ENGINEER.
- ALL STORM SEWERS SHALL BE REINFORCED CONCRETE PIPE CONFORMING TO ASTM DESIGNATION C76, WALL B, CLASS IV FOR PIPE DIAMETERS 12 INCHES TO 15 INCHES, CLASS III FOR 18 INCHES TO 24 INCH PIPES, AND 27 INCHES AND LARGER PIPE SHALL BE CLASS II, UNLESS OTHERWISE SHOWN ON THE APPROVED CONSTRUCTION DRAWINGS.
- HEADWALLS AND ENDWALLS SHALL BE REQUIRED AT ALL STORM SEWER INLETS OR OUTLETS TO AND FROM STORMWATER MANAGEMENT FACILITIES. NATURAL STONE AND/OR BRICK APPROVED BY THE CITY ENGINEER SHALL BE PROVIDED ON ALL VISIBLE HEADWALLS AND/OR ENDWALLS SURFACES.
- STORM INLETS OR CATCH BASINS SHALL BE CHANNELIZED AND HAVE BICYCLE SAFE GRATES. MANHOLE LIDS SHALL INCLUDE CITY OF DUBLIN LOGO AND ALL CURB INLET AND CATCH BASIN GRATES SHALL INCLUDE ENGRAVED LETTERING: "DUMP NO WASTE; DRAINS TO RIVER".
- STORM SEWER OUTLETS GREATER THAN 18 INCHES IN DIAMETER ACCESSIBLE FROM STORMWATER MANAGEMENT FACILITIES OR WATERCOURSES SHALL BE PROVIDED WITH SAFETY GRATES, AS APPROVED BY THE CITY ENGINEER.
- MAIL DELIVERY
- THE CONTRACTOR SHALL BE RESPONSIBLE TO ENSURE THAT US MAIL DELIVERY WITHIN THE PROJECT LIMITS IS NOT DISRUPTED BY CONSTRUCTION OPERATIONS. THIS RESPONSIBILITY IS LIMITED TO RELOCATION OF MAILBOXES TO A TEMPORARY LOCATION THAT WILL ALLOW THE COMPLETION OF THE WORK AND SHALL ALSO INCLUDE THE RESTORATION OF MAILBOXES TO THEIR ORIGINAL LOCATION OR APPROVED LOCATION. ANY RELOCATION OF MAILBOX SERVICES MUST BE FIRST COORDINATED WITH THE US POSTAL SERVICE AND THE HOMEOWNER.
- BEFORE RELOCATING ANY MAILBOXES, THE CONTRACTOR SHALL CONTACT THE U.S. POSTAL SERVICE AND RELOCATE MAILBOXES ACCORDING TO THE REQUIREMENTS OF THE POSTAL SERVICE.
- USE OF FIRE HYDRANTS
- THE CONTRACTOR SHALL MAKE PROPER ARRANGEMENTS WITH THE DUBLIN SERVICE DEPARTMENT AND THE COLUMBUS DIVISION OF WATER FOR THE USE OF FIRE HYDRANTS WHEN USED FOR WORK PERFORMED UNDER THIS CONTRACT AND PROVIDE THE CITY OF DUBLIN A COPY OF THE HYDRANT USAGE PERMIT OBTAINED FROM THE CITY OF COLUMBUS. THE CONTRACTOR SHALL ALSO SEND A COPIES OF PERMITS OBTAINED FROM DUBLIN AND COLUMBUS TO THE WASHINGTON AND/OR PERRY TOWNSHIP FIRE DEPARTMENT. PERMITS SHALL BE KEPT AT THE CONSTRUCTION SITE AT ALL TIMES.
- BEFORE THE FINAL ESTIMATE IS PAID, THE CONTRACTOR SHALL SUBMIT A LETTER FROM THE CITY OF COLUMBUS DIVISION OF WATER TO THE CITY ENGINEER STATING THAT THE CONTRACTOR HAS RETURNED THE SIAMSE VALVE TO THE CITY OF COLUMBUS AND HAS PAID ALL COSTS ARISING FROM THE USE OF THE FIRE HYDRANTS.

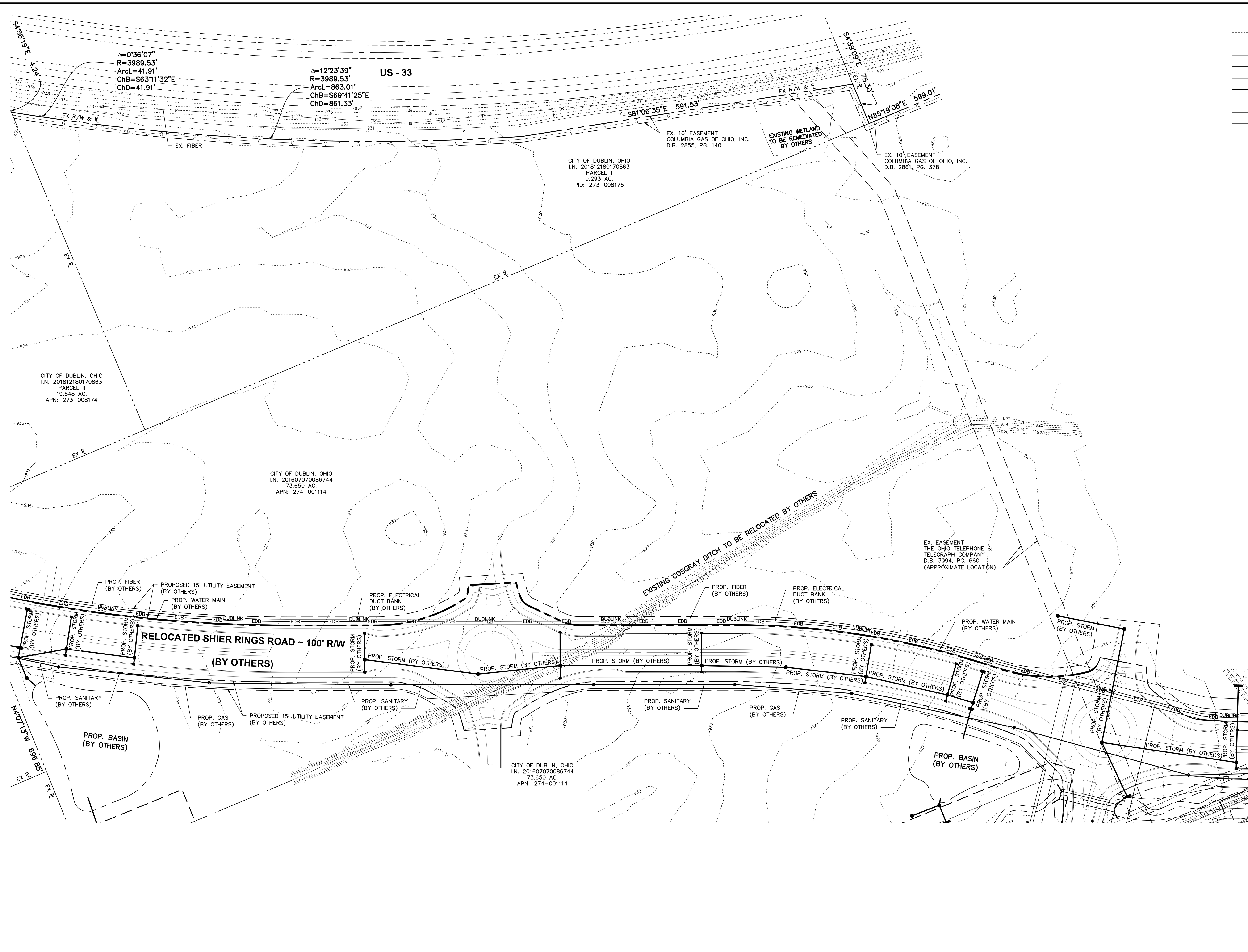
CITY OF DUBLIN, OHIO  
PRELIMINARY DEVELOPMENT PLAN  
GENERAL NOTES  
FOR  
OSU MEDICAL CAMPUS PUD



PLAN PREPARED BY:  
422 Beecher Road  
Gahanna, Ohio 43230  
ph 614.428.7750  
fax 614.428.7755



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**LEGEND**

- 929 --- EXISTING 1' CONTOUR
- 930 --- EXISTING 5' CONTOUR
- ST ST EXISTING STORM SEWER
- PROPOSED STORM SEWER (BY OTHERS)
- PROPOSED SANITARY SEWER (BY OTHERS)
- PROPOSED WATER MAIN (BY OTHERS)
- DUBLINK --- PROPOSED FIBER (BY OTHERS)
- TR EXISTING FIBER
- EDB PROPOSED ELECTRICAL DUCT BANK (BY OTHERS)
- PROPOSED GAS MAIN (BY OTHERS)

$\Delta=0^{\circ}36'07''$   
 $R=3989.53'$   
 $Arcl=41.91'$   
 $ChB=S63^{\circ}11'32''E$   
 $ChD=41.91'$

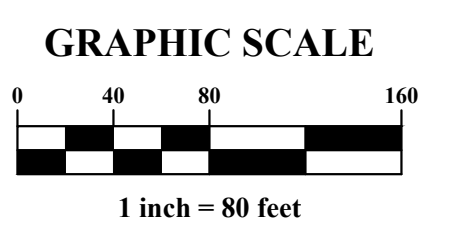
**US - 33**  
 $\Delta=12^{\circ}23'39''$   
 $R=3989.53'$   
 $Arcl=863.01'$   
 $ChB=S69^{\circ}41'25''E$   
 $ChD=861.33'$

CITY OF DUBLIN, OHIO  
I.N. 201812180170863  
PARCEL 1  
9.293 AC.  
PID: 273-008175

CITY OF DUBLIN, OHIO  
I.N. 201812180170863  
PARCEL II  
19.548 AC.  
APN: 273-008174

CITY OF DUBLIN, OHIO  
I.N. 201607070086744  
73.850 AC.  
APN: 274-001114

CITY OF DUBLIN, OHIO  
I.N. 201607070086744  
73.850 AC.  
APN: 274-001114



**CITY OF DUBLIN, OHIO  
PRELIMINARY DEVELOPMENT PLAN  
EXISTING CONDITIONS  
FOR  
OSU MEDICAL CAMPUS PUD**

PLAN PREPARED BY:



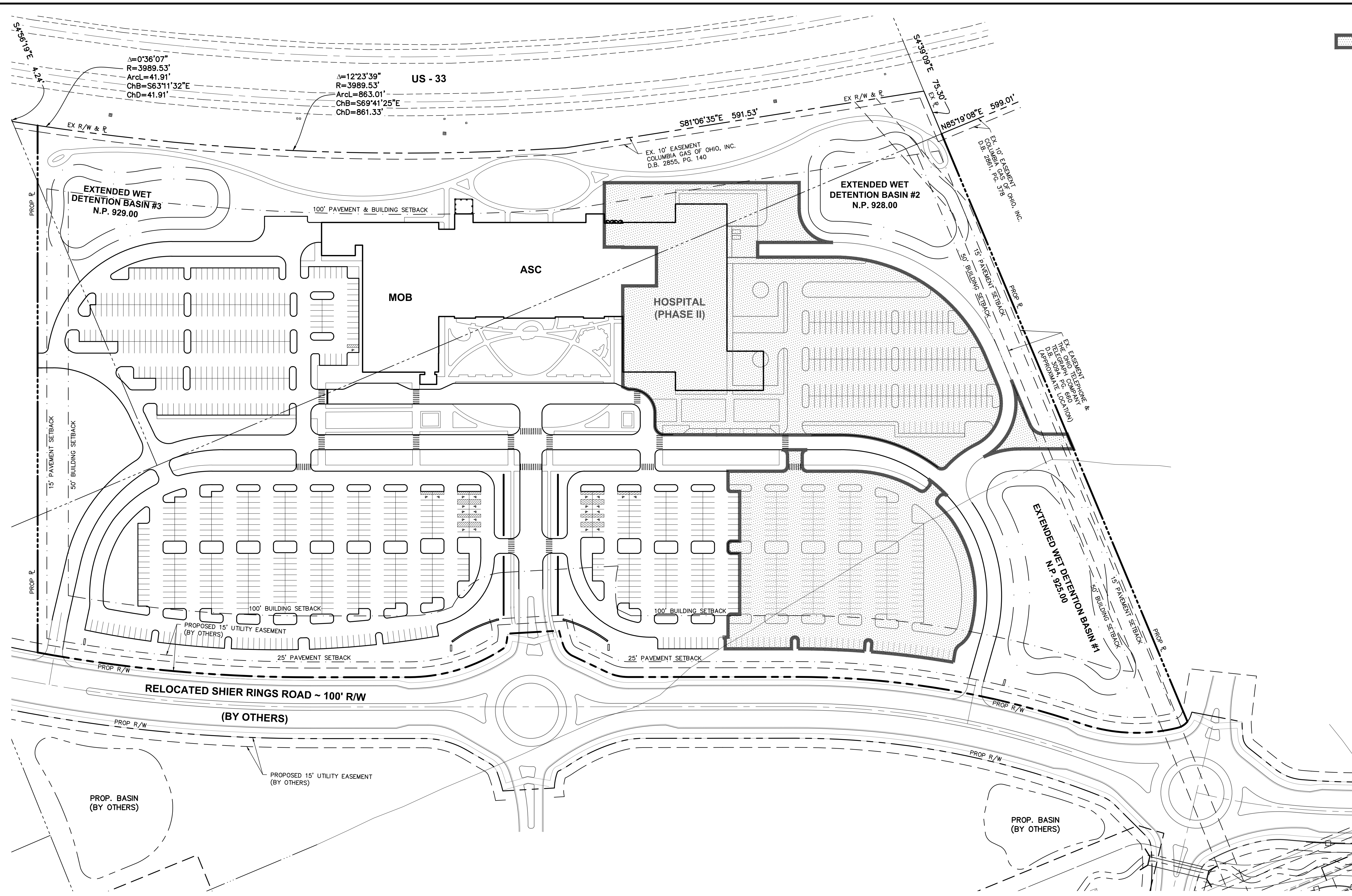
**ADVANCED  
CIVIL DESIGN**  
ENGINEERS SURVEYORS

422 Beecher Road  
Gahanna, Ohio 43230  
ph 614.428.7750  
fax 614.428.7755

SCALE: 1" = 80'  
DATE: 08/29/2019 SHEET 4 / 15

Z:\18-0003-114\DWG\PRODUCTION DRAWINGS\PRE DEVELOPMENT PLAN\003-114\_PDP\_Phase Plan.dwg PHASE Aug 29, 2019 - 7:08:53am rfwlwr

LEGEND



$\Delta=0^{\circ}36'07''$   
 $R=3989.53'$   
 $Arcl=41.91'$   
 $ChB=S63^{\circ}11'32''E$   
 $ChD=41.91'$

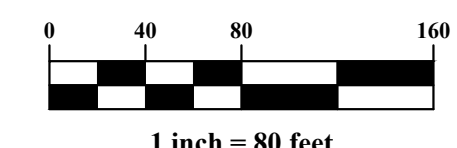
**US - 33**  
 $\Delta=12^{\circ}23'39''$   
 $R=3989.53'$   
 $Arcl=863.01'$   
 $ChB=S69^{\circ}41'25''E$   
 $ChD=861.33'$

EX 10' EASEMENT  
COLUMBIA GAS OF OHIO, INC.  
D.B. 2855, PG. 140

EX 10' EASEMENT  
COLUMBIA GAS OF OHIO, INC.  
D.B. 2855, PG. 140

EX EASEMENT TELEPHONE &  
CABLE TELEVISION (SEE LOCATION)  
D.B. APPROXIMATELY 1981

GRAPHIC SCALE



1 inch = 80 feet

CITY OF DUBLIN, OHIO  
PRELIMINARY DEVELOPMENT PLAN  
PHASING PLAN  
FOR  
OSU MEDICAL CAMPUS PUD

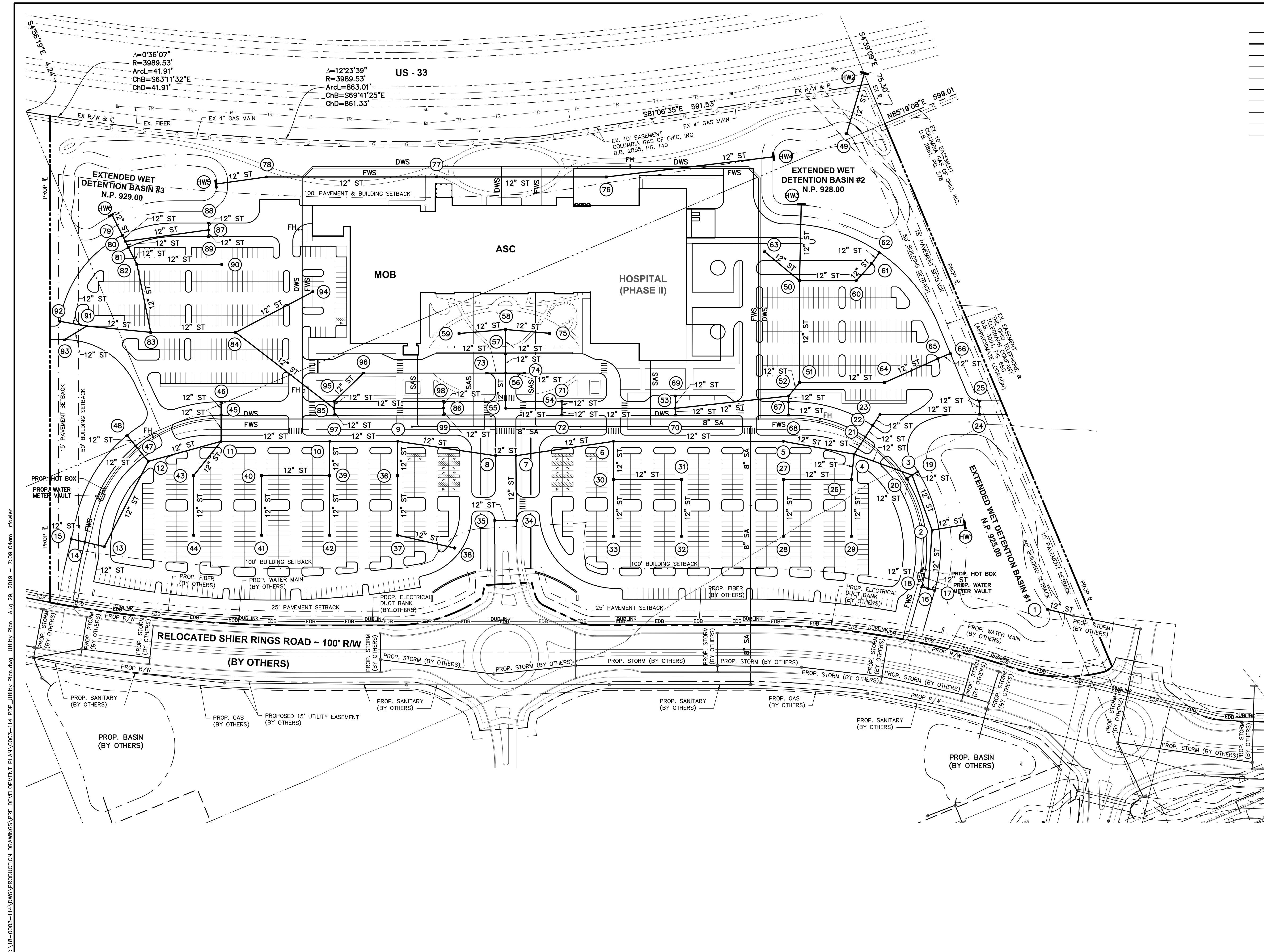
PLAN PREPARED BY:



**ADVANCED**  
CIVIL DESIGN  
ENGINEERS SURVEYORS

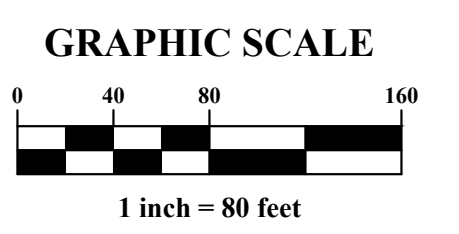
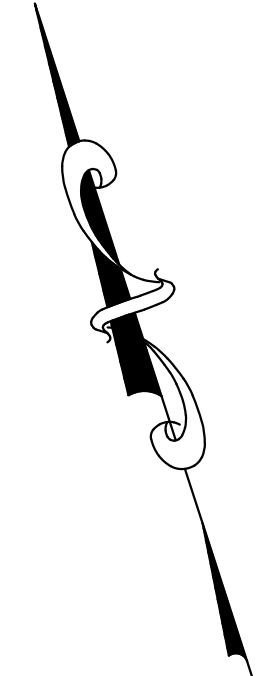
422 Beecher Road  
Gahanna, Ohio 43230  
ph 614.428.7750  
fax 614.428.7755

SCALE: 1" = 80'  
DATE: 08/29/2019 SHEET 5 / 15



**LEGEND**

- ST — ST — EXISTING STORM SEWER
- ■ — PROPOSED STORM SEWER
- ● — PROPOSED STORM SEWER (BY OTHERS)
- ● — PROPOSED SANITARY SEWER (BY OTHERS)
- ● — PROPOSED WATER MAIN (BY OTHERS)
- ● — PROPOSED FIBER (BY OTHERS)
- ● — DUBLINK — PROPOSED FIBER (BY OTHERS)
- EDB — PROPOSED ELECTRIC DUCT BANK (BY OTHERS)
- G — G — EXISTING GAS MAIN
- ● — PROPOSED GAS MAIN (BY OTHERS)



**CITY OF DUBLIN, OHIO  
PRELIMINARY DEVELOPMENT PLAN  
UTILITY PLAN  
FOR  
OSU MEDICAL CAMPUS PUD**

PLAN PREPARED BY:

**ADVANCED  
CIVIL DESIGN  
ENGINEERS SURVEYORS**

422 Beecher Road  
Gahanna, Ohio 43230  
ph 614.428.7750  
fax 614.428.7755

SCALE: 1" = 80'  
DATE: 08/29/2019

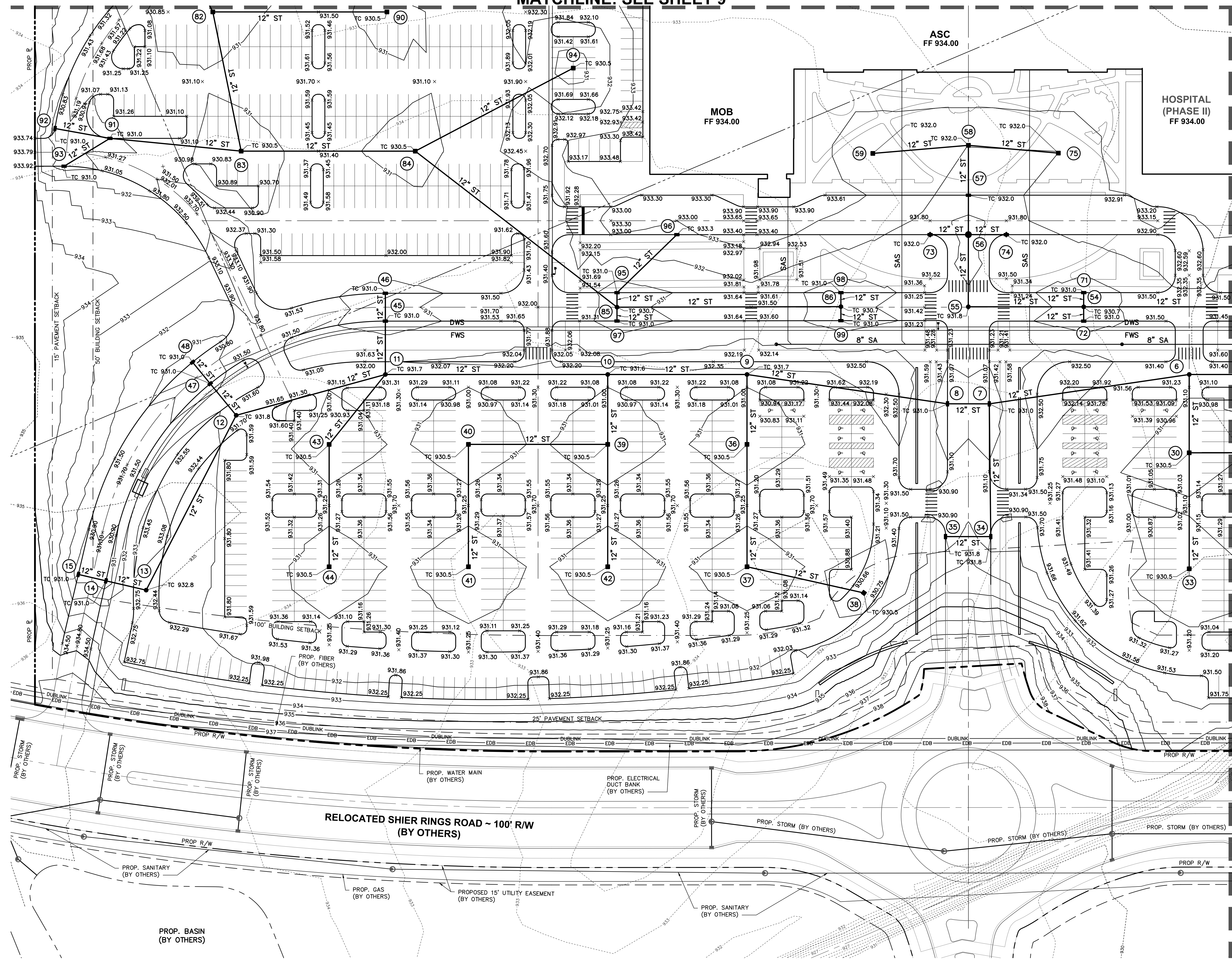
SHEET 6 / 15

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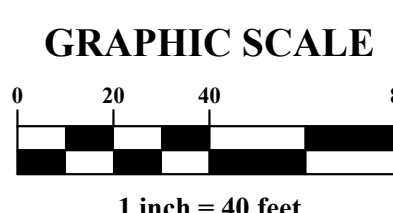
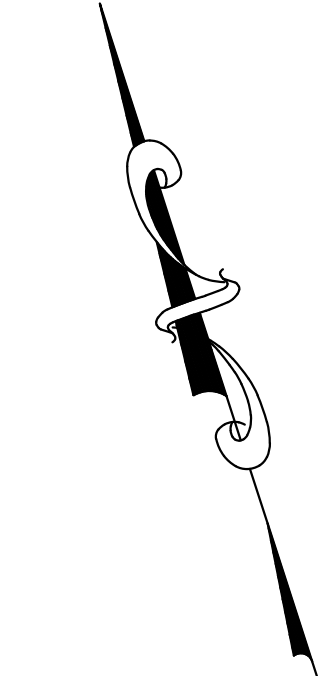
MATCHLINE: SEE SHEET 9

LEGEND

- - - - - 929 - - - - - EXISTING 1' CONTOUR
- - - - - 930 - - - - - EXISTING 5' CONTOUR
- - - - - 928 - - - - - PROPOSED 1' CONTOUR
- - - - - 930 - - - - - PROPOSED 5' CONTOUR
- ST - ST EXISTING STORM SEWER
- ■ — PROPOSED STORM SEWER
- ■ — PROPOSED STORM SEWER (BY OTHERS)
- ● — PROPOSED SANITARY SEWER (BY OTHERS)
- ● — PROPOSED WATER MAIN (BY OTHERS)
- ● — PROPOSED FIBER (BY OTHERS)
- — — DUBLINK
- — — PROPOSED ELECTRIC DUCT BANK (BY OTHERS)
- — — PROPOSED GAS MAIN (BY OTHERS)



MATCHLINE: SEE SHEET 8



**CITY OF DUBLIN, OHIO**  
**PRELIMINARY DEVELOPMENT PLAN**  
**GRADING PLAN**  
**FOR**  
**OSU MEDICAL CAMPUS PUD**

PLAN PREPARED BY:

**ADVANCED**  
**CIVIL DESIGN**  
**ENGINEERS SURVEYORS**

422 Beecher Road  
Gahanna, Ohio 43230  
ph 614.428.7750  
fax 614.428.7755

SCALE: 1" = 40'  
DATE: 08/29/2019

SHEET 7 / 15

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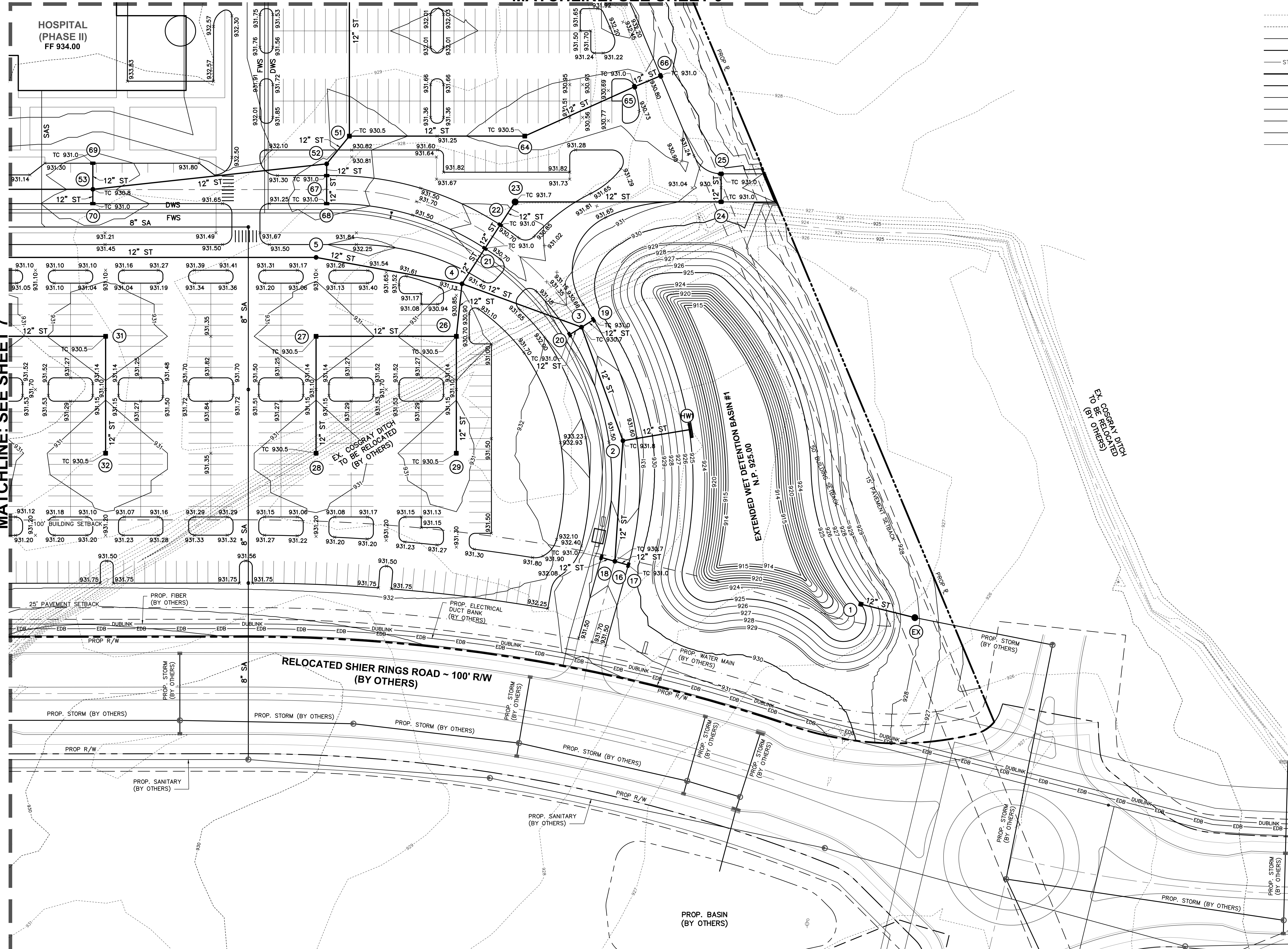
MATCHLINE: SEE SHEET 9

LEGEND

- 929 --- EXISTING 1" CONTOUR
- 930 --- EXISTING 5" CONTOUR
- 929 --- PROPOSED 1" CONTOUR
- 930 --- PROPOSED 5" CONTOUR
- ST ST EXISTING STORM SEWER
- PROPOSED STORM SEWER
- PROPOSED STORM SEWER (BY OTHERS)
- PROPOSED SANITARY SEWER (BY OTHERS)
- PROPOSED WATER MAIN (BY OTHERS)
- DUBLINK --- PROPOSED FIBER (BY OTHERS)
- EDB --- PROPOSED ELECTRIC DUCT BANK (BY OTHERS)
- PROPOSED GAS MAIN (BY OTHERS)

MATCHLINE: SEE SHEET 7

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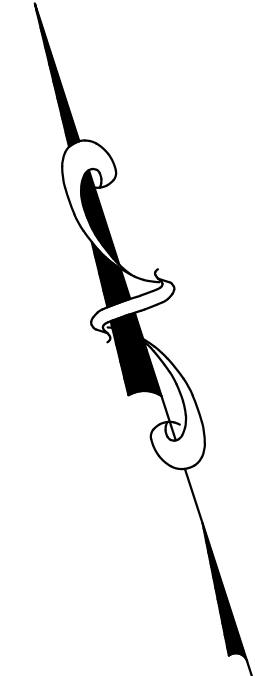


EX COSGRAY DITCH (BY OTHERS)

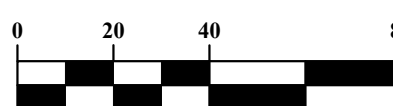
EXTENDED WET DETENTION BASIN #1  
IN P. 925.00

RELOCATED SHIER RINGS ROAD ~ 100' R/W  
(BY OTHERS)

PROP. BASIN  
(BY OTHERS)



GRAPHIC SCALE



1 inch = 40 feet

CITY OF DUBLIN, OHIO  
 PRELIMINARY DEVELOPMENT PLAN  
 GRADING PLAN  
 FOR  
 OSU MEDICAL CAMPUS PUD

PLAN PREPARED BY:

**ADVANCED**  
 CIVIL DESIGN  
 ENGINEERS SURVEYORS

422 Beecher Road  
 Gahanna, Ohio 43230  
 ph 614.428.7750  
 fax 614.428.7755

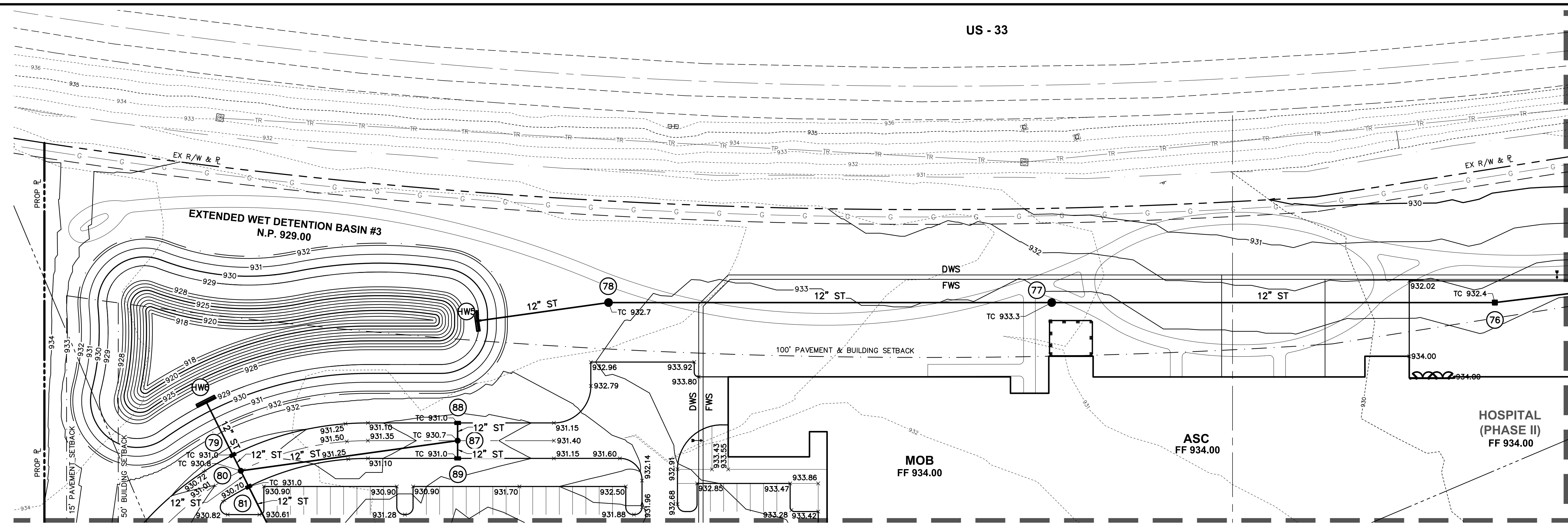
SCALE: 1" = 40'  
 DATE: 08/29/2019

SHEET 8 / 15

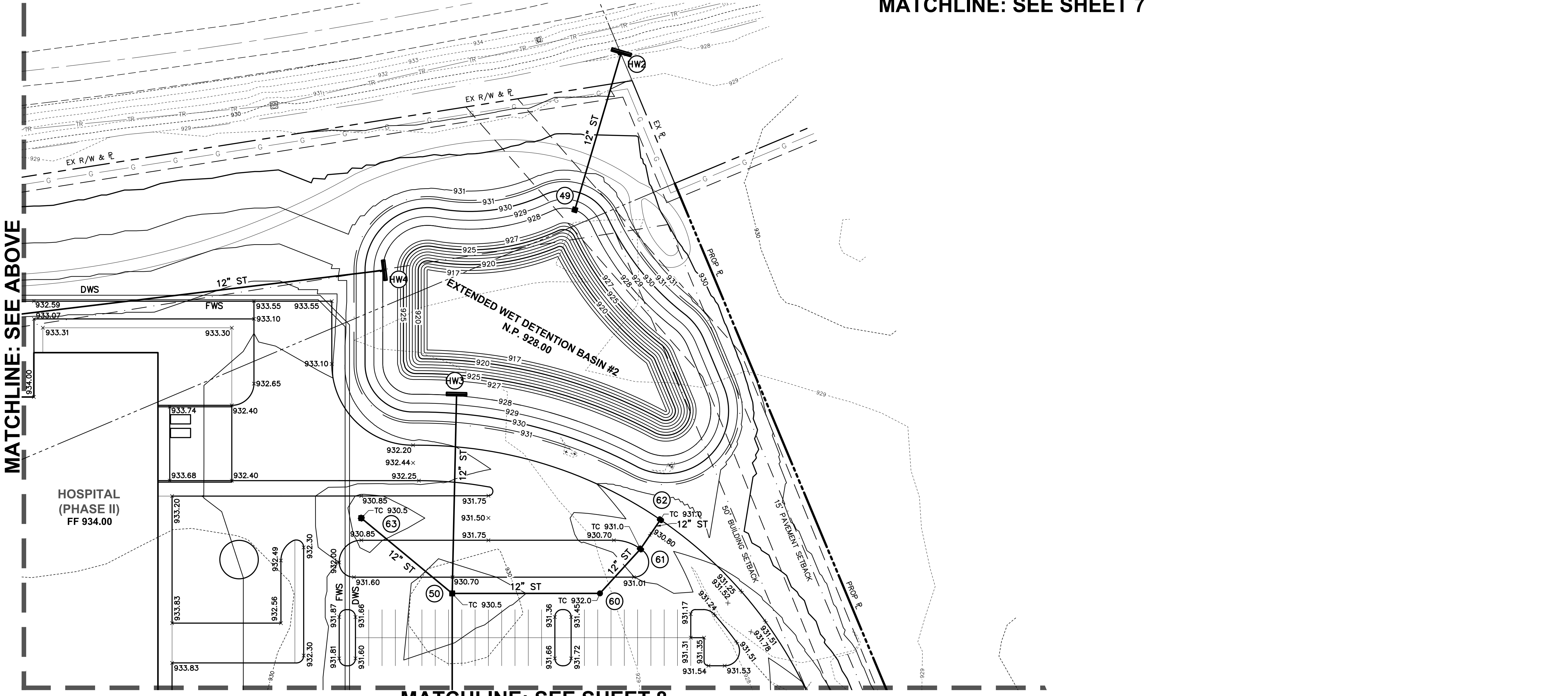
LEGEND

- 929 --- EXISTING 1" CONTOUR
- 930 --- EXISTING 5" CONTOUR
- 929 --- PROPOSED 1" CONTOUR
- 930 --- PROPOSED 5" CONTOUR
- ST --- ST EXISTING STORM SEWER
- ■ --- PROPOSED STORM SEWER
- ■ --- PROPOSED STORM SEWER (BY OTHERS)
- ● --- PROPOSED SANITARY SEWER (BY OTHERS)
- ● --- PROPOSED WATER MAIN (BY OTHERS)
- ● --- PROPOSED FIBER (BY OTHERS)
- ED --- DUBLINK
- ED --- PROPOSED ELECTRIC DUCT BANK (BY OTHERS)
- ED --- PROPOSED GAS MAIN (BY OTHERS)

MATCHLINE: SEE BELOW



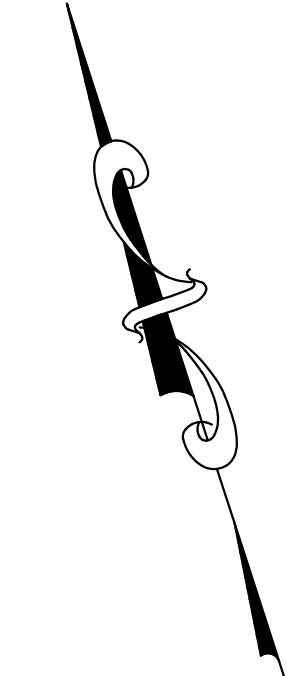
MATCHLINE: SEE SHEET 7



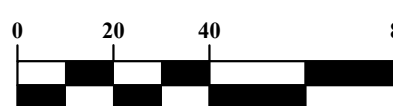
MATCHLINE: SEE SHEET 8

Z:\18-0003-114\DWG\PRODUCTION DRAWINGS\PRE DEVELOPMENT PLAN\003-114\_PDP\_Grading.dwg GRADING (3) Aug 29, 2019 - 7:05:36am rfwfwr

MATCHLINE: SEE ABOVE



GRAPHIC SCALE



1 inch = 40 feet

CITY OF DUBLIN, OHIO  
PRELIMINARY DEVELOPMENT PLAN  
GRADING PLAN  
FOR  
OSU MEDICAL CAMPUS PUD

PLAN PREPARED BY:



**ADVANCED**  
CIVIL DESIGN  
ENGINEERS SURVEYORS

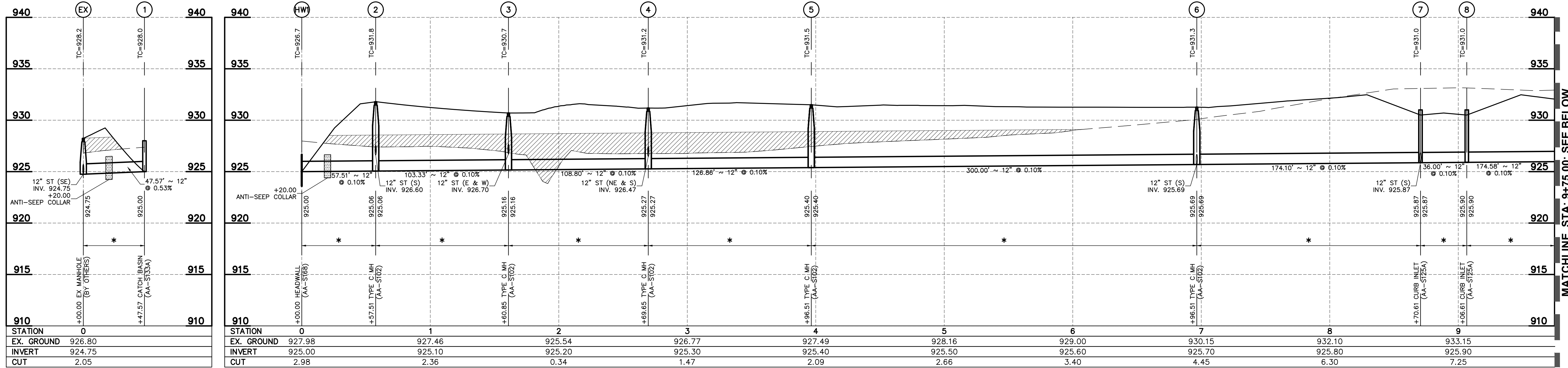
422 Beecher Road  
Gahanna, Ohio 43230  
ph 614.428.7750  
fax 614.428.7755

SCALE: 1" = 40'  
DATE: 08/29/2019

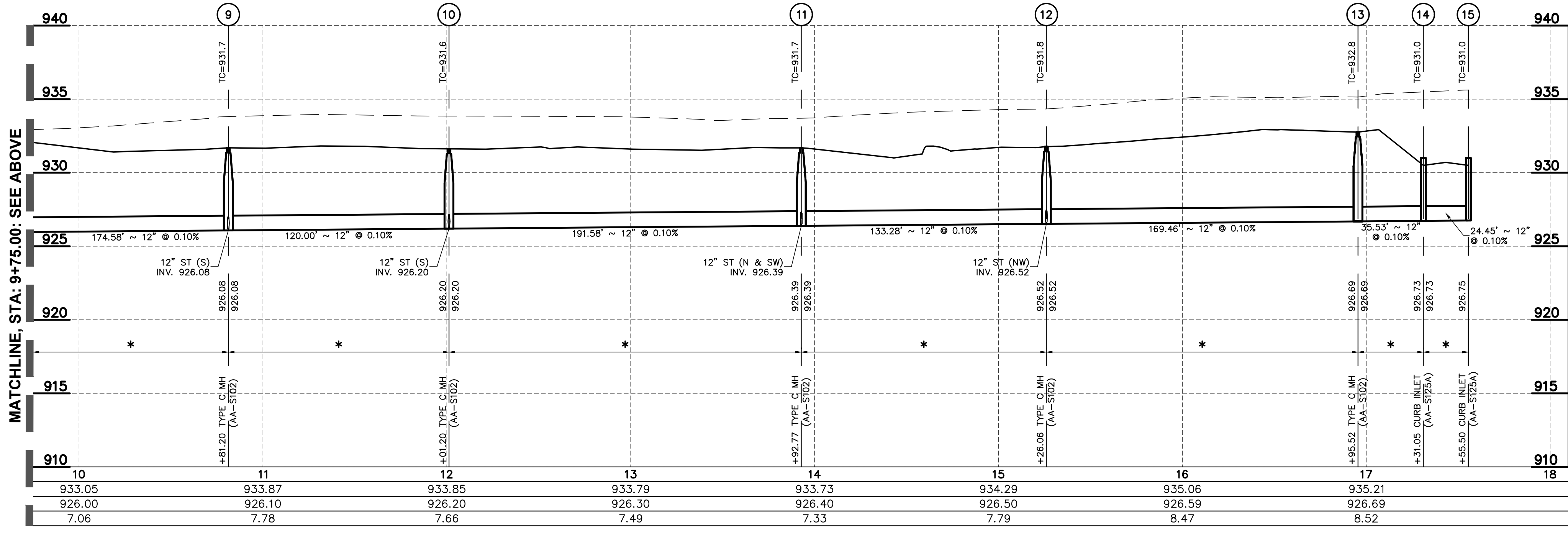
SHEET 9 / 15



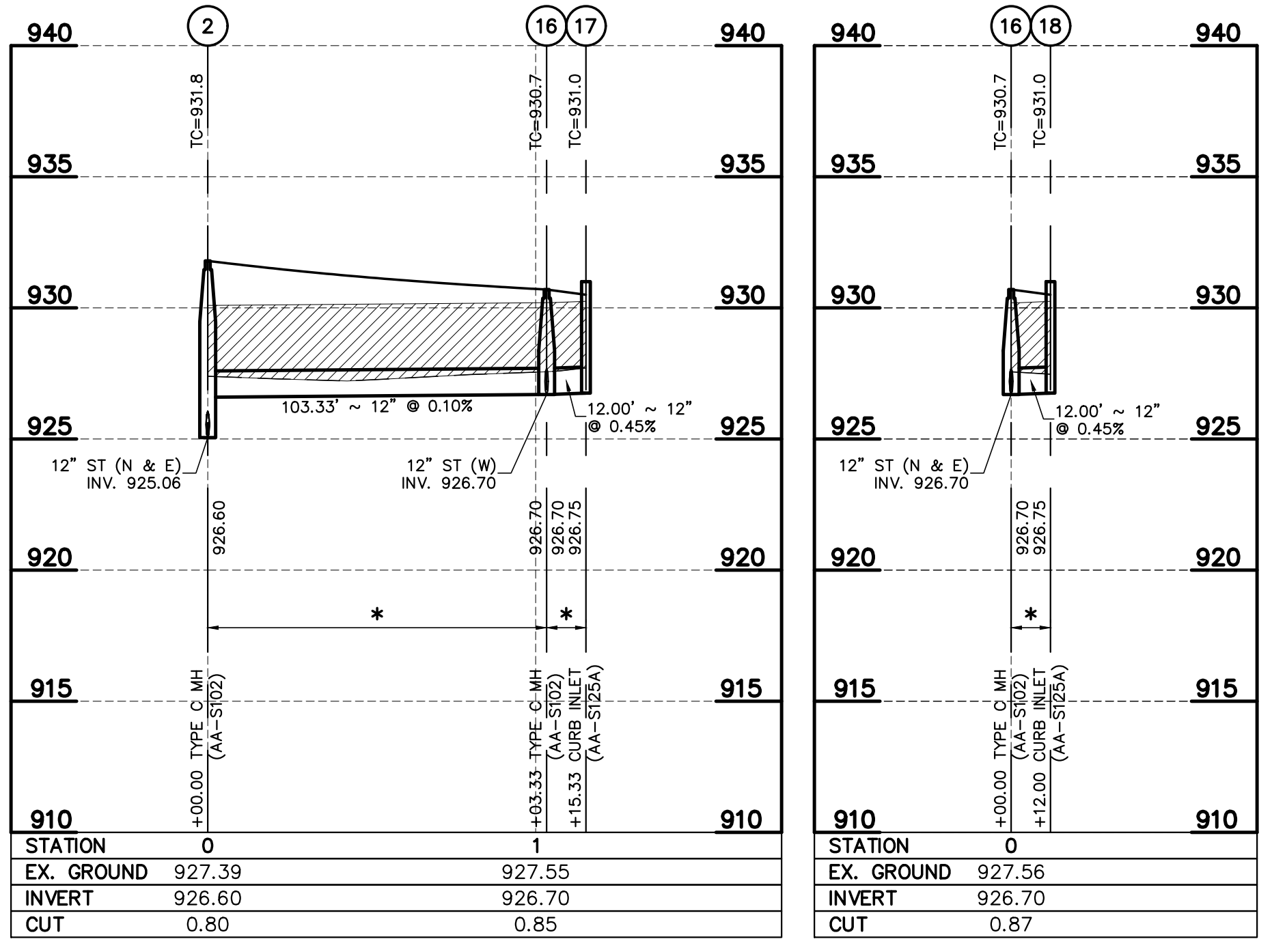
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MATCHLINE, STA: 9+75.00. SEE BELOW



MATCHLINE, STA: 9+75.00. SEE ABOVE



**PROFILE VERTICAL DATUM = NAVD 88**

⊕ : SANITARY GRADE PIPE W/ WATERTIGHT JOINT FOR I/I CROSSING

ALL TRENCH BACKFILL SHALL BE COMPACTED BACKFILL TO 95% OF MAXIMUM DRY WEIGHT AT OPTIMUM MOISTURE CONTENT (±2%) UNLESS NOTED AS:

\* : COMPACTED BACKFILL, ITEM 911

\*\* : COMPACTED GRANULAR BACKFILL, ITEM 912

/// DENOTES AREA TO BE FILLED PRIOR TO INSTALLATION OF STORM SEWER PER ITEM 203 CMSC

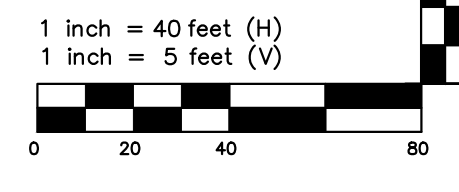
NOTE:

PER CMSC ITEM 911, WHERE NATURAL MATERIAL DOES NOT MEET THE REQUIREMENTS OF ITEM 911, THEN COMPACTED GRANULAR BACKFILL, ITEM 912, SHALL BE USED.

ITEM 912 IS TO BE USED IN R/W PER 2179 STD. DWG.

F.V. = ELEVATION AND LOCATION FIELD VERIFIED

\*\*\* = MUST MAINTAIN A MIN. 18" CLEARANCE



CITY OF DUBLIN, OHIO  
 PRELIMINARY DEVELOPMENT PLAN  
 STORM SEWER PROFILES  
 FOR  
 OSU MEDICAL CAMPUS PUD

PLAN PREPARED BY:

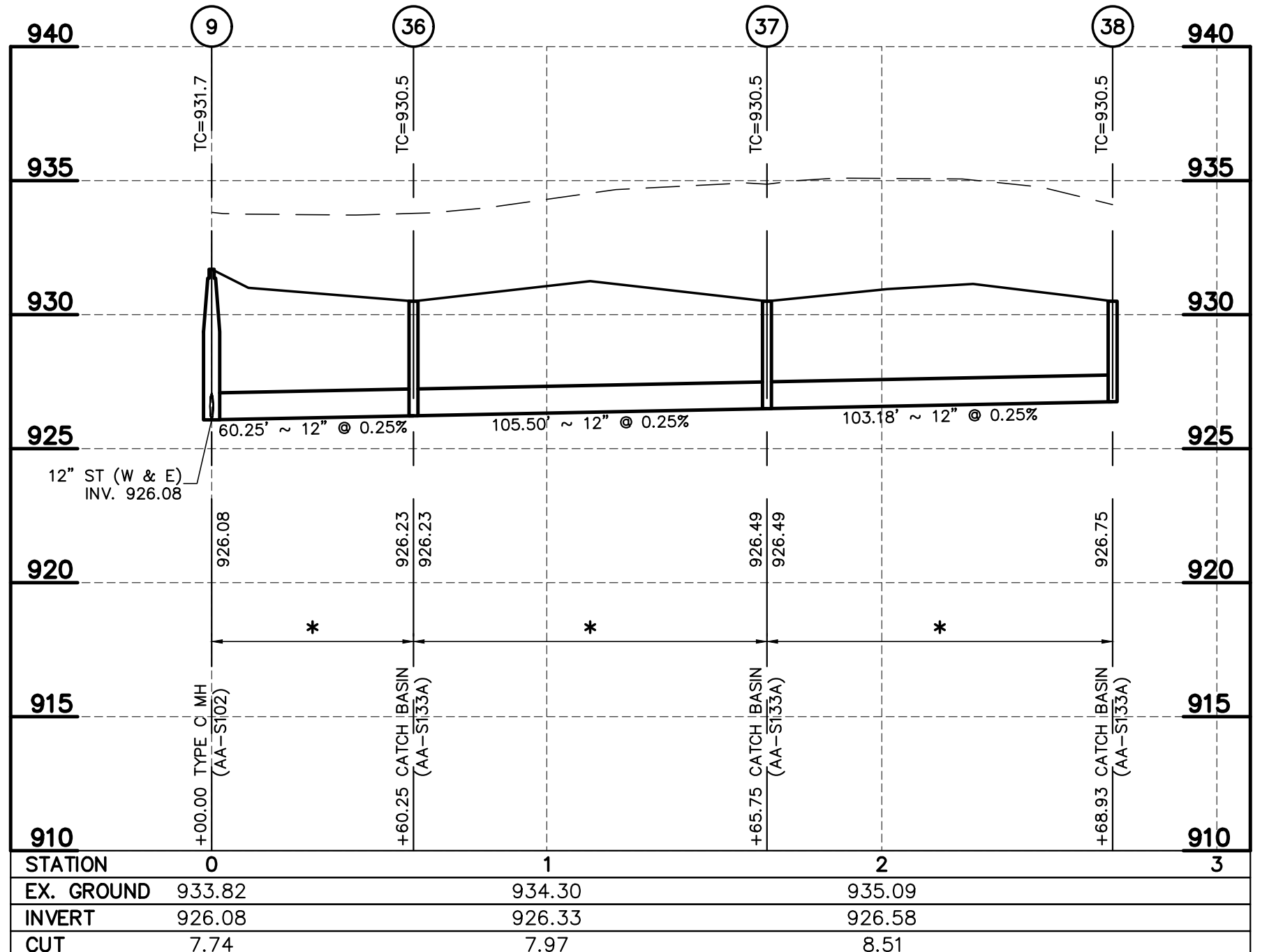
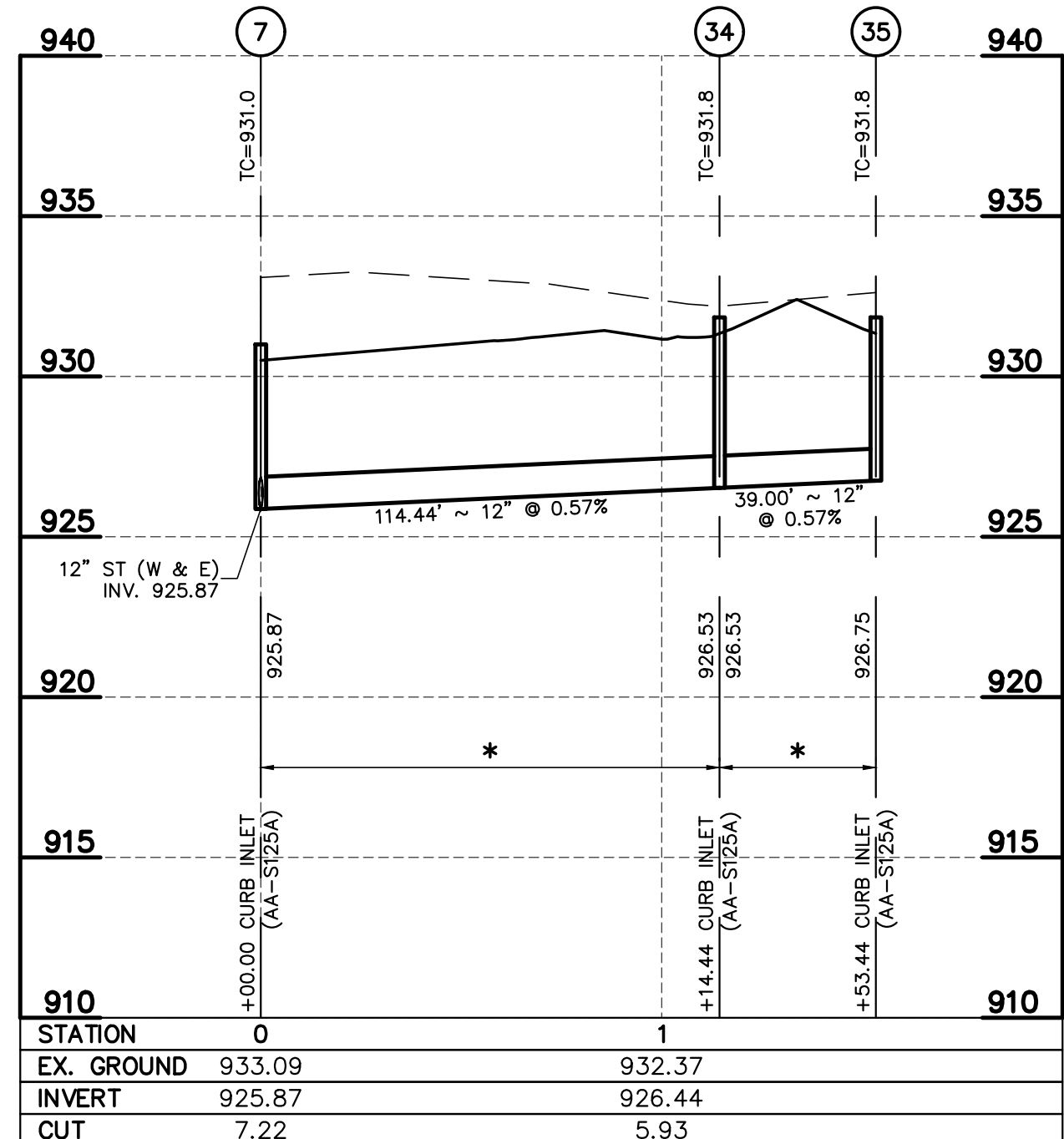
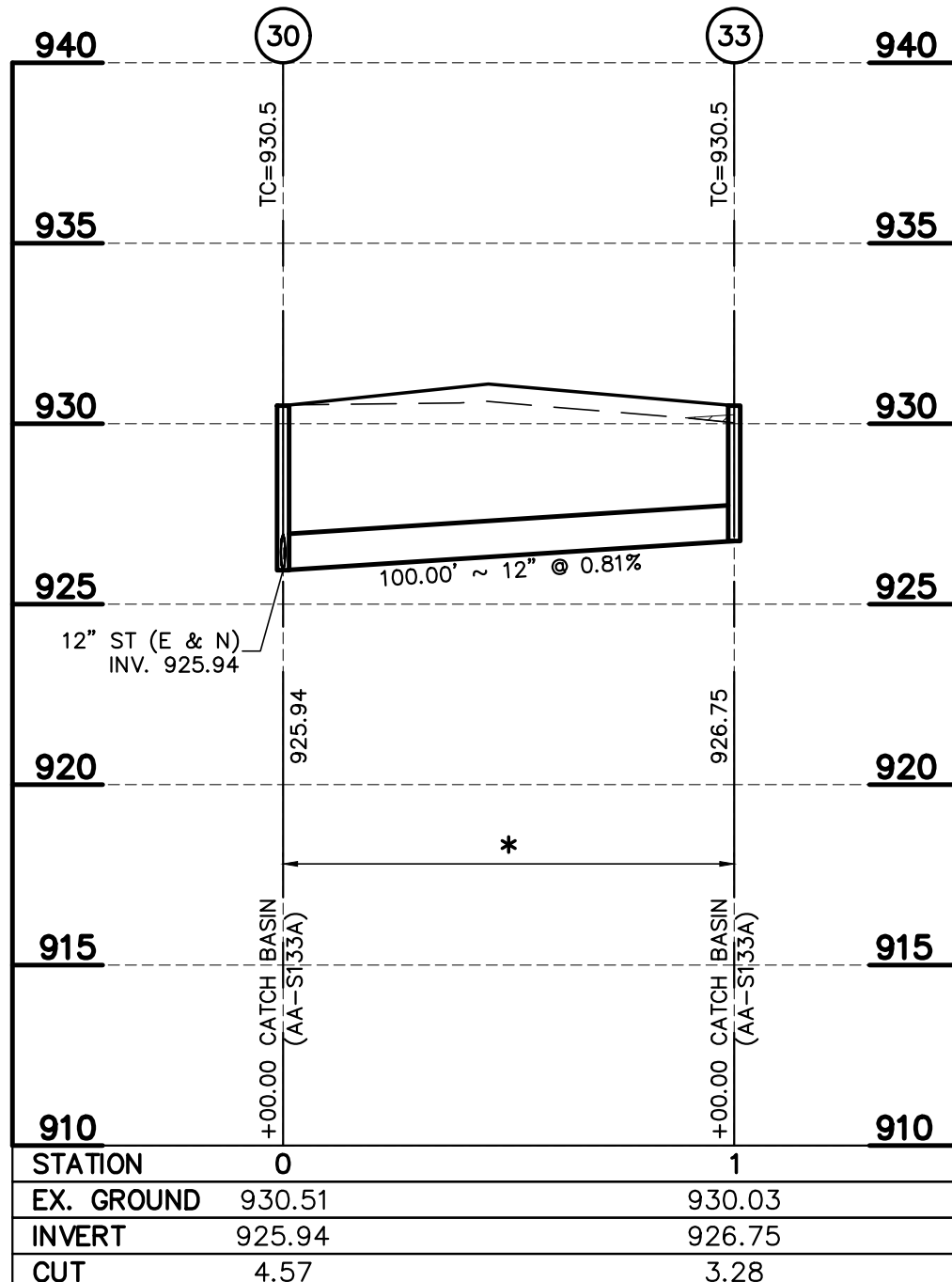
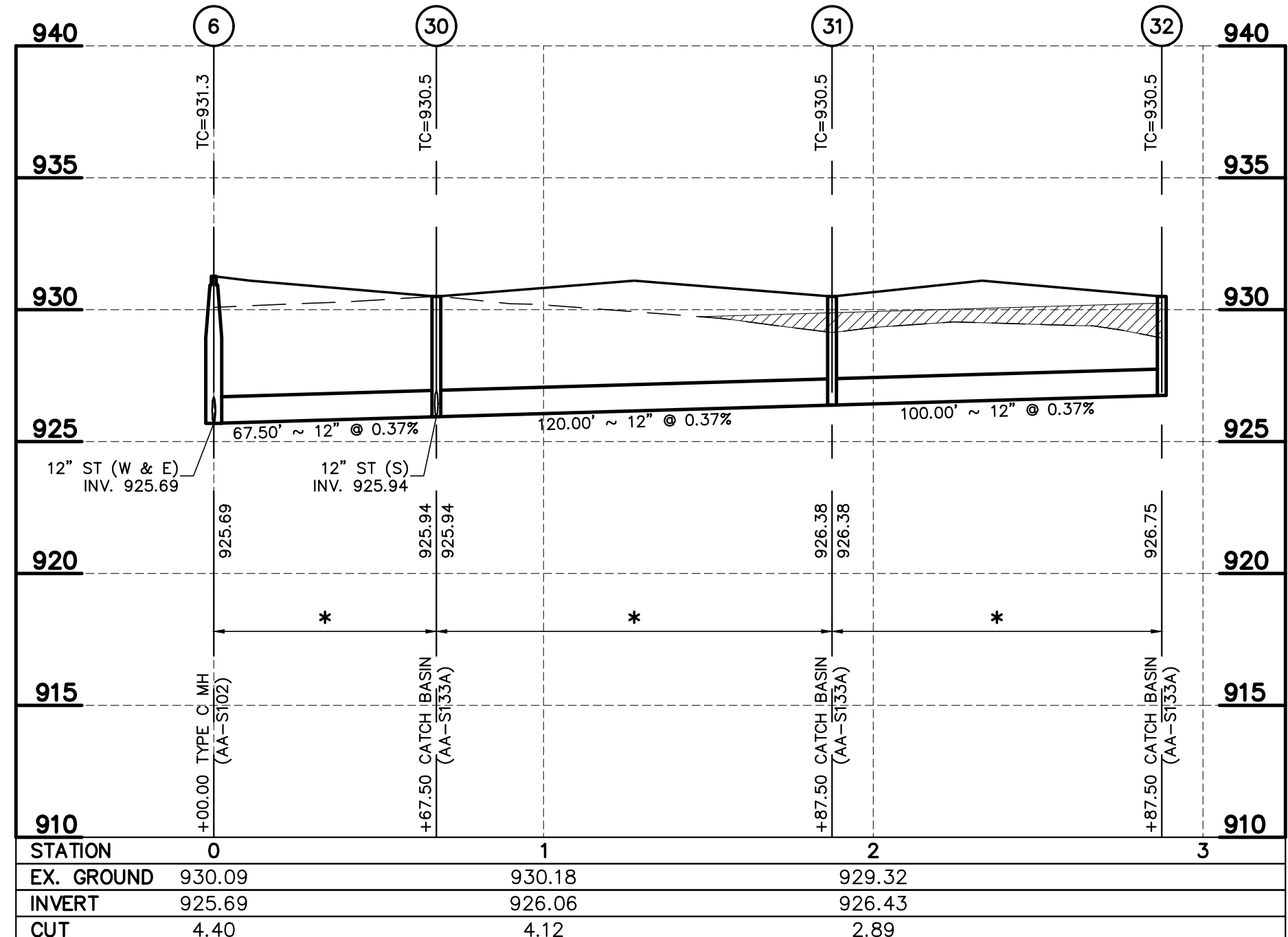
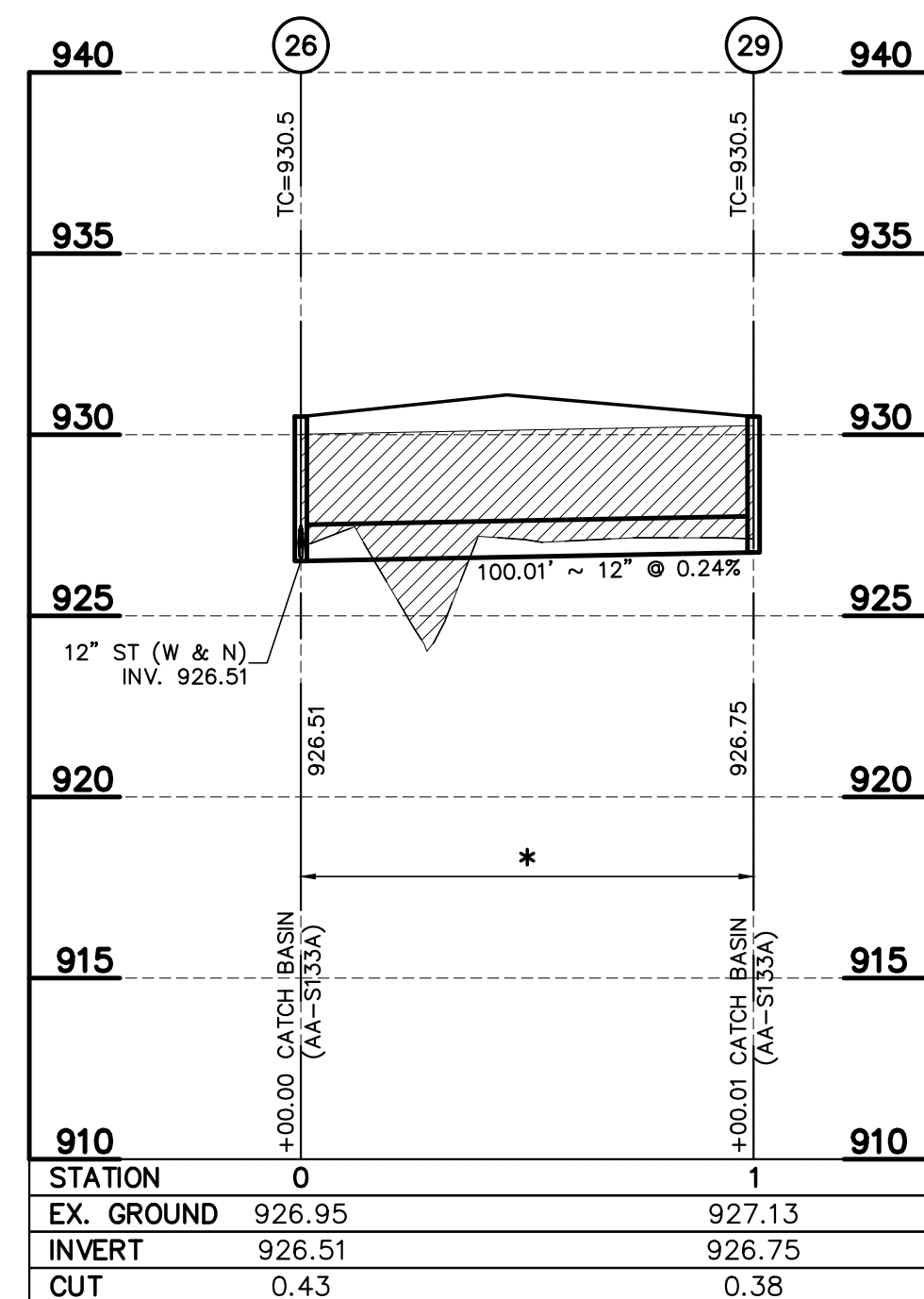
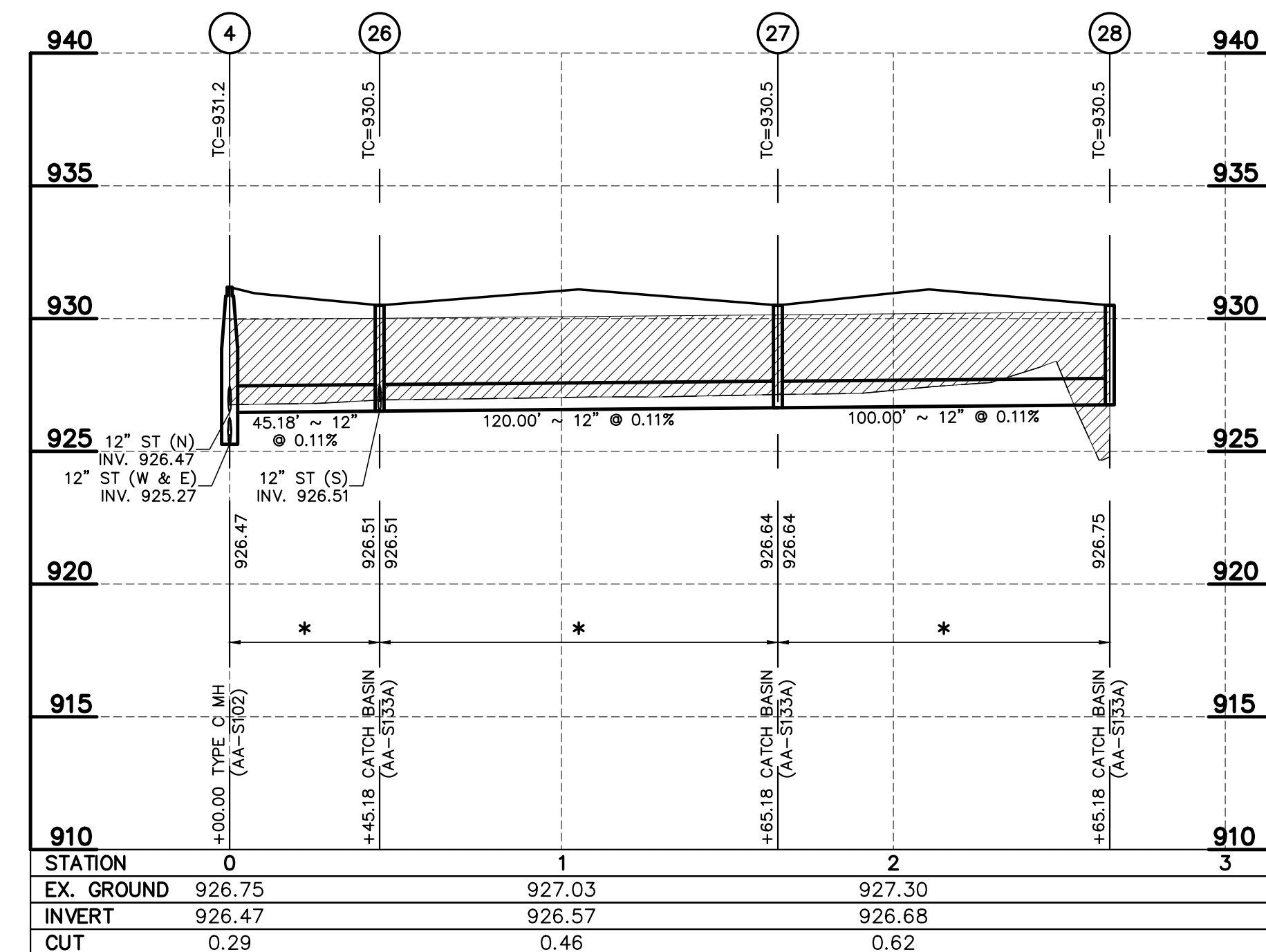
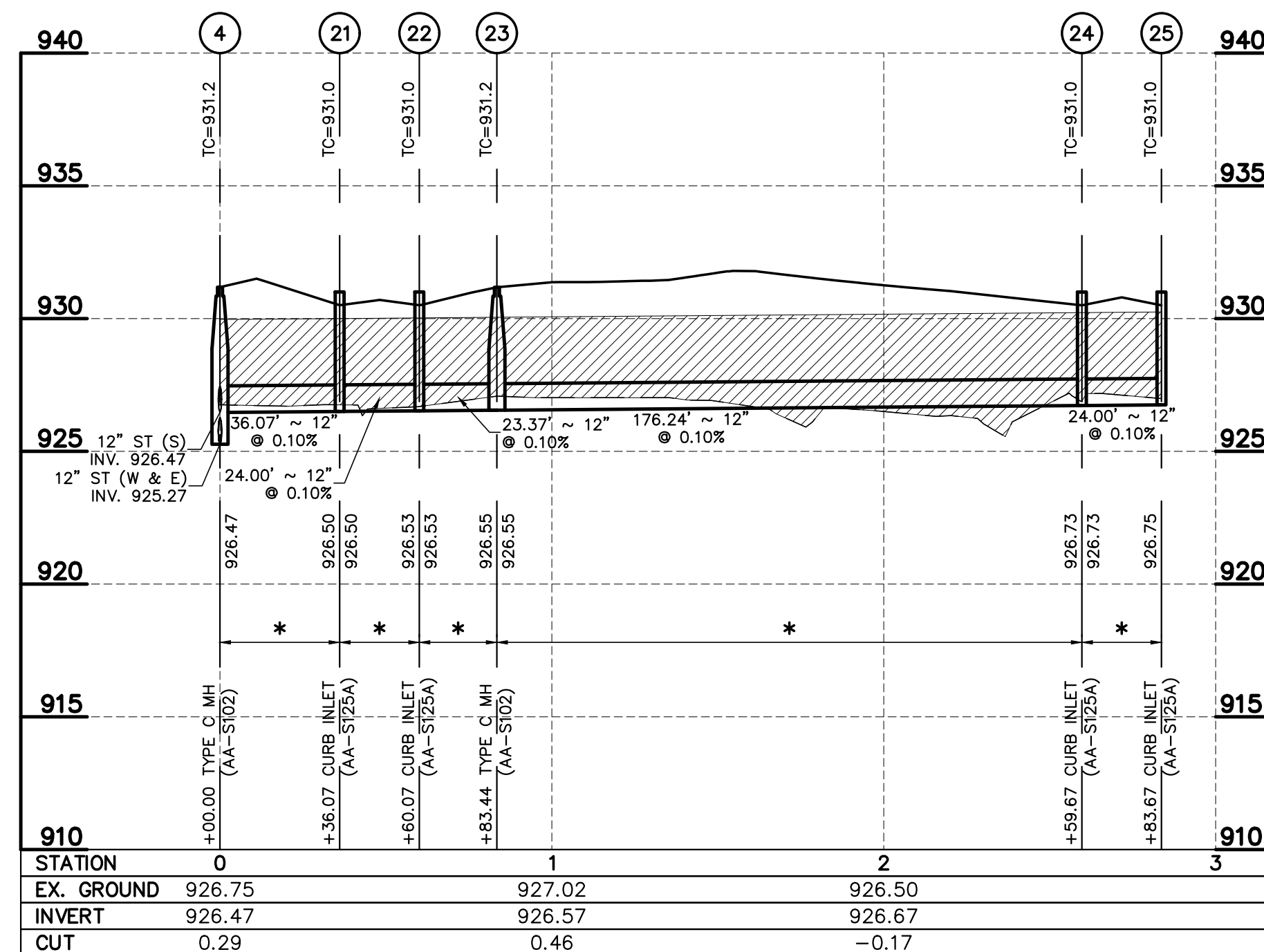
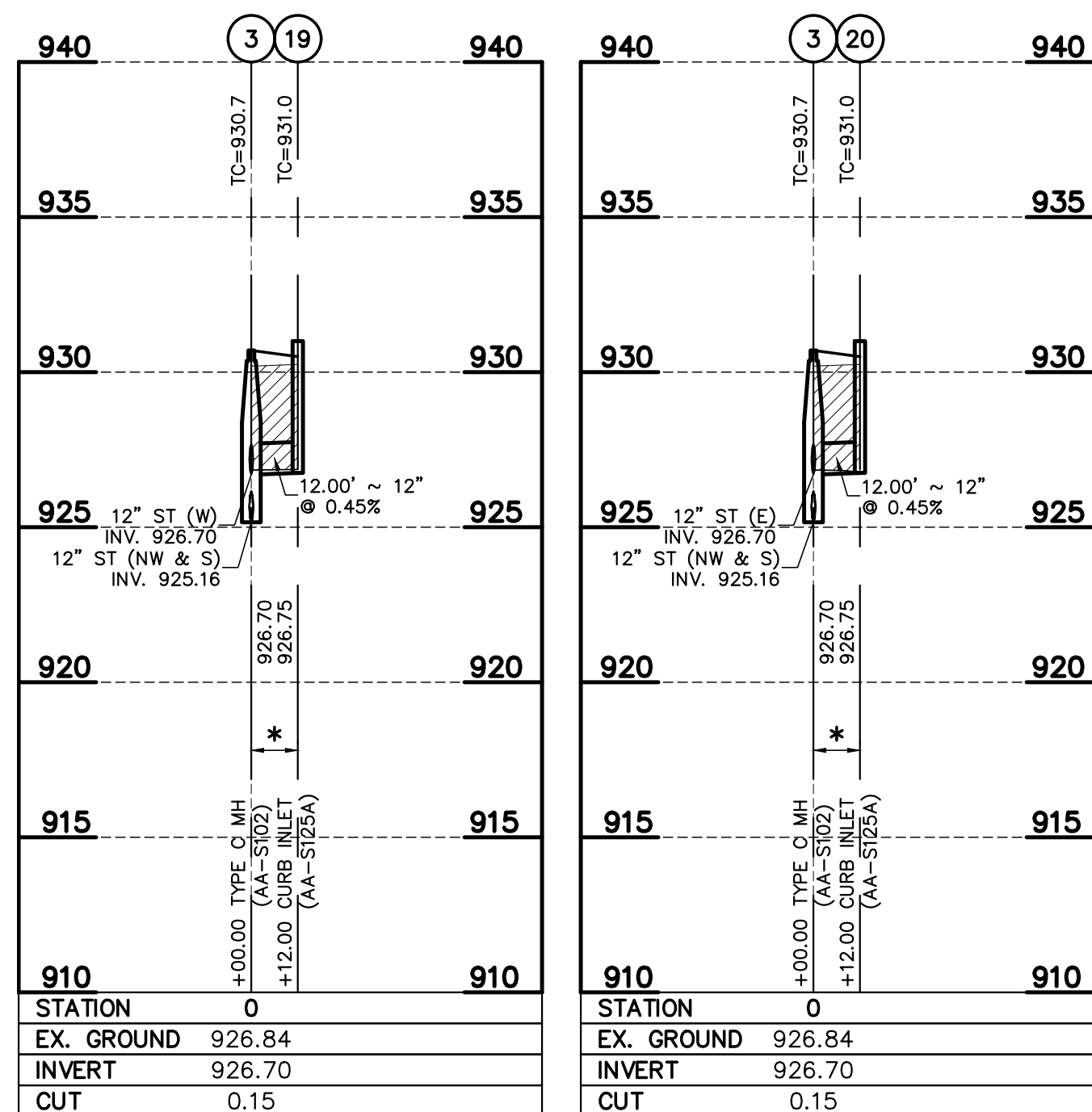
422 Beecher Road  
 Gahanna, Ohio 43230  
 ph 614.428.7750  
 fax 614.428.7755

ENGINEERS SURVEYORS

SCALE: 1" = 40' HORIZ.  
 1" = 5' VERT.  
 DATE: 08/29/2019

SHEET 10 / 15

Z:\18-0003-114\DWG\PRODUCTION DRAWINGS\PRE DEVELOPMENT PLAN\003-114\_PDP\_Grading.dwg PROFILE (2) Aug 29, 2019 - 7:09:56am rflower



PROFILE VERTICAL DATUM = NAVD 88

⊕ : SANITARY GRADE PIPE W/ WATERTIGHT JOINT FOR 1/1 CROSSING

ALL TRENCH BACKFILL SHALL BE COMPACTED BACKFILL TO 95% OF MAXIMUM DRY WEIGHT AT OPTIMUM MOISTURE CONTENT (±2%) UNLESS NOTED AS:

\* : COMPACTED BACKFILL, ITEM 911

\*\* : COMPACTED GRANULAR BACKFILL, ITEM 912

/// DENOTES AREA TO BE FILLED PRIOR TO INSTALLATION OF STORM SEWER PER ITEM 203 CMSC

NOTE:

PER CMSC ITEM 911, WHERE NATURAL MATERIAL DOES NOT MEET THE REQUIREMENTS OF ITEM 911, THEN COMPACTED GRANULAR BACKFILL, ITEM 912, SHALL BE USED.

ITEM 912 IS TO BE USED IN R/W PER 2179 STD. DWG.

F.V. = ELEVATION AND LOCATION FIELD VERIFIED

\*\*\* = MUST MAINTAIN A MIN. 18" CLEARANCE

1 inch = 40 feet (H)  
1 inch = 5 feet (V)

CITY OF DUBLIN, OHIO  
PRELIMINARY DEVELOPMENT PLAN  
STORM SEWER PROFILES  
FOR  
OSU MEDICAL CAMPUS PUD

PLAN PREPARED BY:

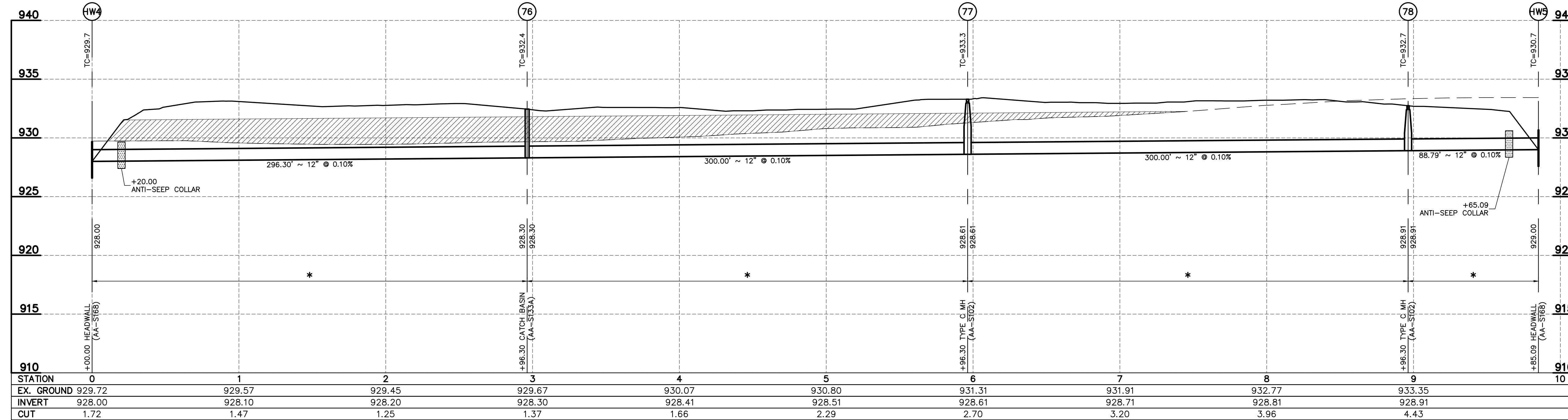
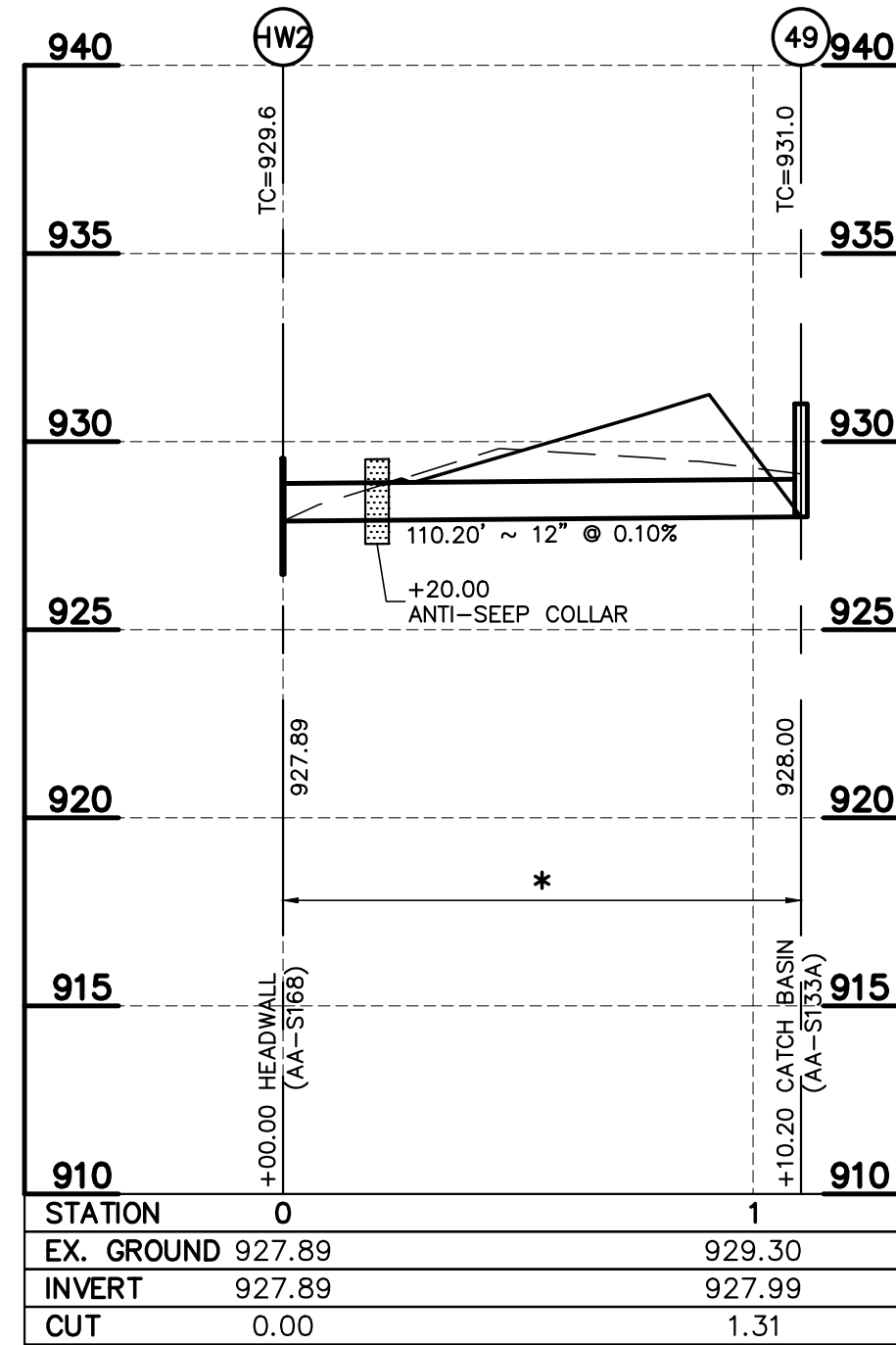
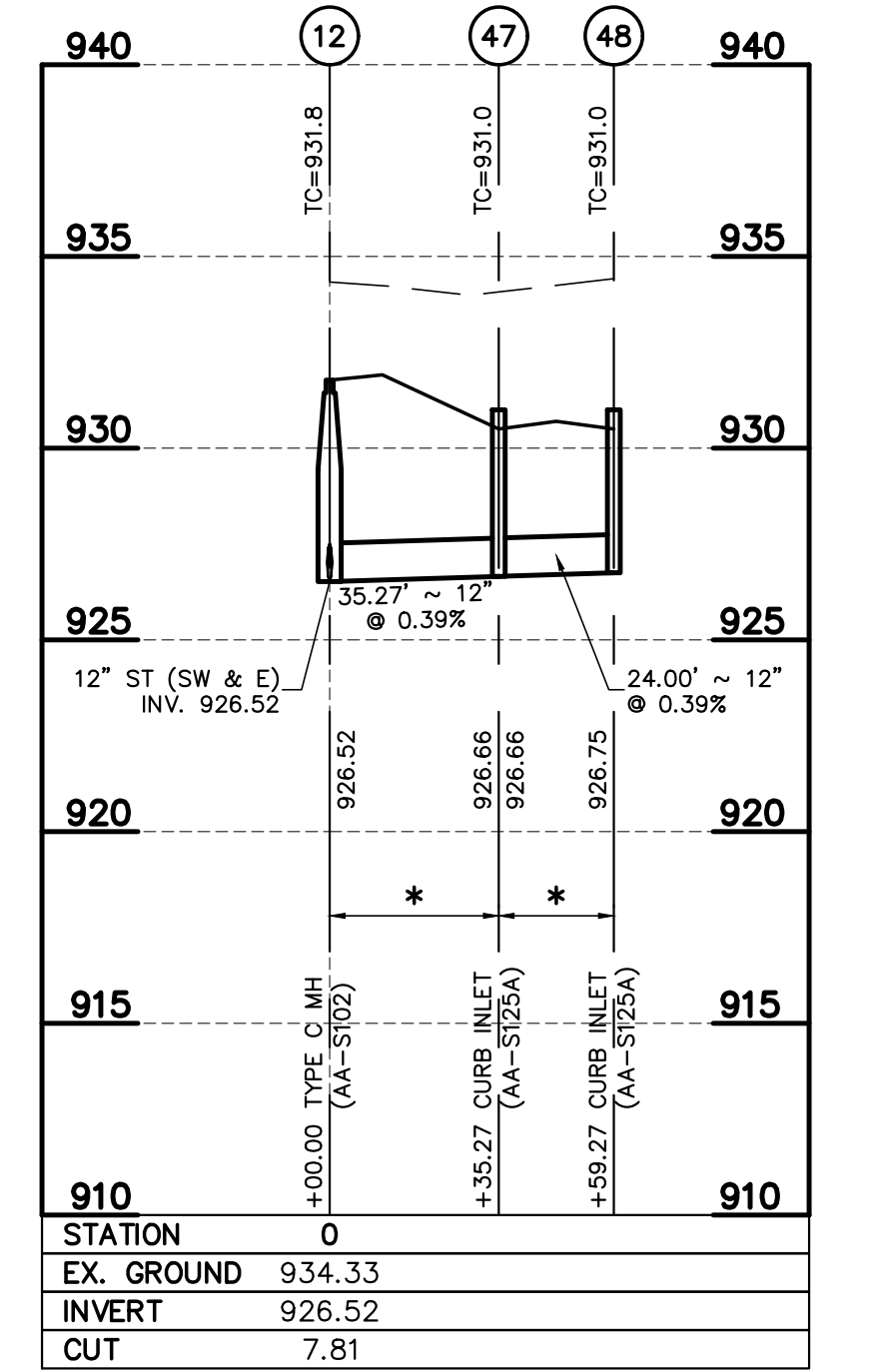
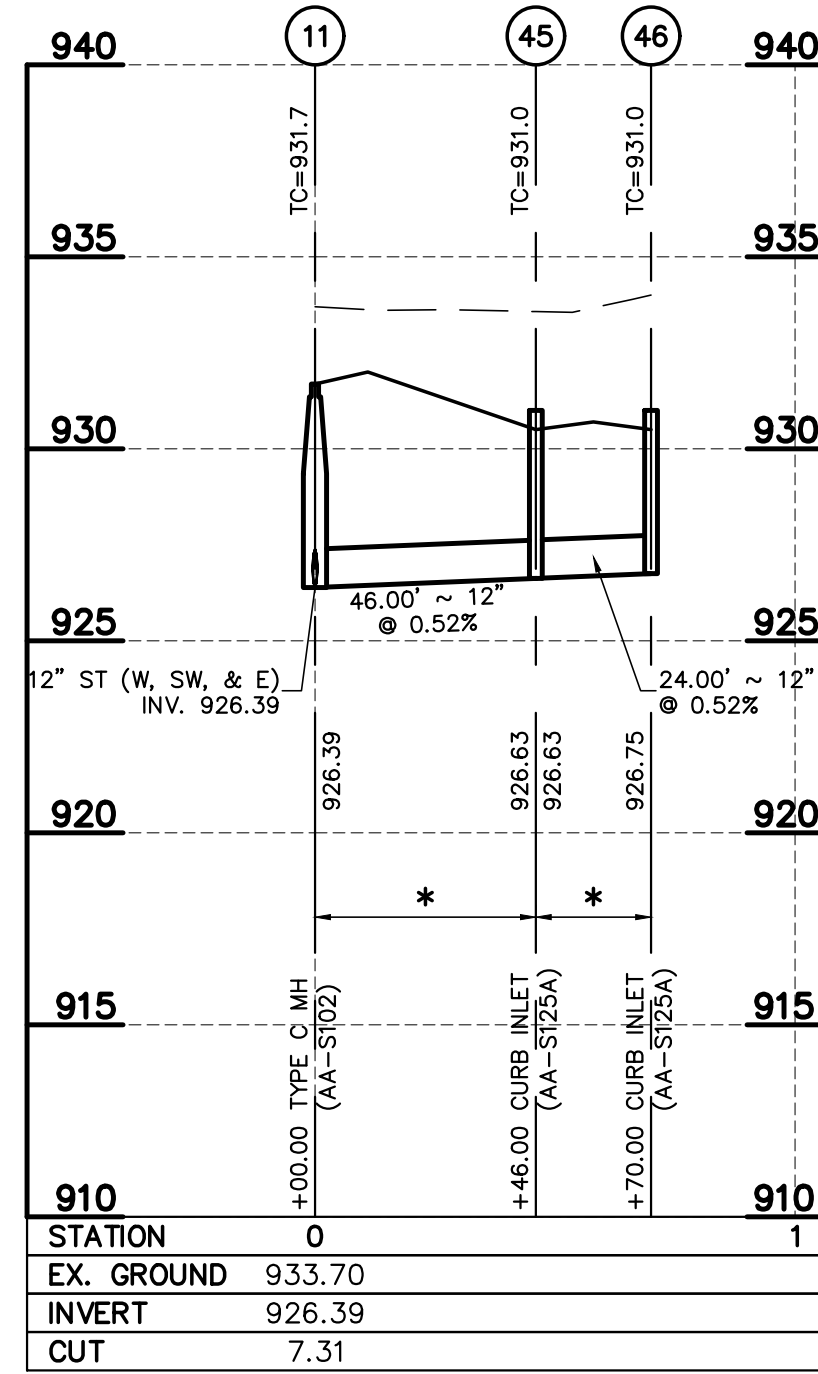
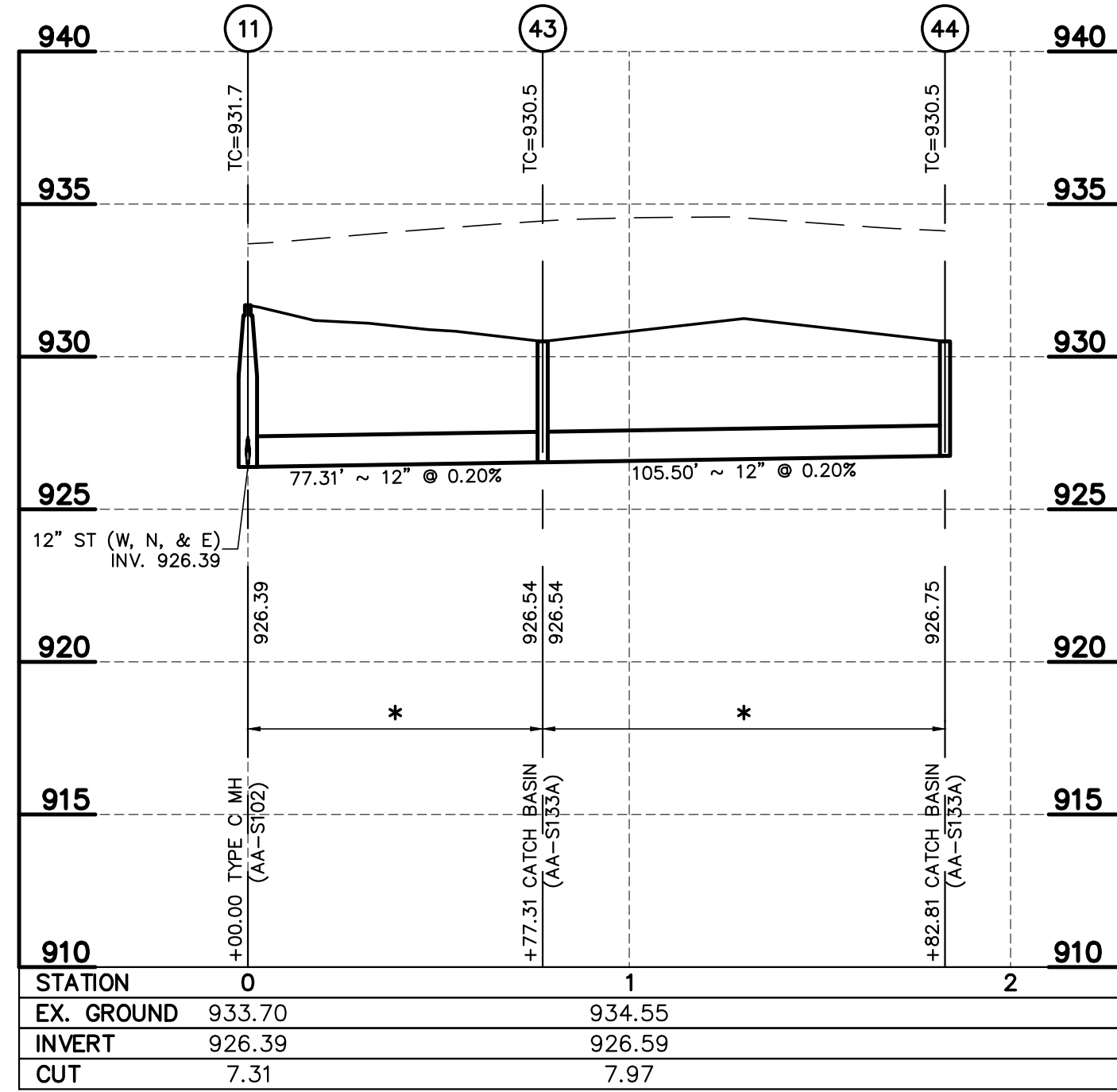
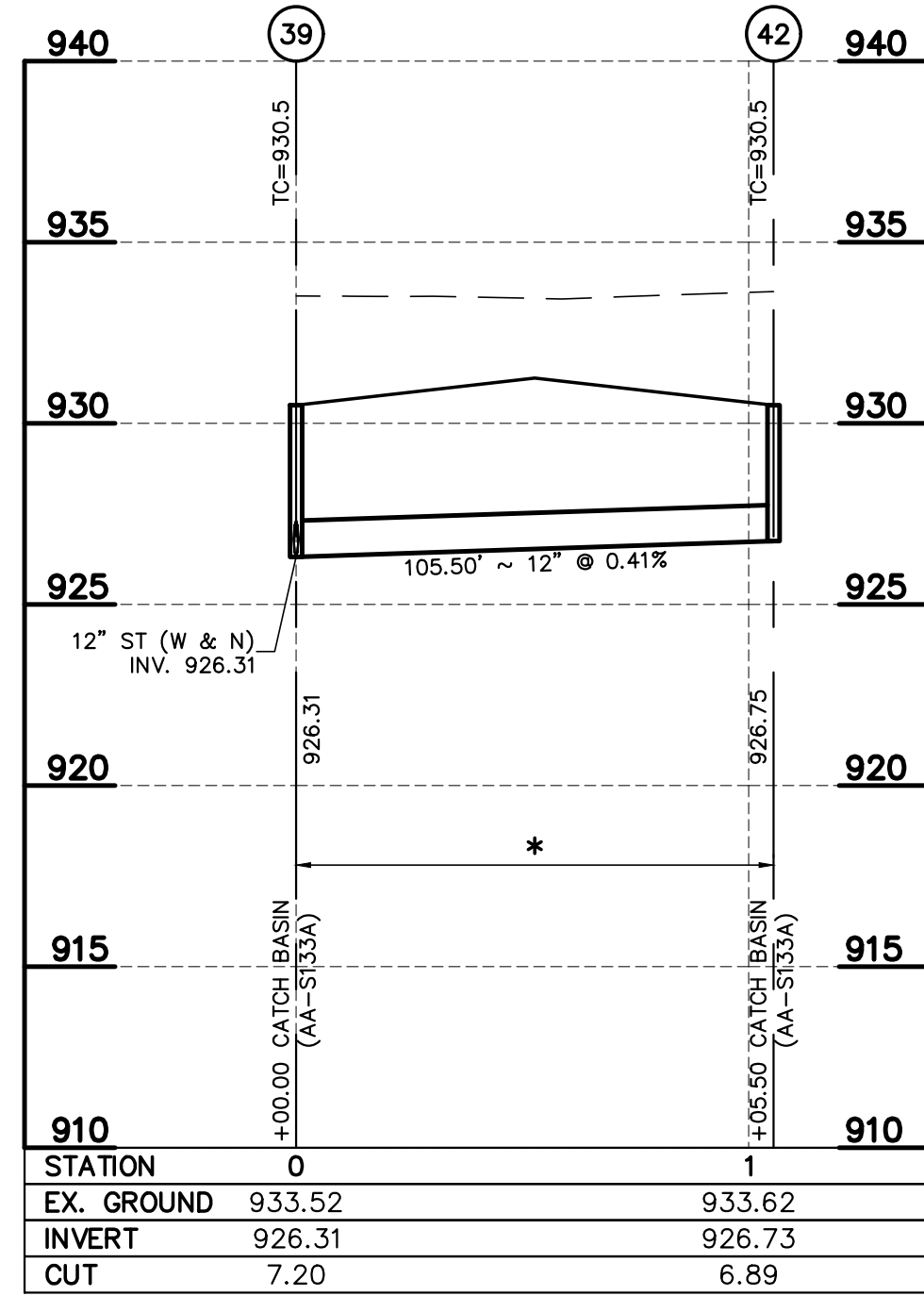
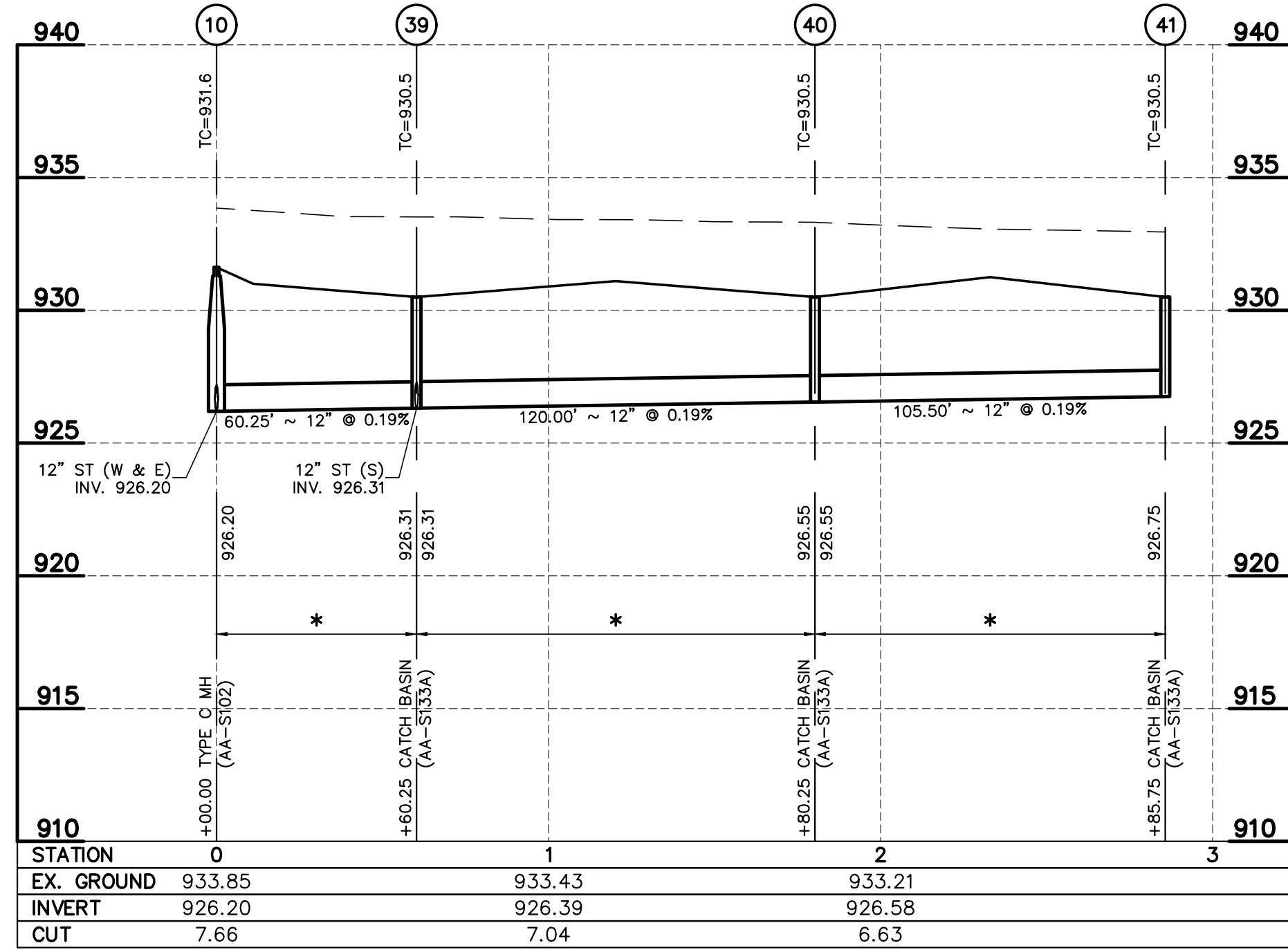
422 Beecher Road  
Gahanna, Ohio 43230  
ph 614.428.7750  
fax 614.428.7755

ENGINEERS SURVEYORS

SCALE: 1" = 40' HORIZ.  
1" = 5' VERT.  
DATE: 08/29/2019

SHEET 11 / 15

Z:\18-0003-114\DWG\PRODUCTION DRAWINGS\PRE DEVELOPMENT PLAN\0003-114\_PDP\_Granding.dwg PROFILE (3) Aug 29, 2019 - 7:10:01am rfwiler



**PROFILE VERTICAL DATUM = NAVD 88**

± : SANITARY GRADE PIPE W/ WATERTIGHT JOINT FOR 1/1 CROSSING

ALL TRENCH BACKFILL SHALL BE COMPACTED BACKFILL TO 95% OF MAXIMUM DRY WEIGHT AT OPTIMUM MOISTURE CONTENT (±2%) UNLESS NOTED AS:

\* : COMPACTED BACKFILL, ITEM 911

\*\* : COMPACTED GRANULAR BACKFILL, ITEM 912

/// DENOTES AREA TO BE FILLED PRIOR TO INSTALLATION OF STORM SEWER PER ITEM 203 CMSC

NOTE:

PER CMSC ITEM 911, WHERE NATURAL MATERIAL DOES NOT MEET THE REQUIREMENTS OF ITEM 911, THEN COMPACTED GRANULAR BACKFILL, ITEM 912, SHALL BE USED.

ITEM 912 IS TO BE USED IN R/W PER 2179 STD. DWG.

F.V. = ELEVATION AND LOCATION FIELD VERIFIED

\*\*\* = MUST MAINTAIN A MIN. 18" CLEARANCE

1 inch = 40 feet (H)  
1 inch = 5 feet (V)

**CITY OF DUBLIN, OHIO**  
**PRELIMINARY DEVELOPMENT PLAN**  
**STORM SEWER PROFILES**  
**FOR**  
**OSU MEDICAL CAMPUS PUD**

PLAN PREPARED BY:

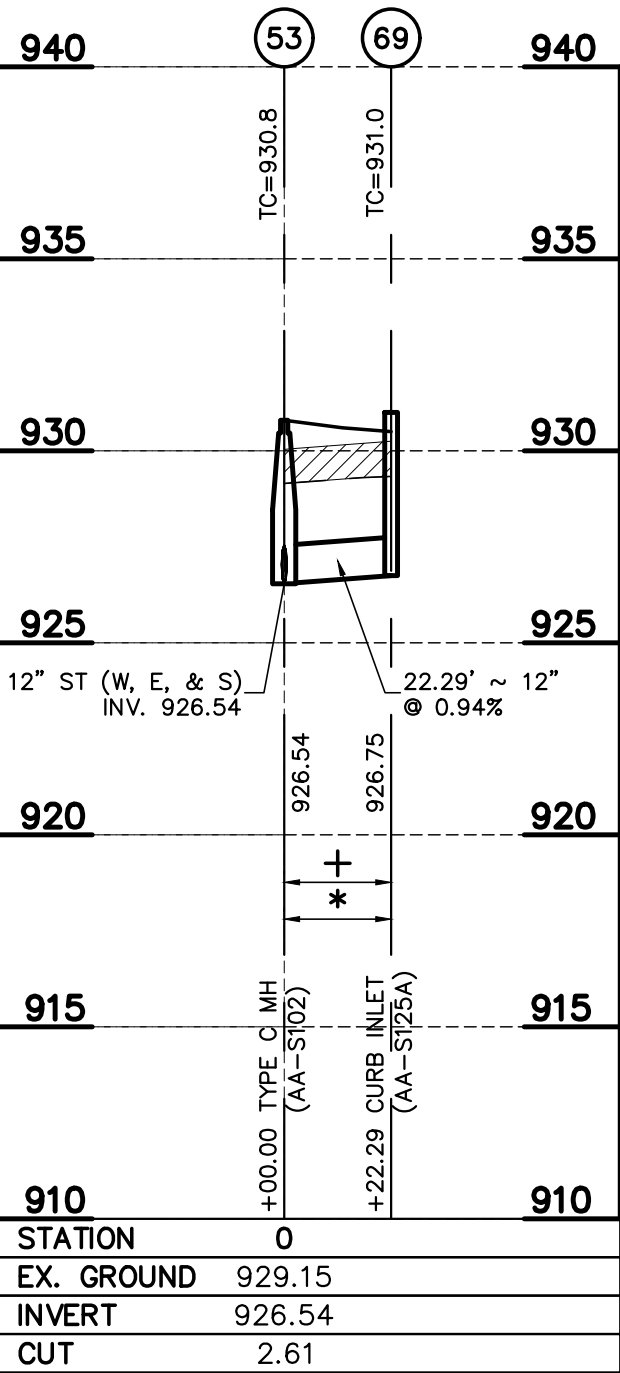
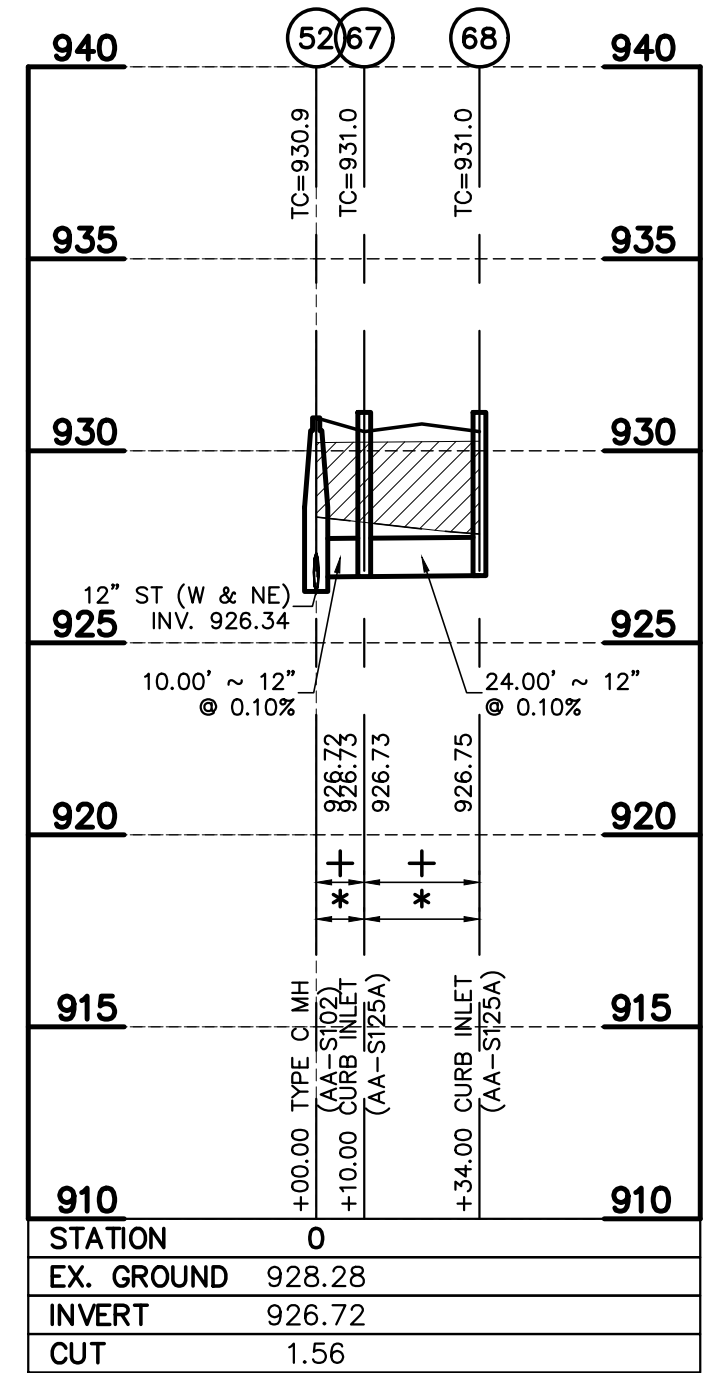
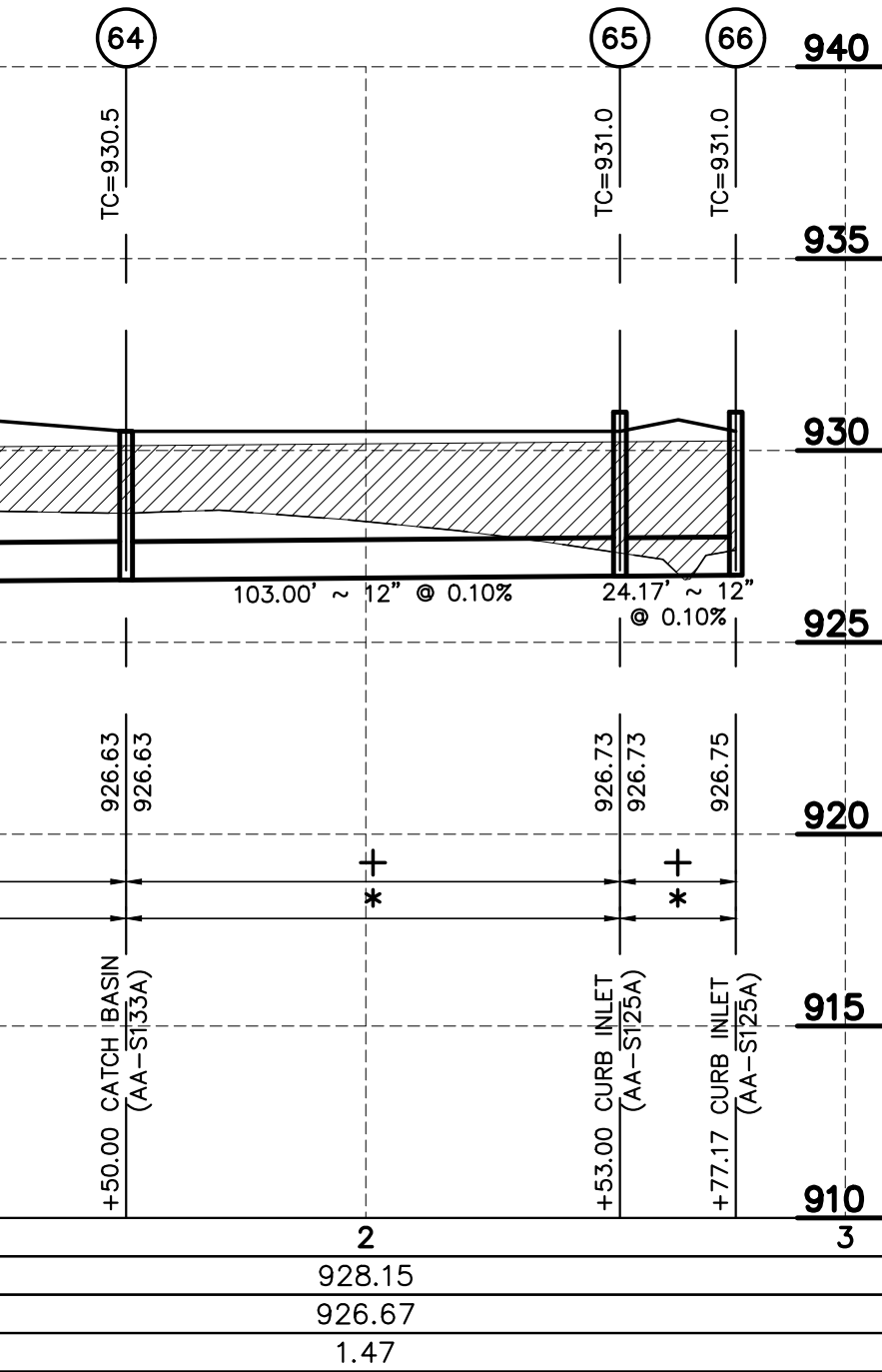
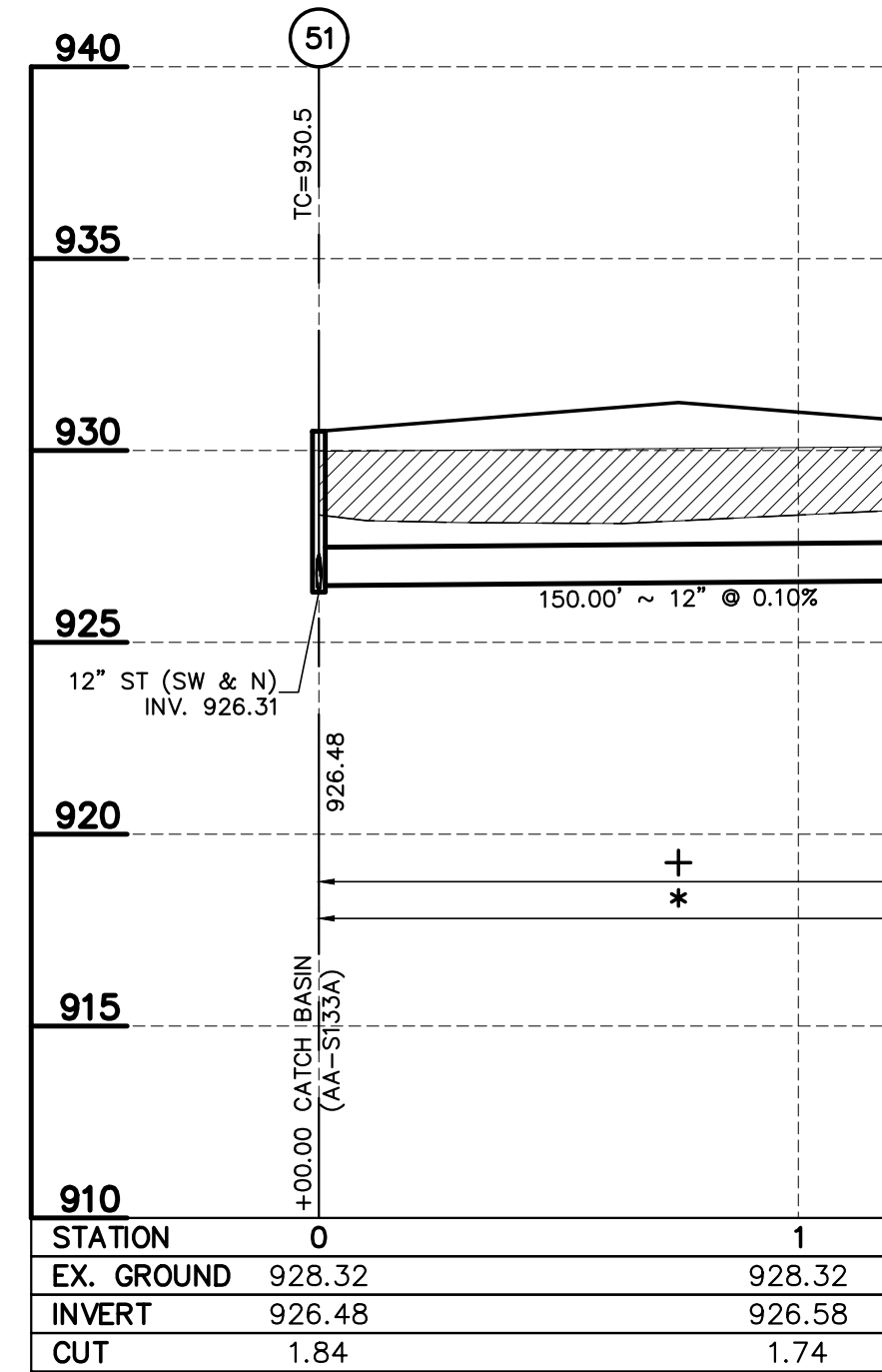
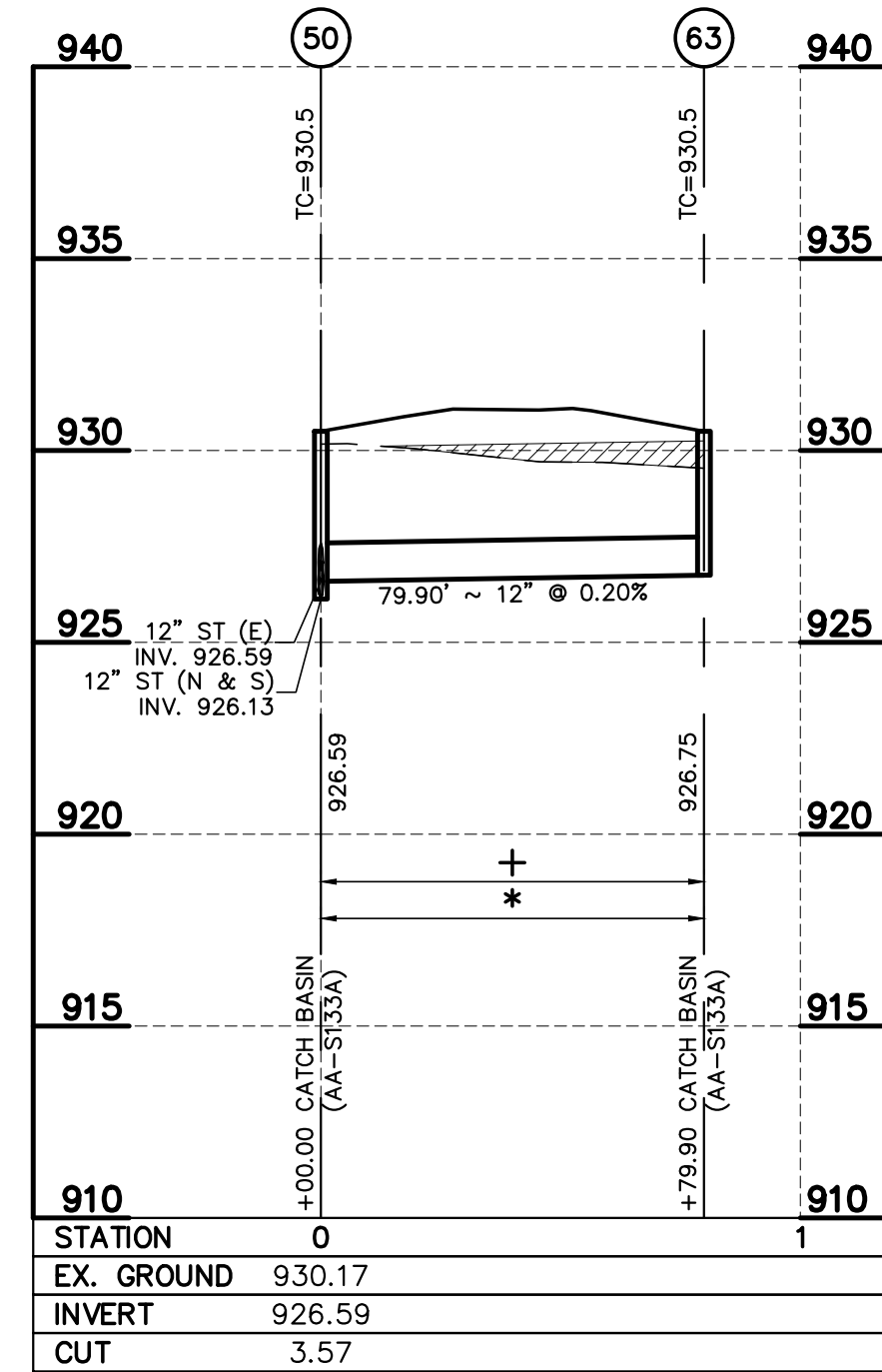
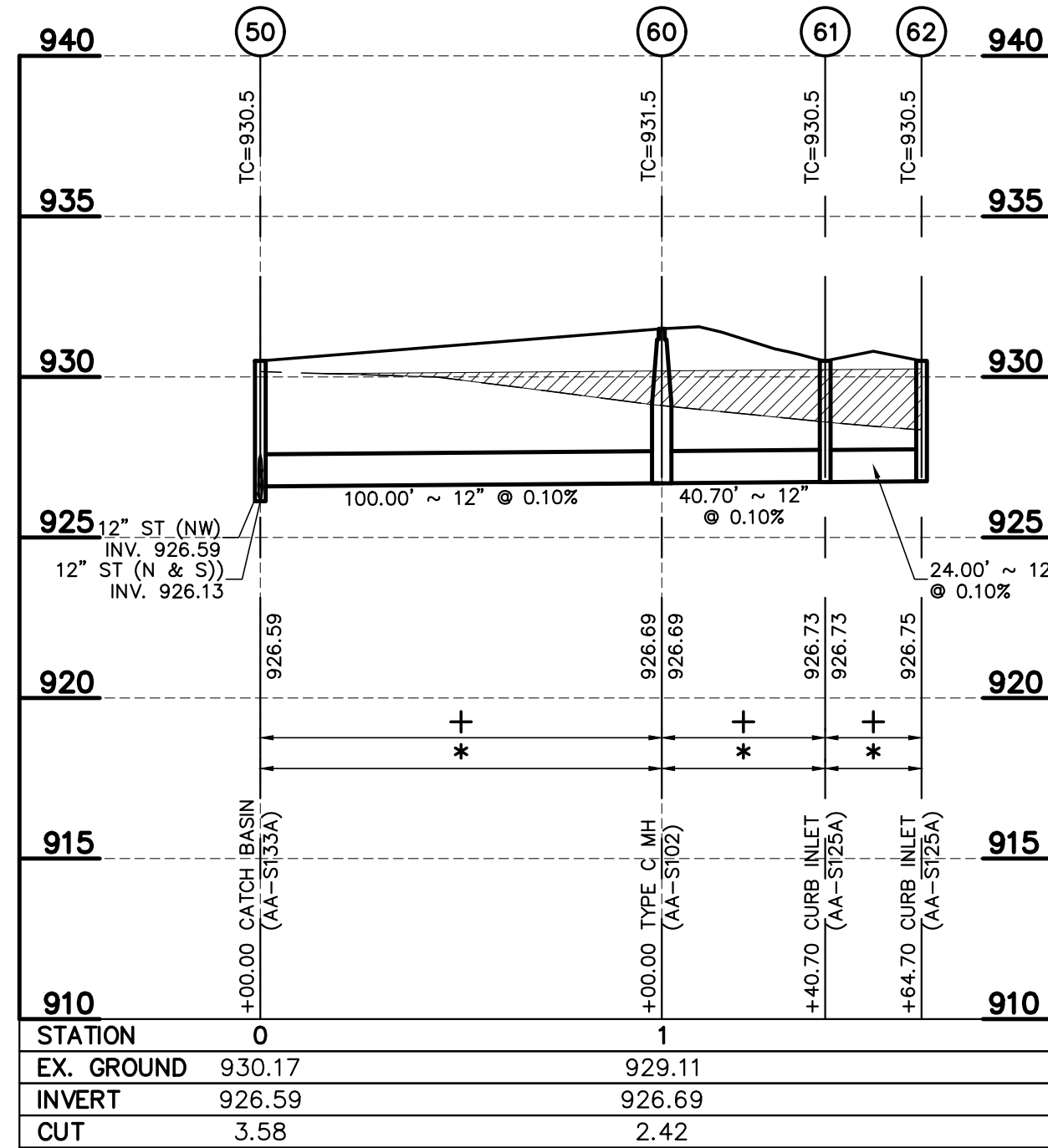
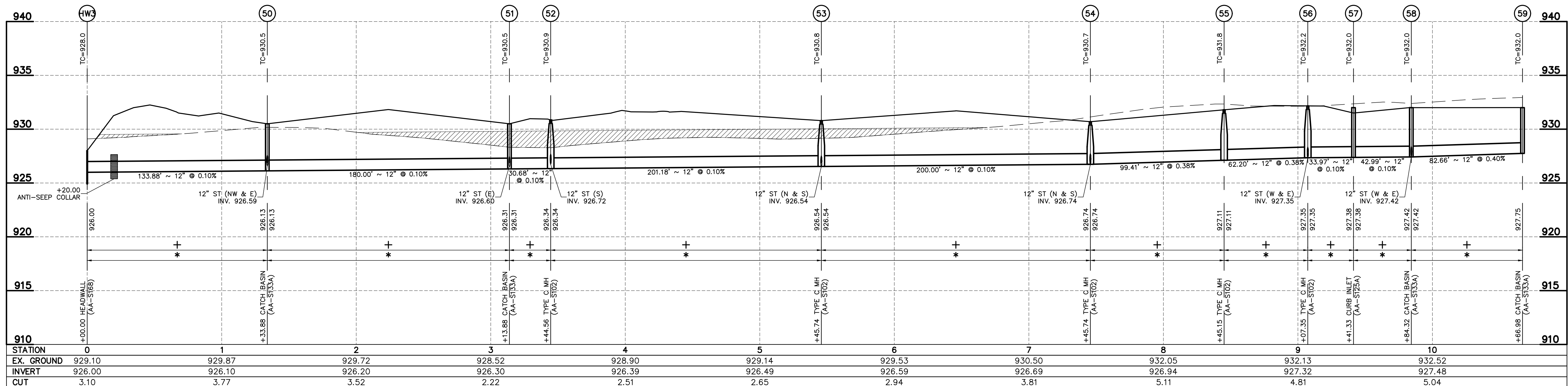
422 Beecher Road  
Gahanna, Ohio 43230  
ph 614.428.7750  
fax 614.428.7755

**ADVANCED**  
**CIVIL DESIGN**  
**ENGINEERS SURVEYORS**

SCALE: 1" = 40' HORIZ.  
1" = 5' VERT.  
DATE: 08/29/2019

SHEET 12 / 15

Z:\18-0003-114\DWG\PRODUCTION DRAWINGS\PRE DEVELOPMENT PLAN\003-114\_PDP\_Sewer.dwg Aug 29, 2019 - 7:10:06am rfwlwr



**PROFILE VERTICAL DATUM = NAVD 88**

⊕ : SANITARY GRADE PIPE W/ WATERTIGHT JOINT FOR 1/1 CROSSING

ALL TRENCH BACKFILL SHALL BE COMPACTED BACKFILL TO 95% OF MAXIMUM DRY WEIGHT AT OPTIMUM MOISTURE CONTENT (±2%) UNLESS NOTED AS:

\* : COMPACTED BACKFILL, ITEM 911

\*\* : COMPACTED GRANULAR BACKFILL, ITEM 912

/// DENOTES AREA TO BE FILLED PRIOR TO INSTALLATION OF STORM SEWER PER ITEM 203 CMSC

NOTE:

PER CMSC ITEM 911, WHERE NATURAL MATERIAL DOES NOT MEET THE REQUIREMENTS OF ITEM 911, THEN COMPACTED GRANULAR BACKFILL, ITEM 912, SHALL BE USED.

ITEM 912 IS TO BE USED IN R/W PER 2179 STD. DWG.

F.V. = ELEVATION AND LOCATION FIELD VERIFIED

\*\*\* = MUST MAINTAIN A MIN. 18" CLEARANCE

1 inch = 40 feet (H)  
1 inch = 5 feet (V)

CITY OF DUBLIN, OHIO  
PRELIMINARY DEVELOPMENT PLAN  
STORM SEWER PROFILES  
FOR  
OSU MEDICAL CAMPUS PUD

PLAN PREPARED BY:

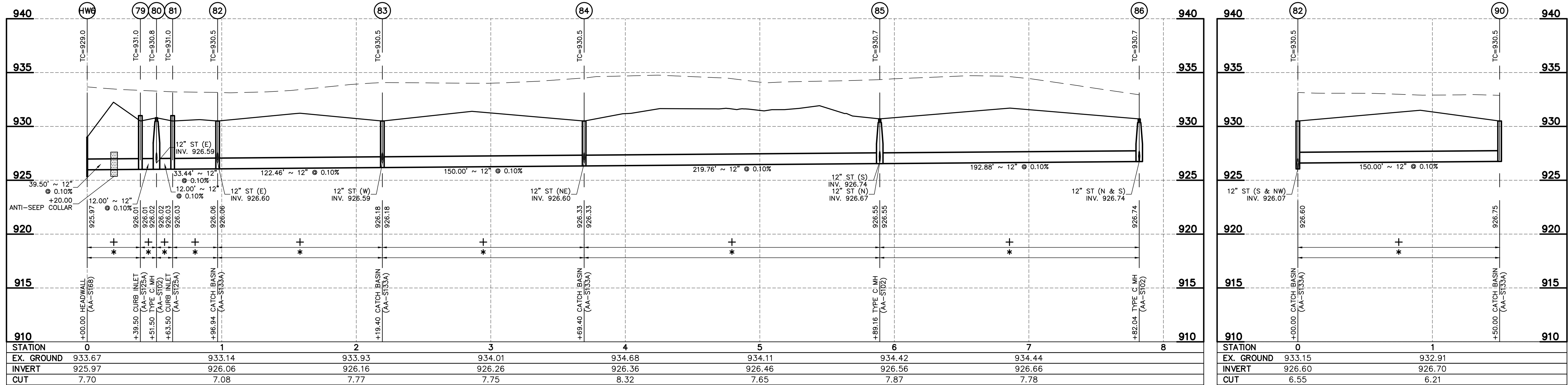
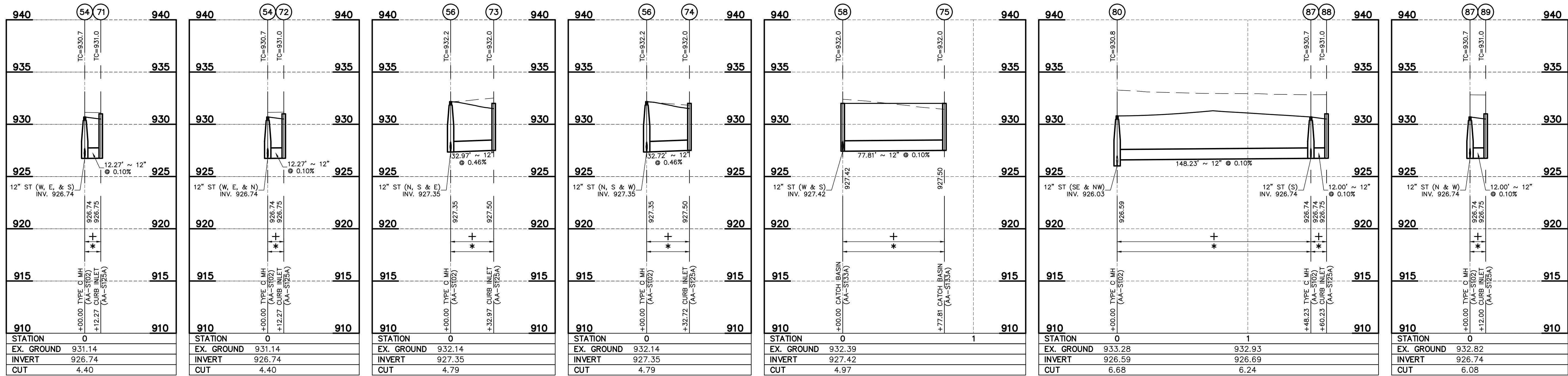
**ADVANCED CIVIL DESIGN**  
ENGINEERS SURVEYORS

422 Beecher Road  
Gahanna, Ohio 43230  
ph 614.428.7750  
fax 614.428.7755

SCALE: 1" = 40' HORIZ.  
1" = 5' VERT.  
DATE: 08/29/2019

SHEET 13 / 15

Z:\18-0003-114\DWG\PRODUCTION DRAWINGS\PRE DEVELOPMENT PLAN\0003-114\_PDP\_Sewer.dwg PROFILE (S) Aug 29, 2019 - 7:10:11am rfwiler



**PROFILE VERTICAL DATUM = NAVD 88**

⊕ : SANITARY GRADE PIPE W/ WATERTIGHT JOINT FOR I/I CROSSING

ALL TRENCH BACKFILL SHALL BE COMPACTED BACKFILL TO 95% OF MAXIMUM DRY WEIGHT AT OPTIMUM MOISTURE CONTENT (±2%) UNLESS NOTED AS:

\* : COMPACTED BACKFILL, ITEM 911

\*\* : COMPACTED GRANULAR BACKFILL, ITEM 912

/// DENOTES AREA TO BE FILLED PRIOR TO INSTALLATION OF STORM SEWER PER ITEM 203 CMSC

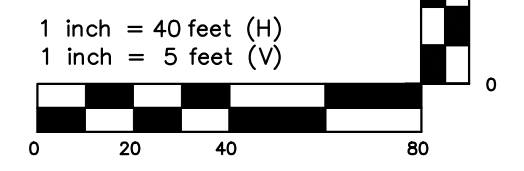
NOTE:

PER CMSC ITEM 911, WHERE NATURAL MATERIAL DOES NOT MEET THE REQUIREMENTS OF ITEM 911, THEN COMPACTED GRANULAR BACKFILL, ITEM 912, SHALL BE USED.

ITEM 912 IS TO BE USED IN R/W PER 2179 STD. DWG.

F.V. = ELEVATION AND LOCATION FIELD VERIFIED

\*\*\* = MUST MAINTAIN A MIN. 18" CLEARANCE



**CITY OF DUBLIN, OHIO**  
**PRELIMINARY DEVELOPMENT PLAN**  
**STORM SEWER PROFILES**  
**FOR**  
**OSU MEDICAL CAMPUS PUD**

PLAN PREPARED BY:

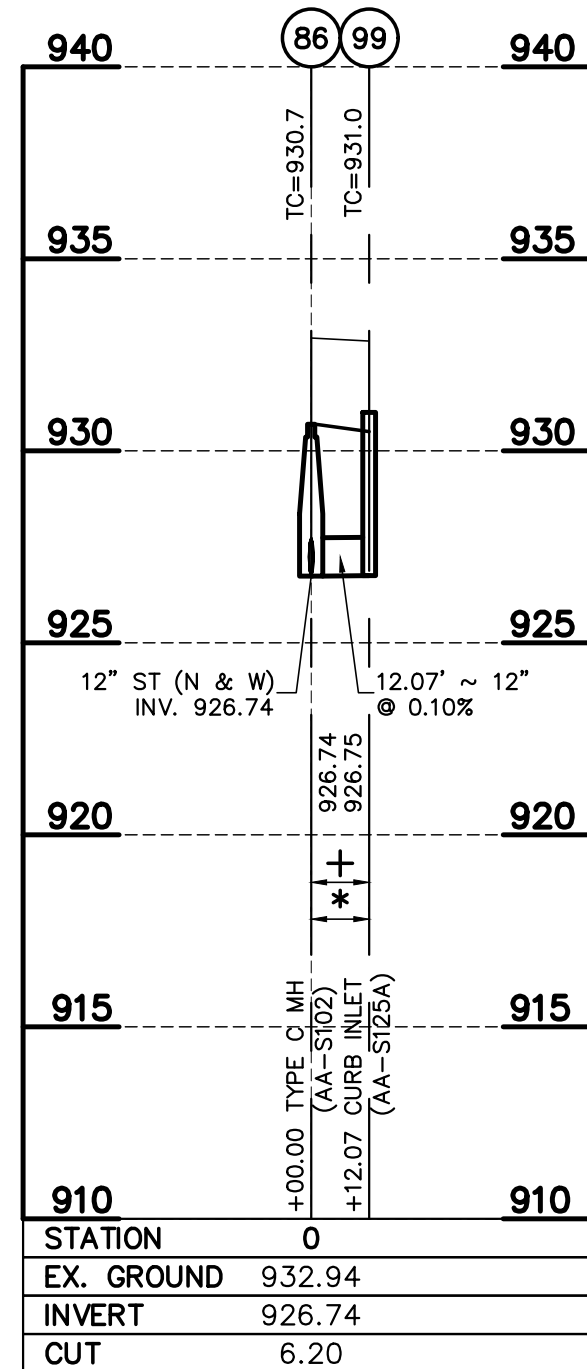
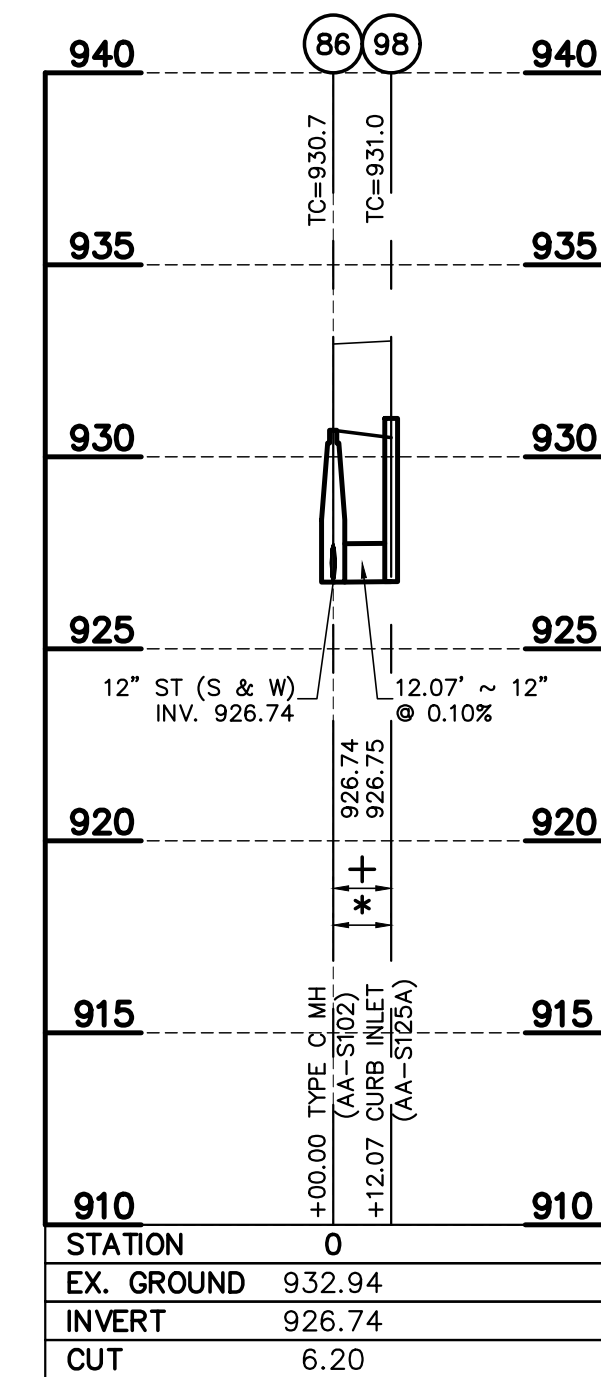
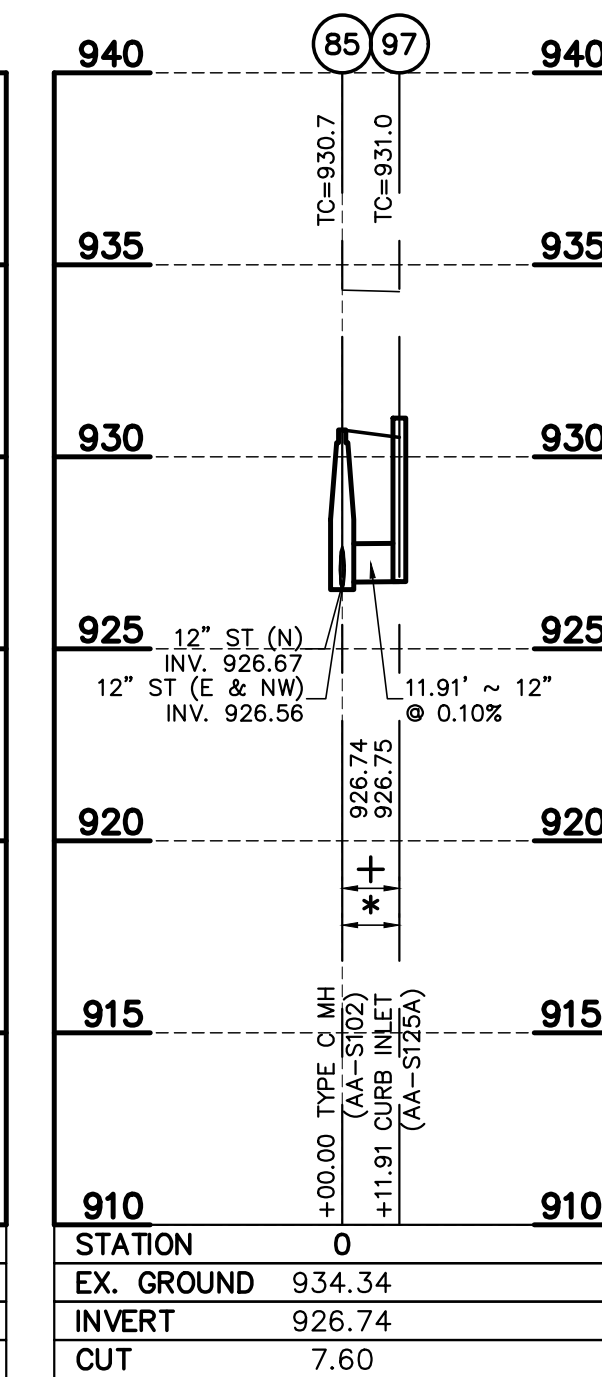
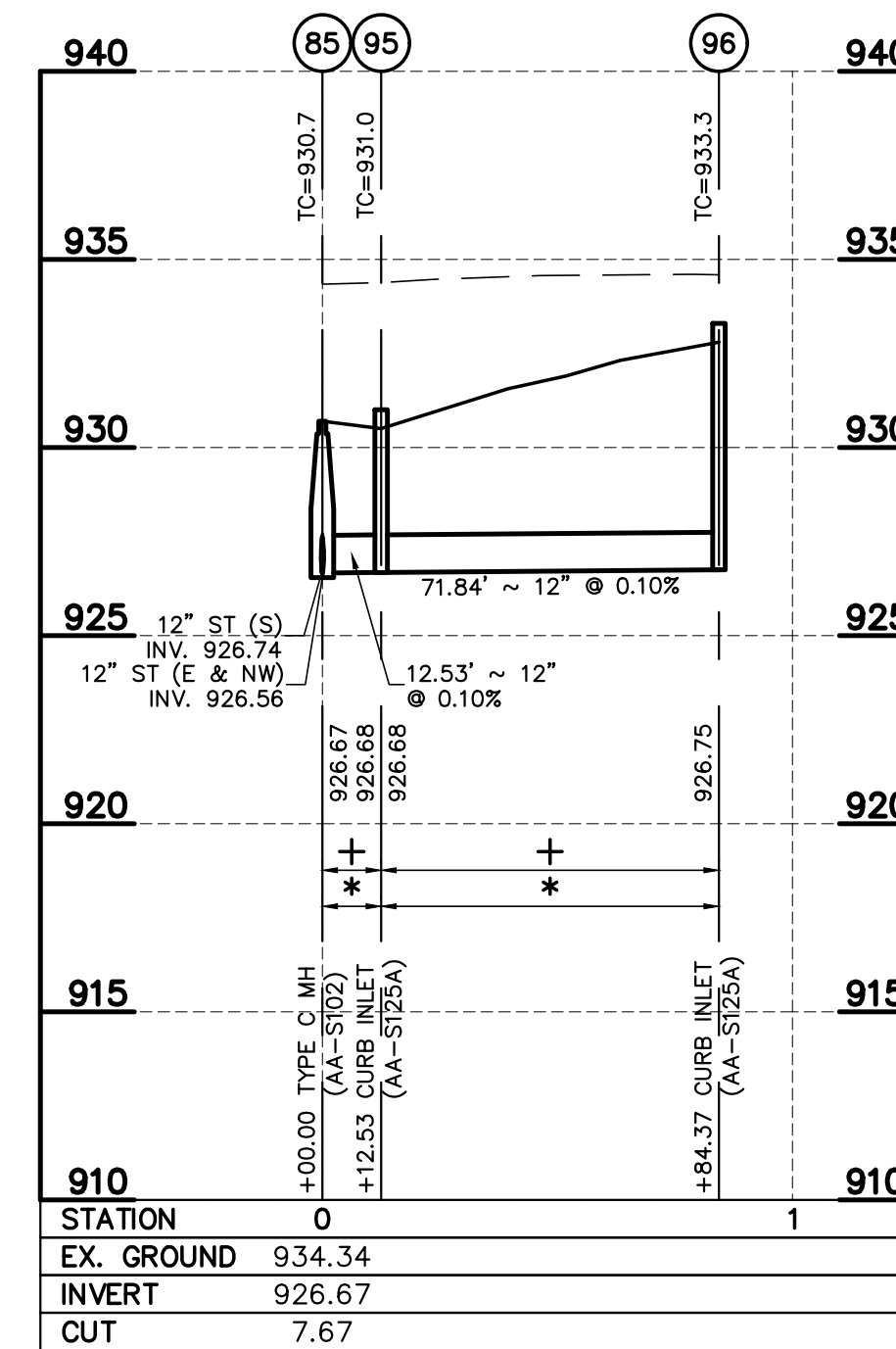
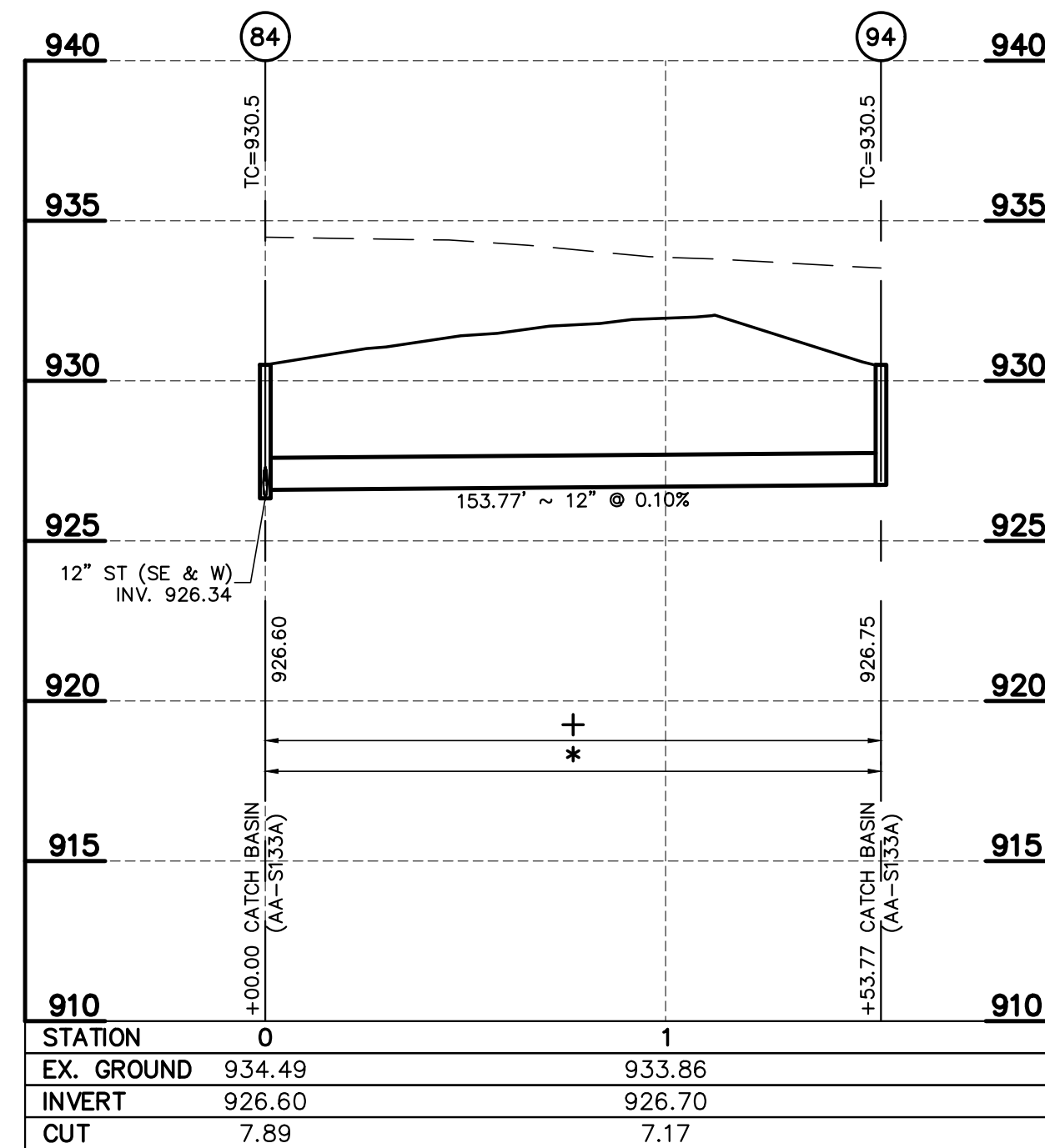
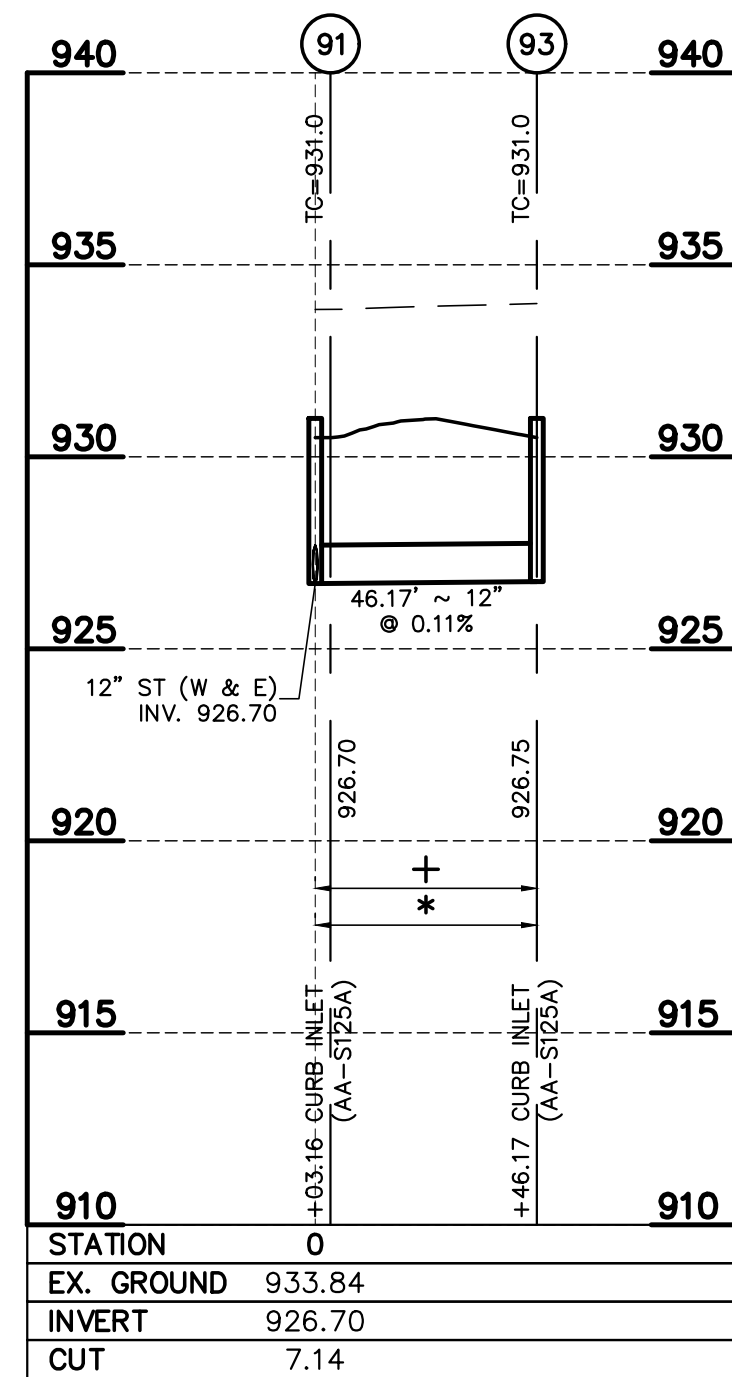
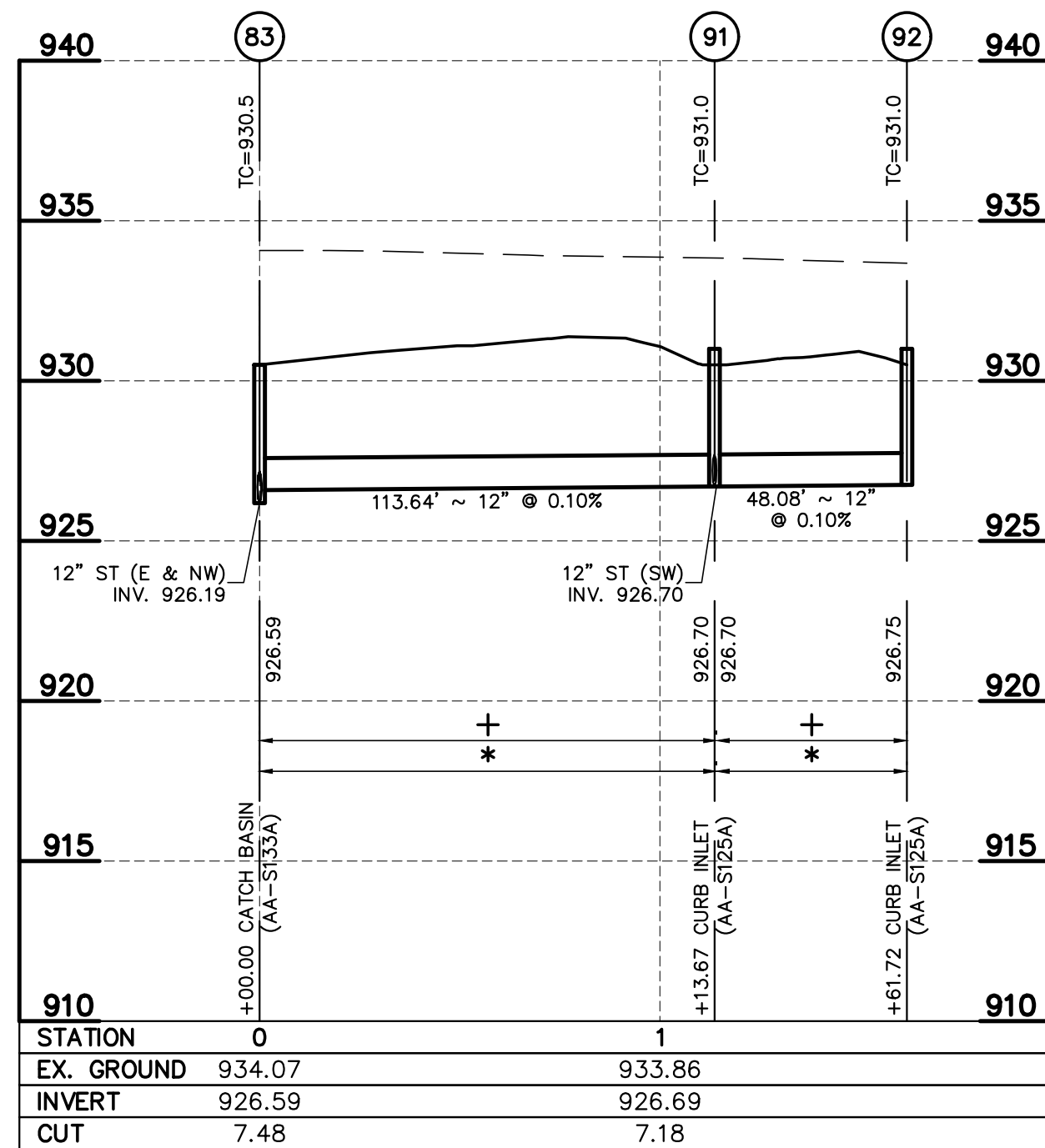
**ADVANCED**  
**CIVIL DESIGN**  
**ENGINEERS SURVEYORS**

422 Beecher Road  
 Gahanna, Ohio 43230  
 ph 614.428.7750  
 fax 614.428.7755

SCALE: 1" = 40' HORIZ.  
 1" = 5' VERT.  
 DATE: 08/29/2019

SHEET 14 / 15

Z:\18-0003-114\DWG\PRODUCTION DRAWINGS\PRE DEVELOPMENT PLAN\0003-114\_PDP\_Screening.dwg PROFILE(S) (S) Aug 29, 2019 - 7:10:21am rfwier



PROFILE VERTICAL DATUM = NAVD 88

± : SANITARY GRADE PIPE W/ WATERTIGHT JOINT FOR 1/1 CROSSING

ALL TRENCH BACKFILL SHALL BE COMPACTED BACKFILL TO 95% OF MAXIMUM DRY WEIGHT AT OPTIMUM MOISTURE CONTENT (±2%) UNLESS NOTED AS:

\* : COMPACTED BACKFILL, ITEM 911

\*\* : COMPACTED GRANULAR BACKFILL, ITEM 912

/// DENOTES AREA TO BE FILLED PRIOR TO INSTALLATION OF STORM SEWER PER ITEM 203 CMSC

NOTE:

PER CMSC ITEM 911, WHERE NATURAL MATERIAL DOES NOT MEET THE REQUIREMENTS OF ITEM 911, THEN COMPACTED GRANULAR BACKFILL, ITEM 912, SHALL BE USED.

ITEM 912 IS TO BE USED IN R/W PER 2179 STD. DWG.

F.V. = ELEVATION AND LOCATION FIELD VERIFIED

\*\*\* = MUST MAINTAIN A MIN. 18" CLEARANCE

1 inch = 40 feet (H)  
1 inch = 5 feet (V)

CITY OF DUBLIN, OHIO  
PRELIMINARY DEVELOPMENT PLAN  
STORM SEWER PROFILES  
FOR  
OSU MEDICAL CAMPUS PUD

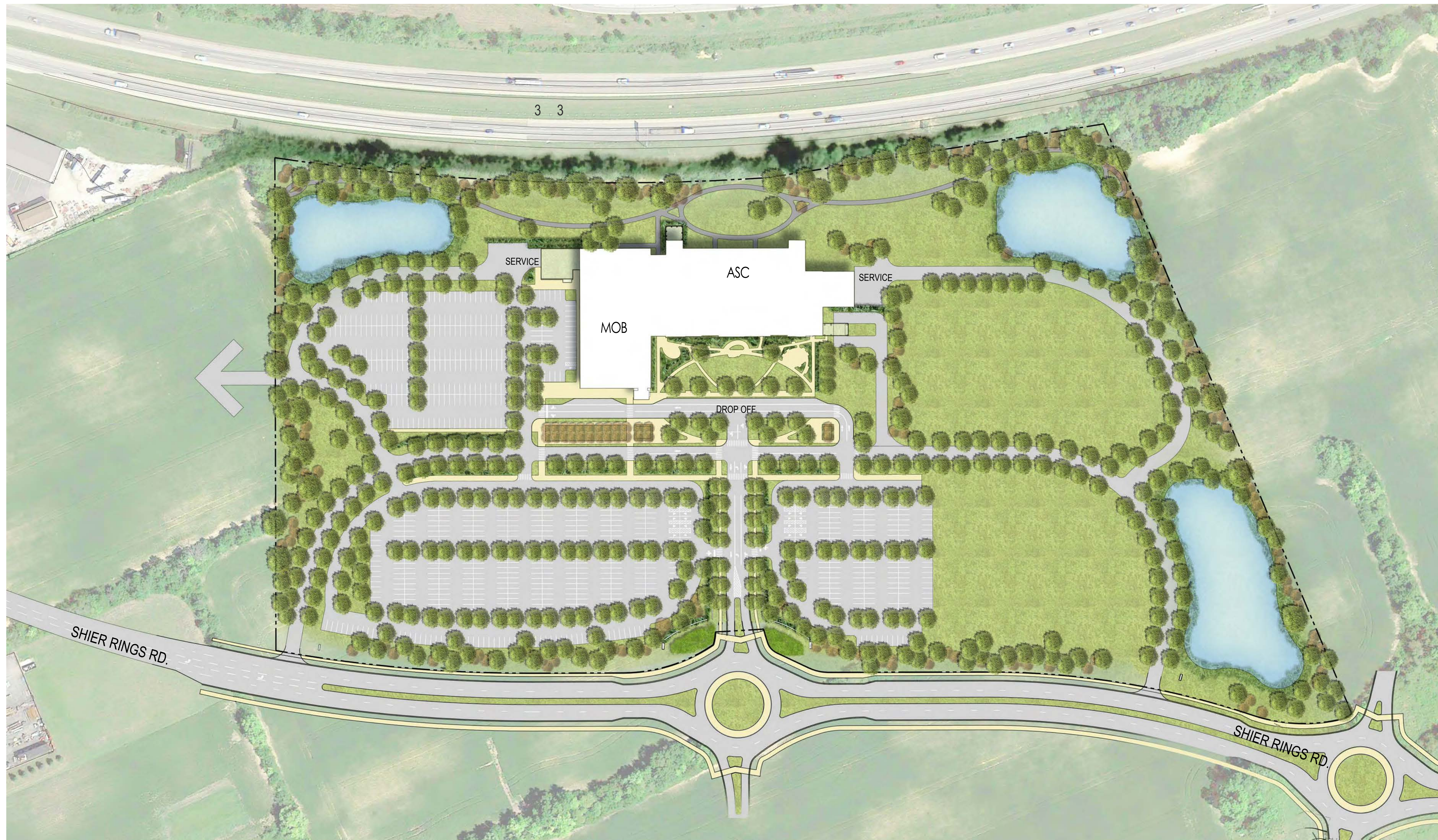
PLAN PREPARED BY:

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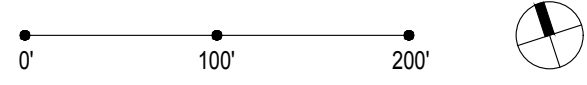
422 Beecher Road  
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SHEET 15 / 15



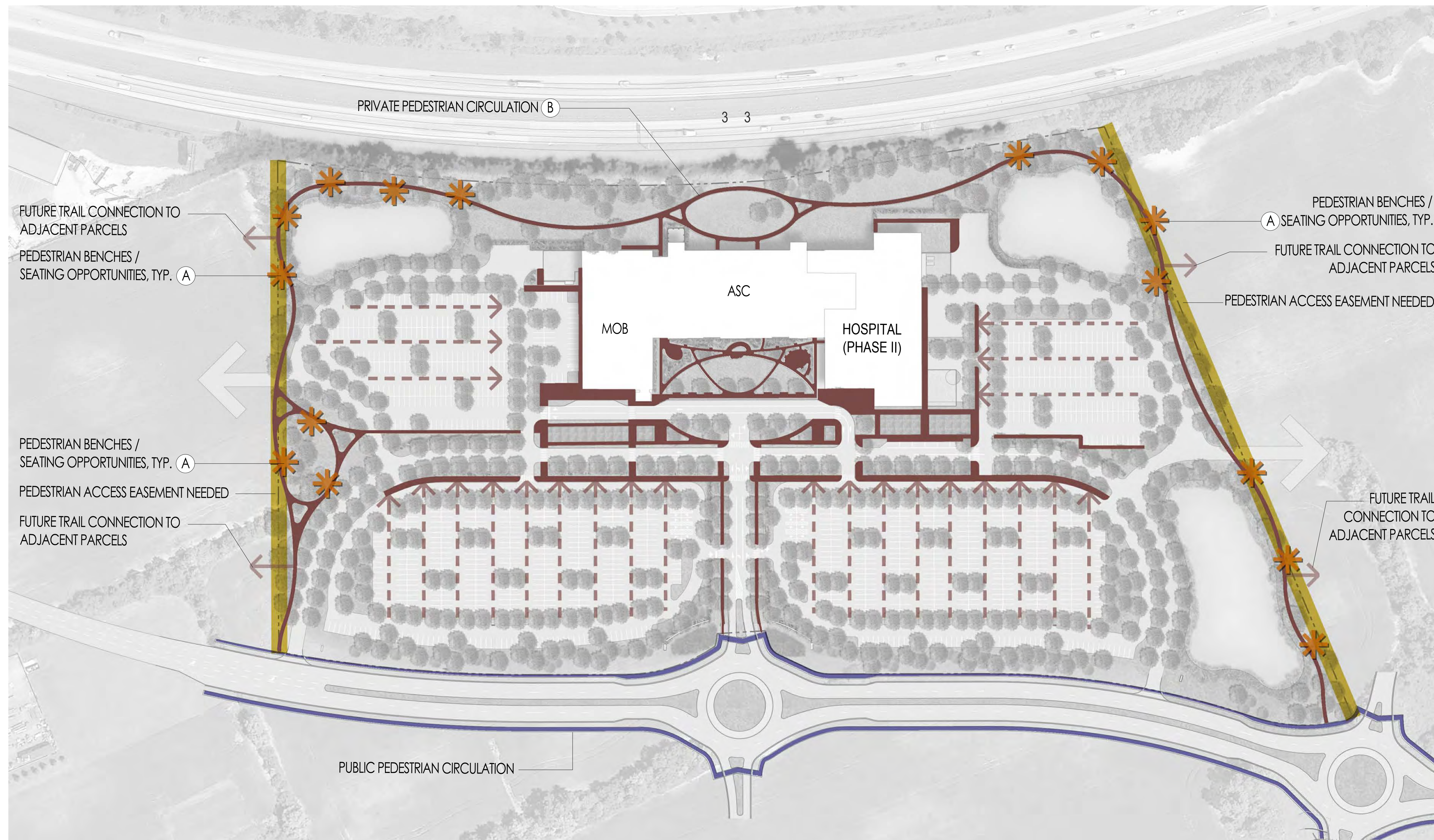
PHASE I SITE PLAN  
22 AUG 2019



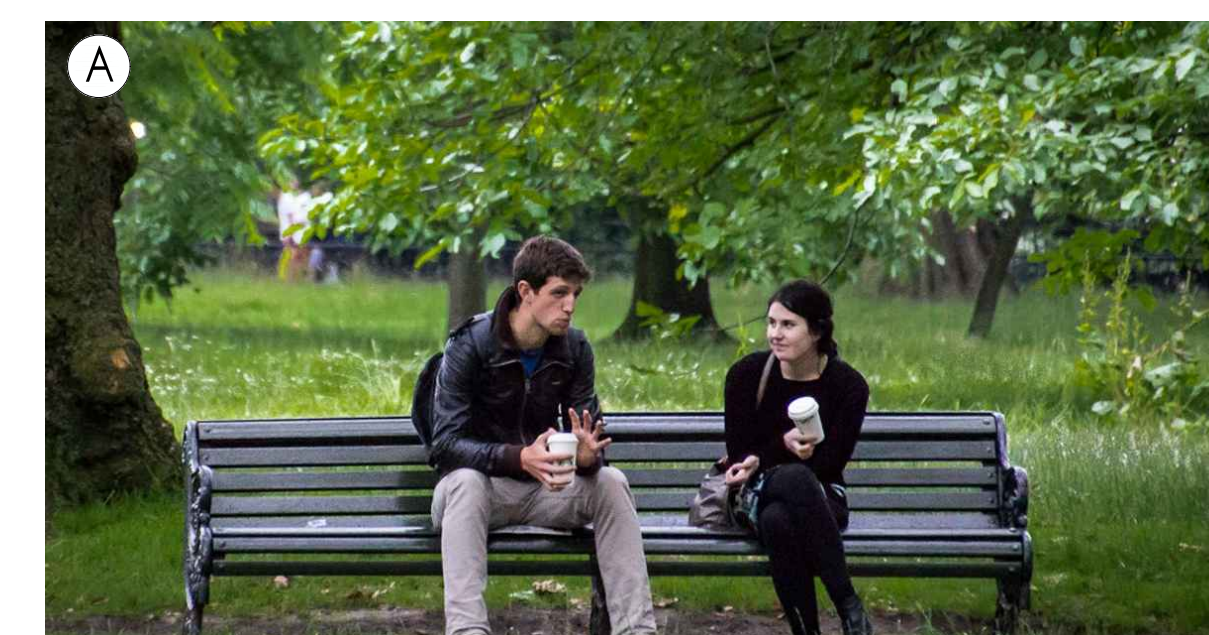
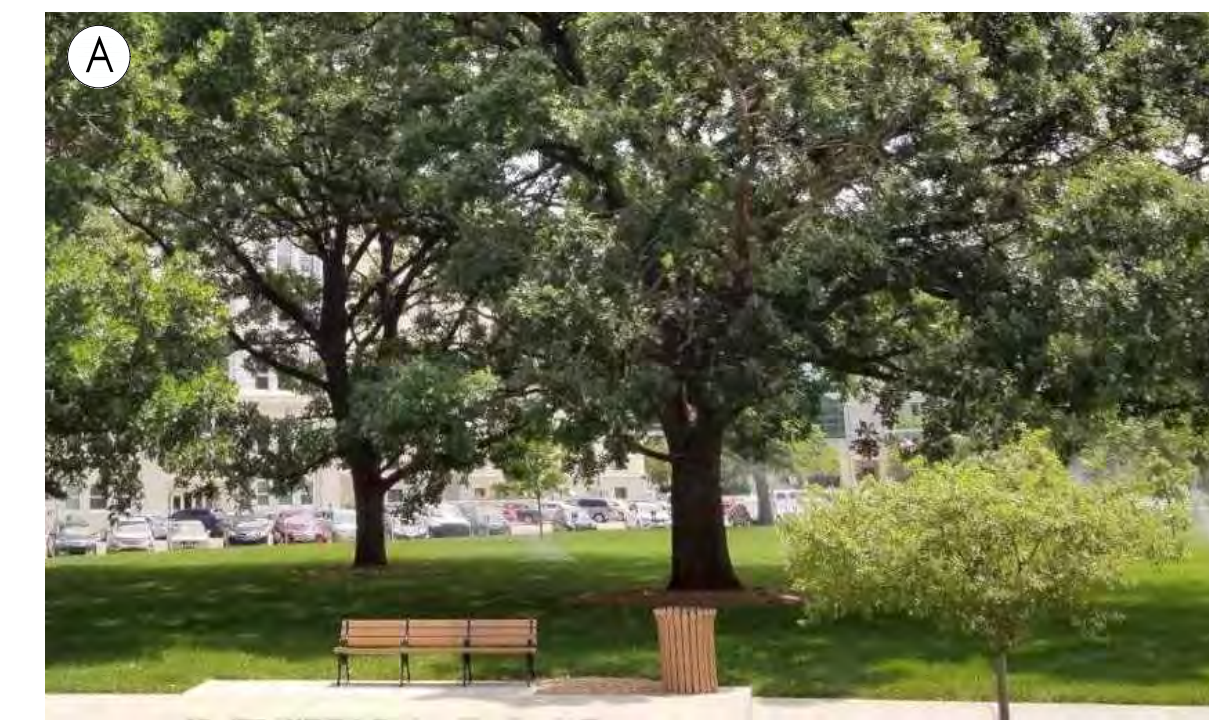


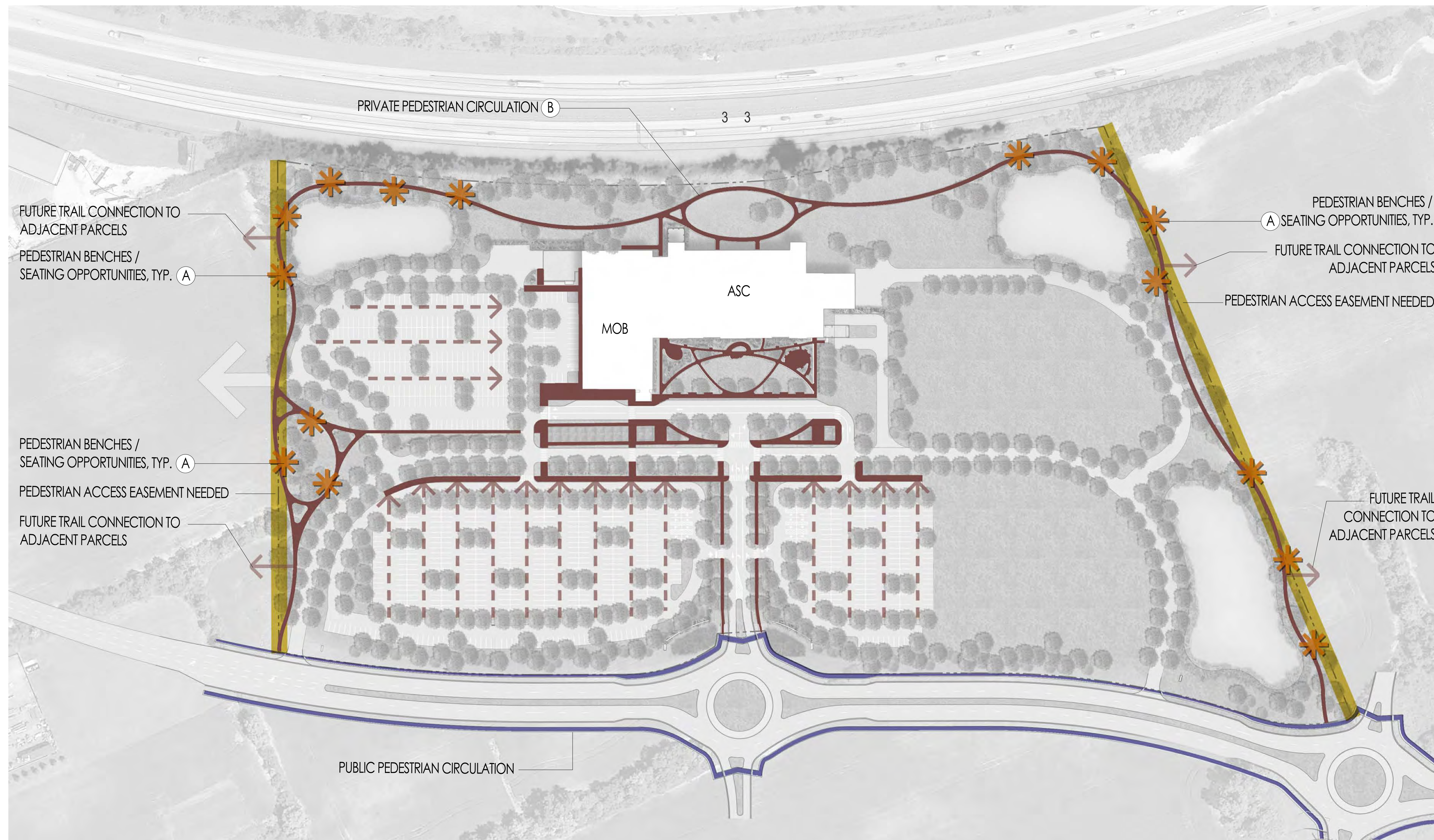
FULL BUILD SITE PLAN  
22 AUG 2019



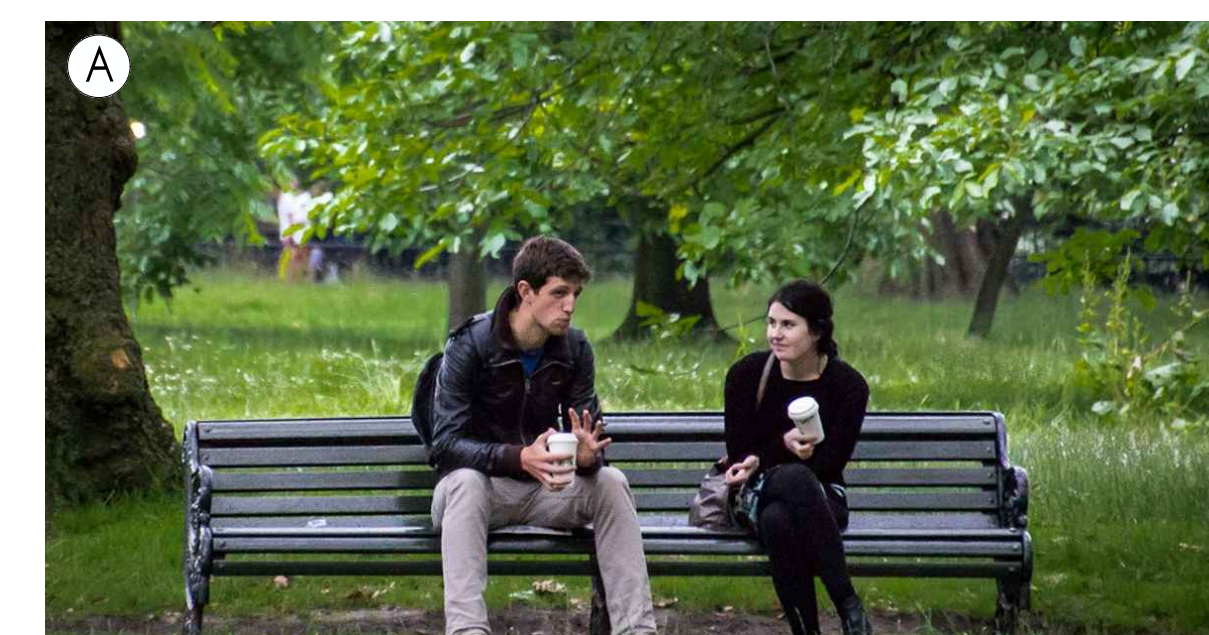
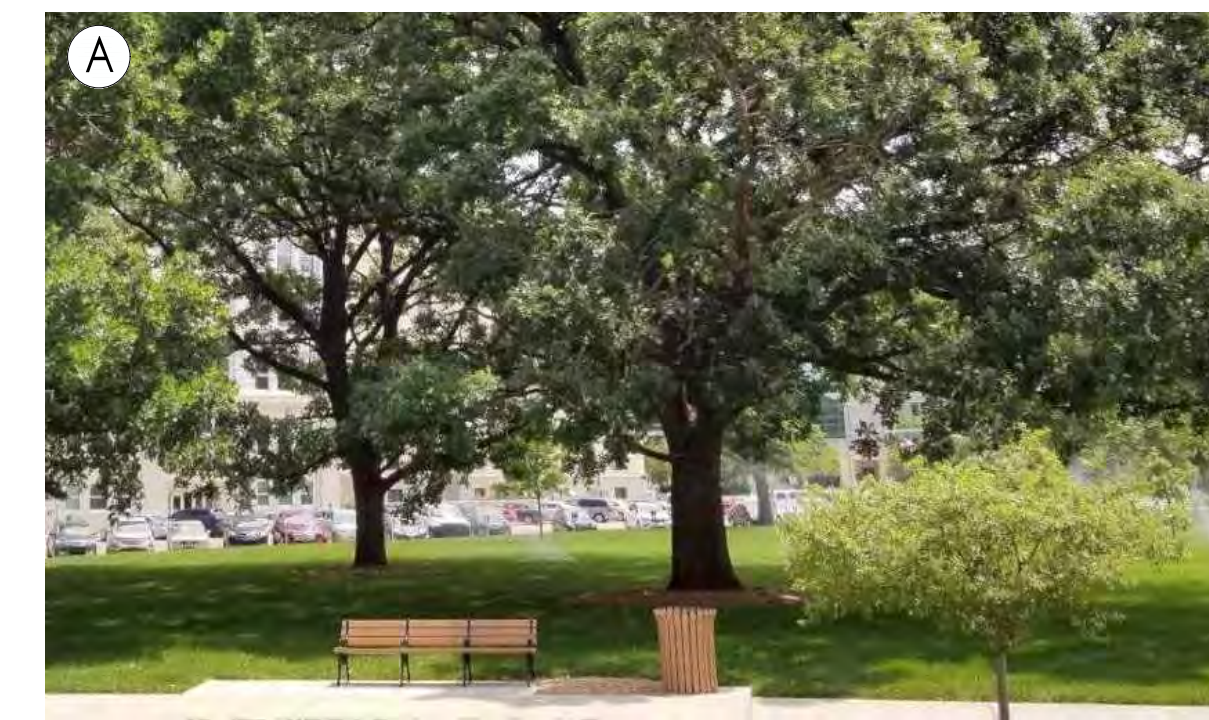


PEDESTRIAN CIRCULATION DIAGRAM



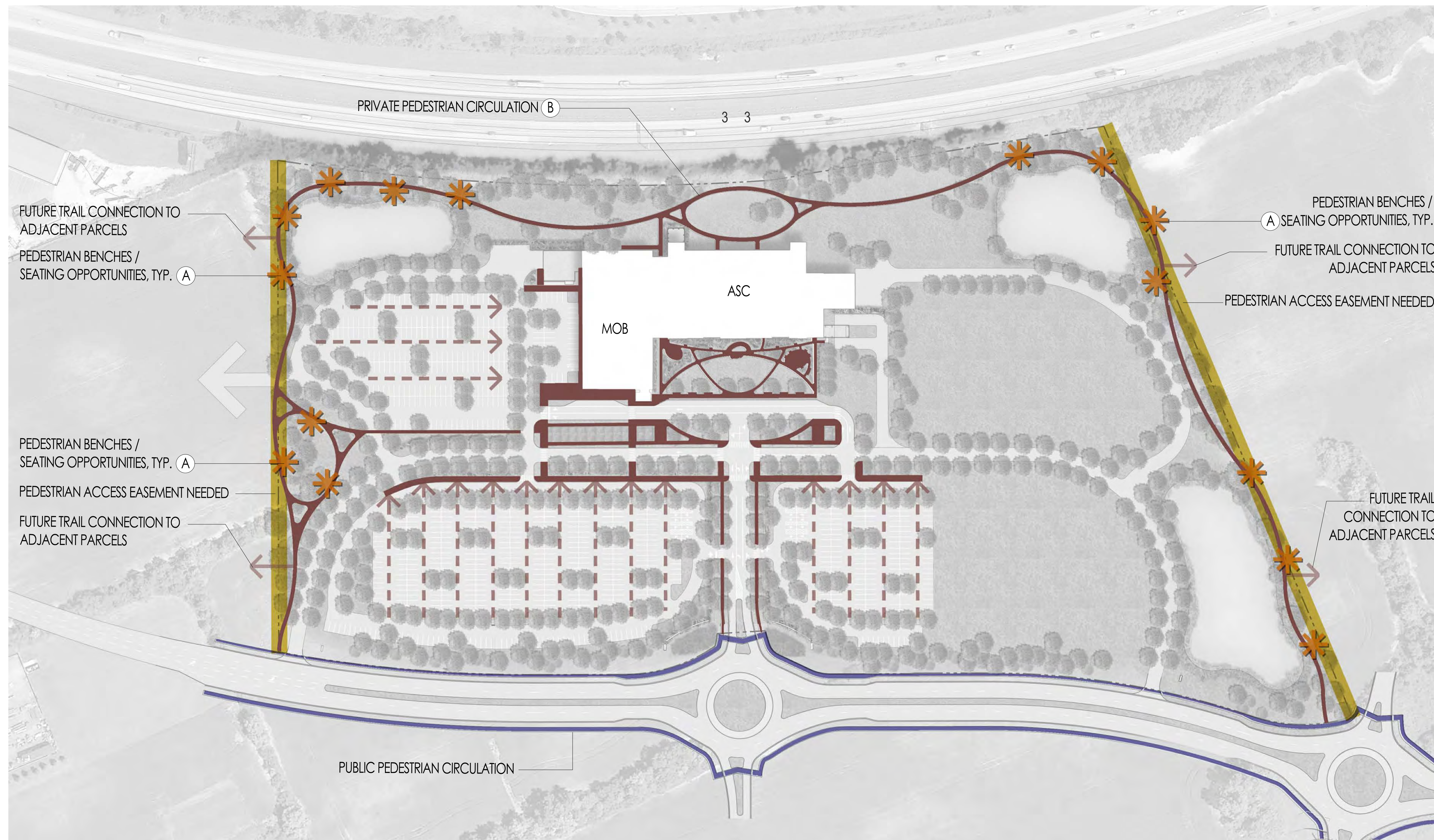


PEDESTRIAN CIRCULATION DIAGRAM, PHASE I

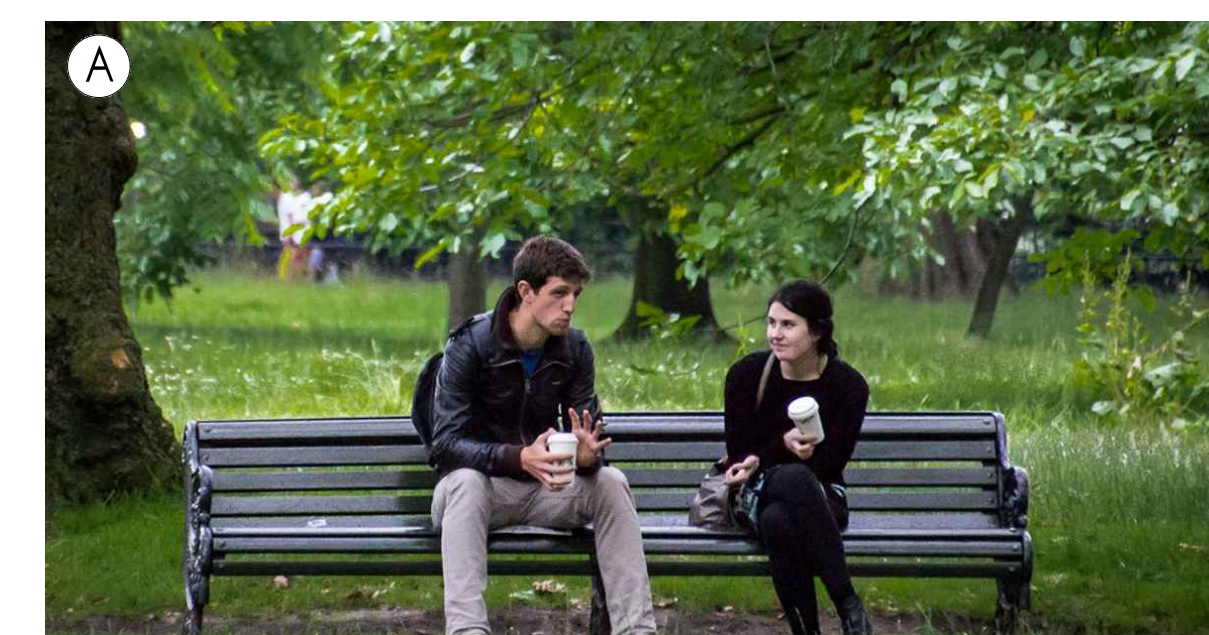
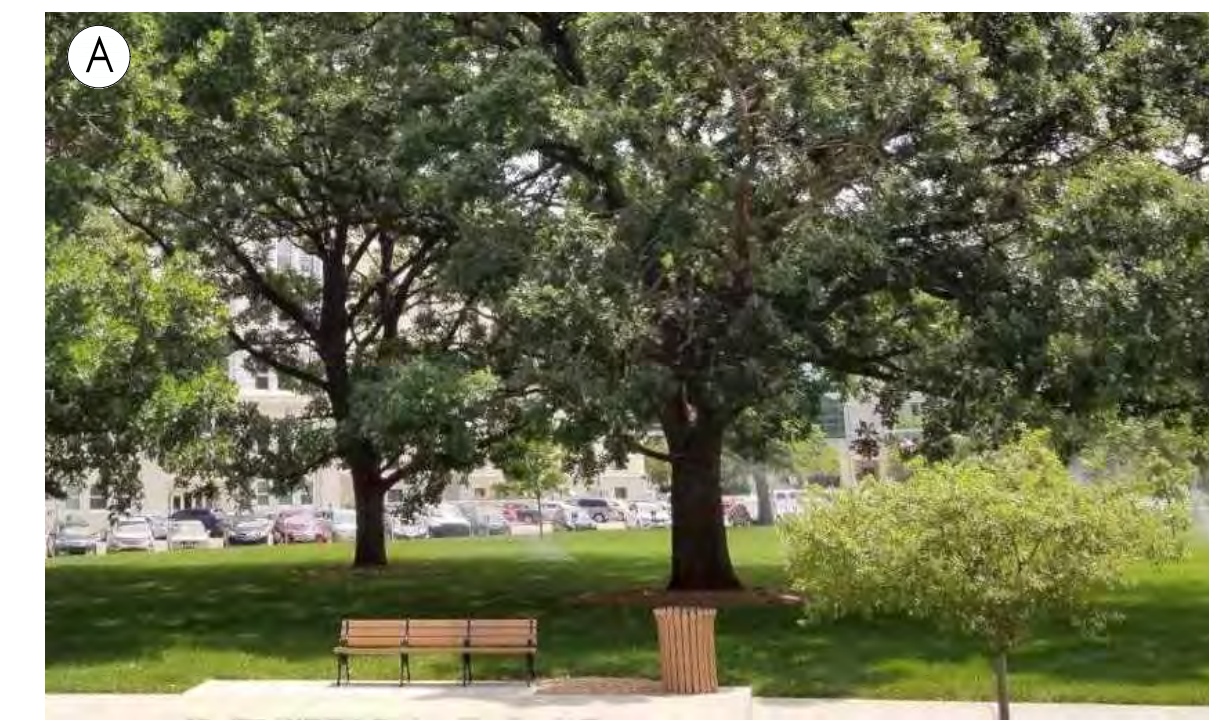




PHASE I SITE PLAN  
06 AUG 2019



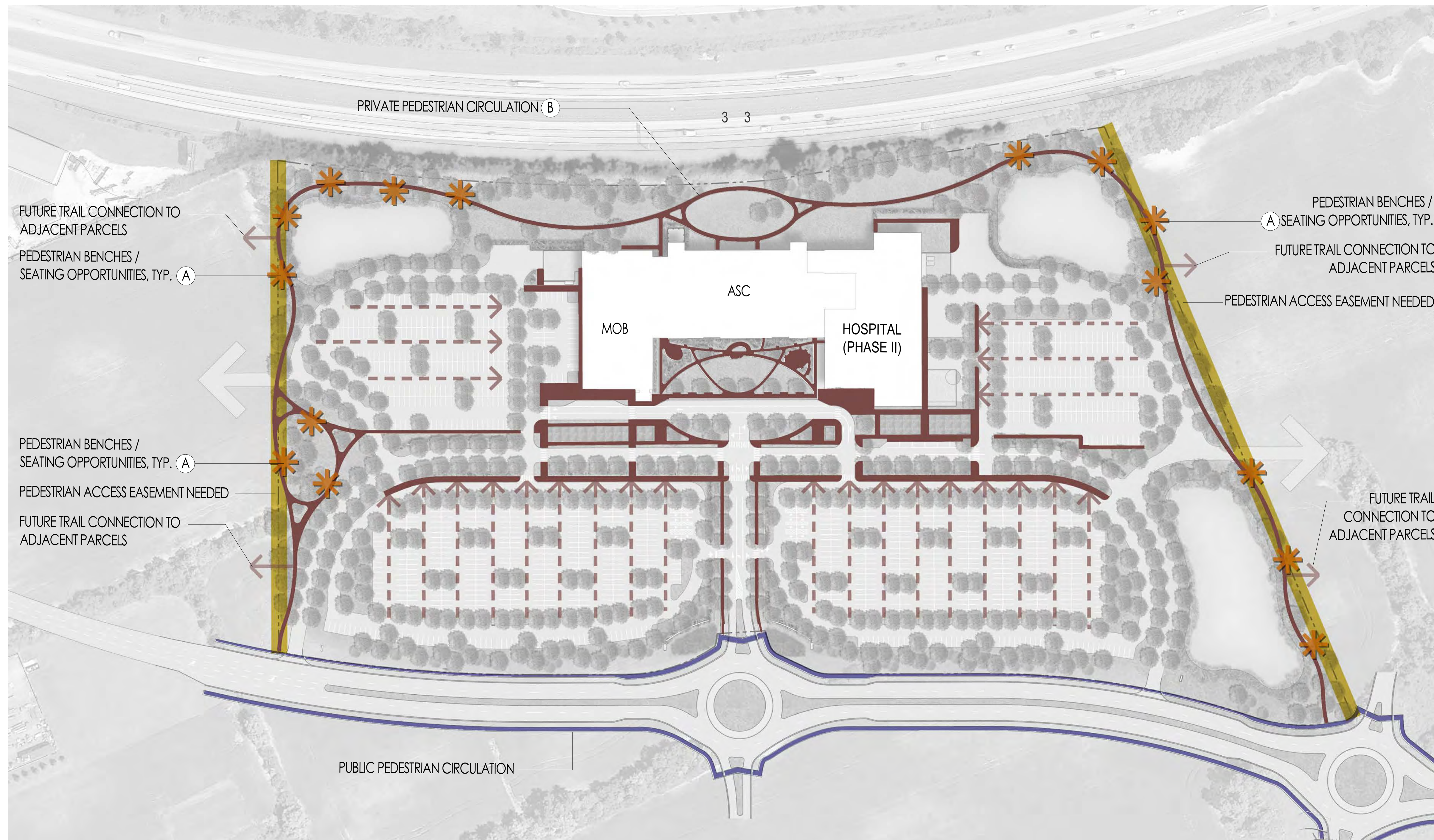
PEDESTRIAN CIRCULATION DIAGRAM, PHASE I



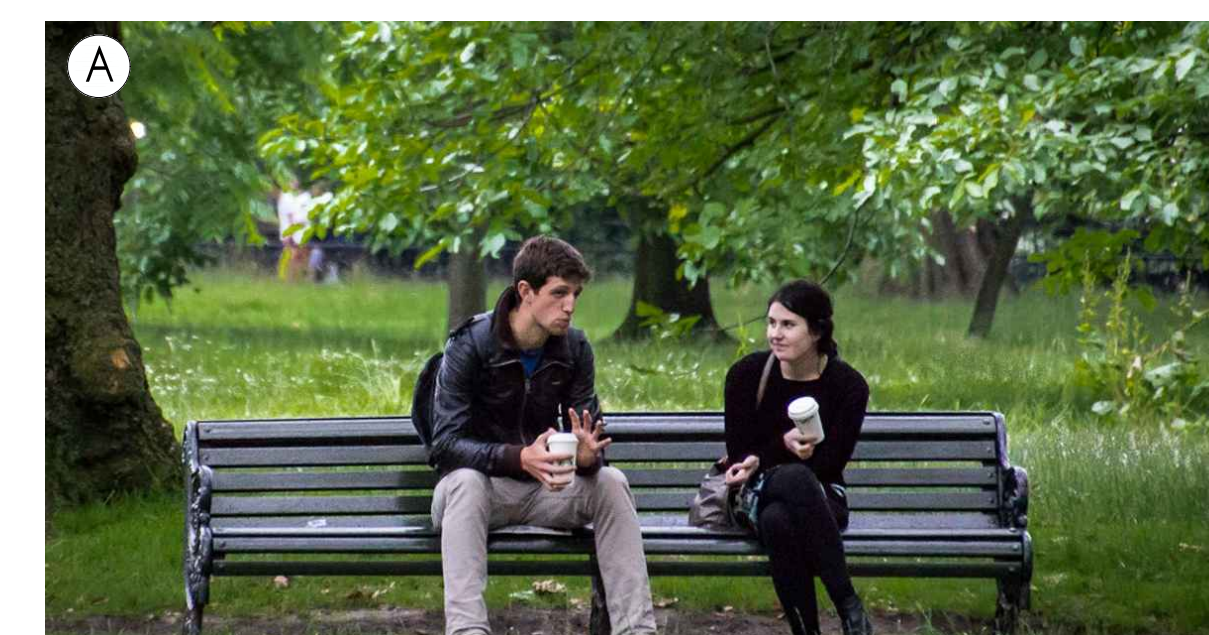
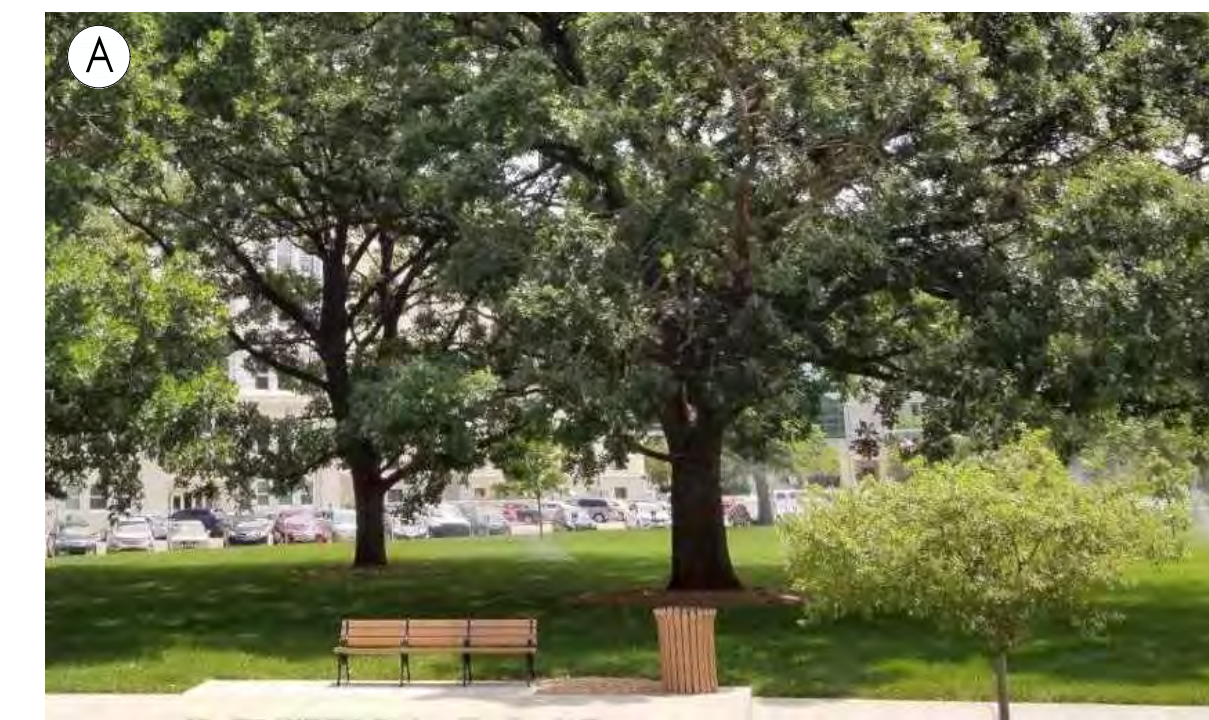
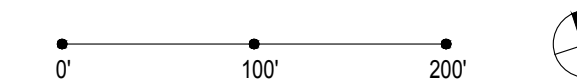


FULL BUILD SITE PLAN  
06 AUG 2019





PEDESTRIAN CIRCULATION DIAGRAM





**4 LIMESTONE ENTRY WALLS**  
 • MORTARED LIMESTONE WALL  
 • RAKED JOINTS



**5 STORMWATER PONDS**  
 • WETLAND / PLANTED EDGES



**1 CAFE COURTYARD & RESPITE GARDEN**  
 • SPECIALTY CONCRETE/PAVERS  
 • TREES AND ACCENT PLANTINGS  
 • MOVEABLE FURNITURE  
 • SPECIALTY LIGHTING



**2 ENTRY COURT**  
 • SPECIALTY CONCRETE/PAVERS  
 • TREES AND ACCENT PLANTINGS  
 • SITE FURNITURE & BOLLARDS  
 • SITE LIGHTING



**3 ENTRY GREEN**  
 • CONCRETE WALKS  
 • SPECIALTY CONCRETE  
 • TREES AND ACCENT PLANTINGS



**6 LANDSCAPE BUFFER**  
 • PARK LIKE LANDSCAPE  
 • TREES AND TURF  
 • PARKING LOT SCREEN

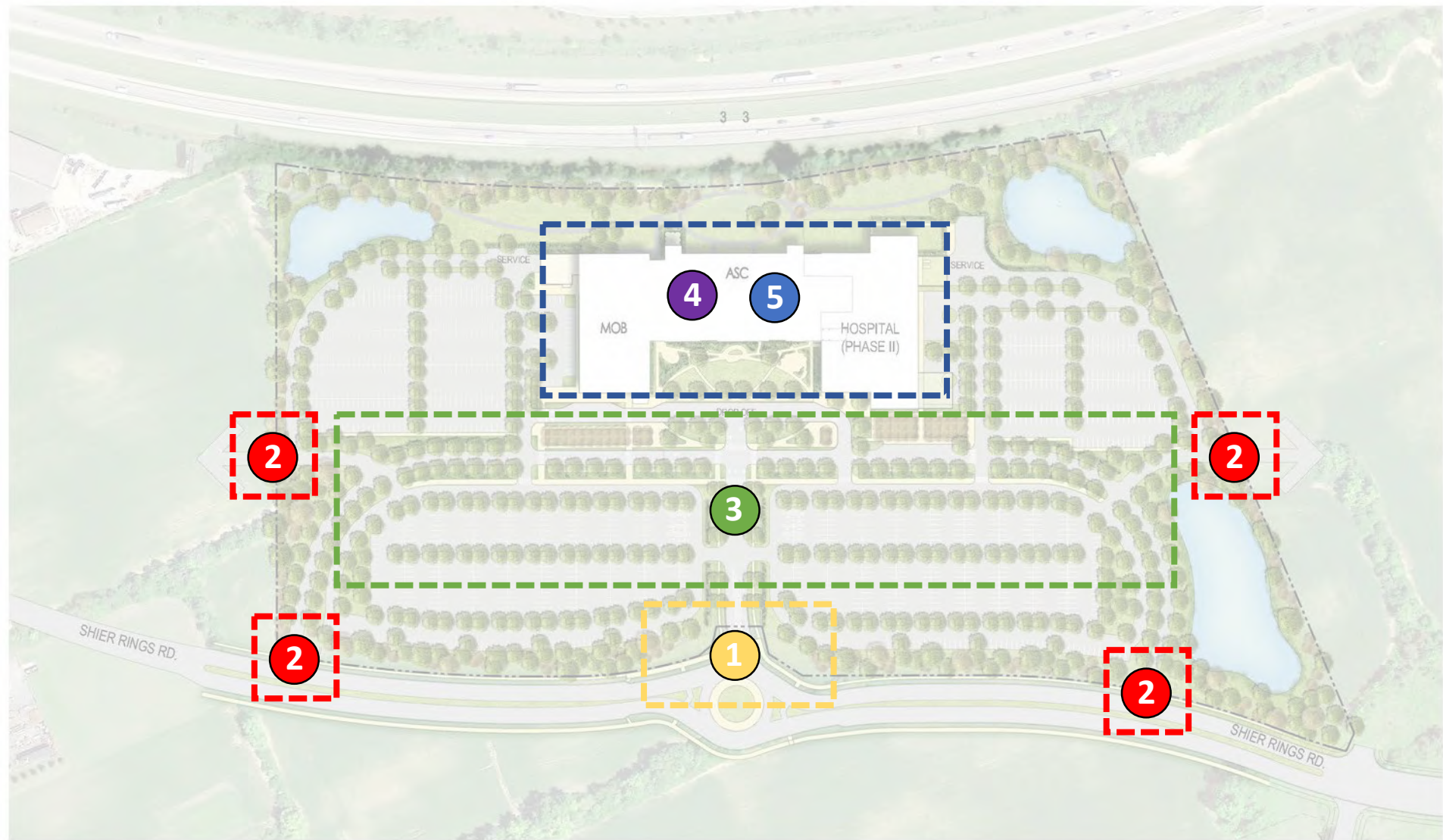




**THE OHIO STATE UNIVERSITY**  
WEXNER MEDICAL CENTER

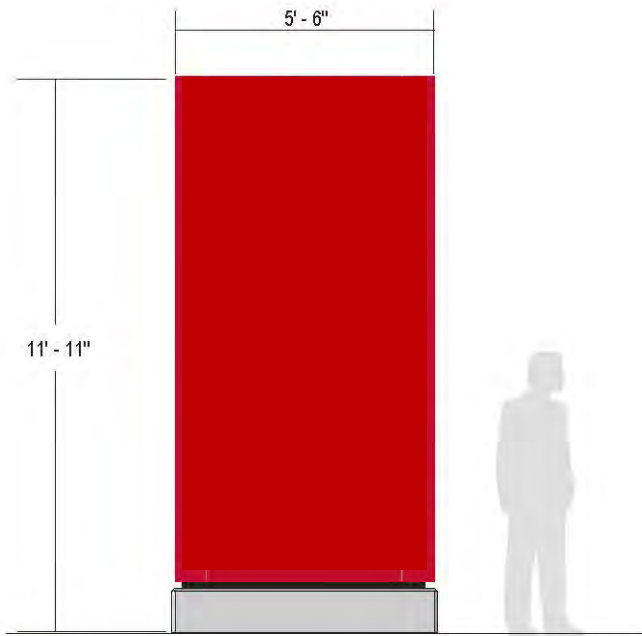
Patient Arrival Courtyard



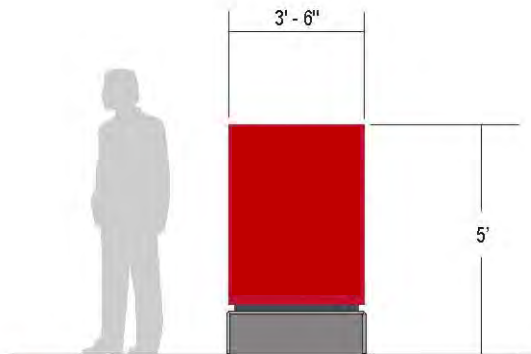


FULL BUILD SITE PLAN  
26 JUNE 2019

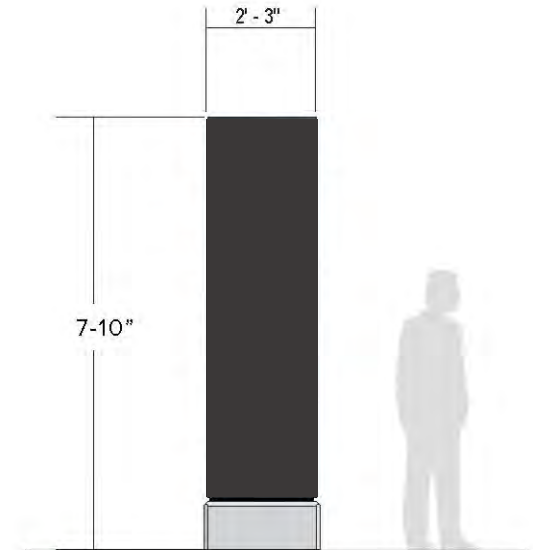




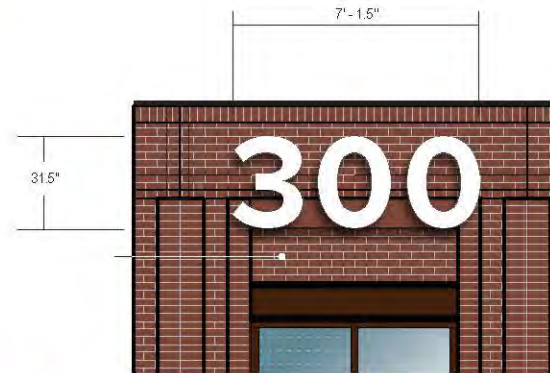
① Primary Entry Monument Sign



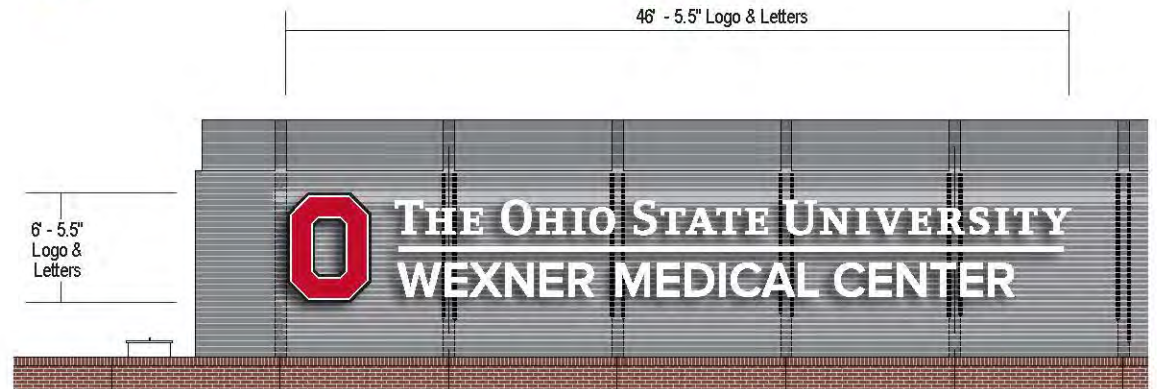
② Secondary Entry Monument Sign



③ Directional Sign



④ Ancillary Wall Signs



⑤ Building Identification Sign



**THE OHIO STATE UNIVERSITY**  
WEXNER MEDICAL CENTER

View looking Northeast towards PH I Medical Office Building



# RECORD OF ACTION

## Planning & Zoning Commission

Thursday, September 19, 2019 | 6:30 pm

The Planning and Zoning Commission took the following action at this meeting:

**1. The Ohio State University Wexner Medical Center 6600 Shier Rings Road  
19-055 Rezoning with Preliminary Development Plan**

Proposal: A Rezoning with Preliminary Development Plan of a ±34-acre site, from ID-1 to PUD to facilitate the future development of a medical office building and an ambulatory care center (Phase I) and a future hospital (Phase II).

Location: On the north side of Shier Rings Road, east of Eiterman Road, west of Avery Road, and south of US 33.

Request: Review and approval of a Rezoning with a Preliminary Development Plan under the provisions of Zoning Code Section 153.050.

Applicant: The Ohio State University Wexner Medical Center represented by Aaron L. Underhill, Underhill & Hodge LLC

Planning Contact: Claudia D. Husak, AICP, Senior Planner

Contact Information: 614.410.4675, [chusak@dublin.oh.us](mailto:chusak@dublin.oh.us)

Case Information: [www.dublinohiousa.gov/pzc/19-055](http://www.dublinohiousa.gov/pzc/19-055)

**MOTION:** Ms. Kennedy moved, Mr. Wilson seconded, to approve the Rezoning with a Preliminary Development Plan with eight conditions:

- 1) That the development text and the sign plan be updated to address the discrepancies prior to Council review, including the correct nomenclature and permitted sizes, permitted lighting and permitted number of secondary monument signs;
- 2) That the applicant update the development text to clarify that peninsulas may count in the interior landscape requirements;
- 3) That the smaller trees shown on the plans in the interior landscape areas be permitted on the development text;
- 4) That the development text be revised to include bio-retention measures, as appropriate, in the parking lot areas to aid in stormwater management and provide for irrigation of trees; this should include the elimination of the requirement of curbed islands;
- 5) That the applicant continue to work with Engineering to demonstrate compliance with stormwater requirements as defined in Chapter 53 to the satisfaction of the City Engineer;
- 6) That the building elements and mechanical equipment areas be screened per Code in the text;



1. **The Ohio State University Wexner Medical Center** **6600 Shier Rings Road**  
**19-055** **Rezoning with Preliminary Development Plan**

- 7) That the applicant work with staff to identify areas where new trees are appropriate to reduce the need for future removal during Phase 2 and update the text accordingly; and
- 8) That the applicant coordinate with ODOT for necessary approvals to remove any dead, invasive or hazardous vegetation along the frontage.

**VOTE:** 7 - 0

**RESULT:** The Rezoning with a Preliminary Development Plan was approved.

**RECORDED VOTES:**

|                  |     |
|------------------|-----|
| Victoria Newell  | Yes |
| Jane Fox         | Yes |
| Warren Fishman   | Yes |
| Kristina Kennedy | Yes |
| William Wilson   | Yes |
| Mark Supelak     | Yes |
| Rebecca Call     | Yes |

**STAFF CERTIFICATION**

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Claudia D. Husak, AICP  
Senior Planner/Current Planning Manager

## **CASES**

### **1. The Ohio State University Wexner Medical Center, 6600 Shier Rings Road, 19-055, Rezoning with Preliminary Development Plan**

Ms. Newell stated that this is a request for a Rezoning with Preliminary Development Plan of a ±34-acre site, from ID-1 to PUD to facilitate the future development of a medical office building and an ambulatory care center (Phase I) and a future hospital (Phase II). The site is on the north side of Shier Rings Road, east of Eiterman Road, west of Avery Road, and south of US 33.

Ms. Husak requested that Cases 1 and 2 be considered together.

### **2. University Boulevard Phase 2, 6600 Shier Rings Road, 19-080, Preliminary Plat**

Ms. Newell stated that this is a request for a Preliminary Plat for the provision of right-of-way for University Boulevard to extend from Shier-Rings Road west to intersect with Eiterman Road and the creation of a ±34-acre parcel for future development of a medical center. The site is north of Shier Rings Road and west of the intersection with Avery Road.

## **Staff Presentation**

Ms. Husak stated that on August 22, 2019, the Commission reviewed and provided non-binding comments on a Concept Plan for this project. Tonight, the Commission is considering the Ohio State University (OSU) development as well as the creation of a new road, University Boulevard. At the Ohio University (OU) site on the south side Post Road, there is a piece of University Boulevard, to which this new section of the road eventually will connect and create a road network that has been planned for multiple years for the West Innovation District (WID). The Preliminary Plat for this project, which is Case 2, contains the detail on the creation of the parcel for Ohio State University, as well as the road. The approximately 34-acre site for the OSU Wexner Medical Center will be developed in two phases. The Preliminary Development Plan is similar to what the Commission reviewed in August. It includes a medical office building and ambulatory care facility. The future University Boulevard will be located on the south side of the site with a roundabout, which will provide site access. There also will be two minor access points on the east and west sides of the lot to provide access to the remaining parcels. In Phase 2, the hospital will be added on the east side of the medical facility, as well as additional parking and access that will allow additional uses. The applicant has included a significant amount of information in the proposed development text, including the permitted materials. Staff has encouraged that the structure be very similar to the existing West Innovation District (WID) zoning text. The rendering, the same as that shown with the Concept Plan review on August 22, depicts the creation of a walkable environment. Additional updated drawings show both respite and walking pathways through the parking lots. There are unique sign requirements for this use. After Dublin Methodist Hospital completed their facility and had data based upon user experience, they returned to the Commission to upgrade their sign rules. Making an effort to be at the forefront of that discussion, OSU has included sign requirements and diagrams in the development text. The rules and regulations in the development text will guide future signage. There are five hierarchy signs. A monument sign is proposed for the entry feature at the roundabout, well as vehicular signs that are taller than the Code permits but address the parking and navigation needs. Staff recommends approval with eight (8) conditions.

For Case 1, the OSU Wexner Medical Center rezoning and preliminary development plan, staff recommends approval with the following conditions:

1. That the development text and the sign plan be updated to address the discrepancies prior to Council review, including the correct nomenclature and permitted sizes, permitted lighting and permitted number of secondary monument signs;
2. That the applicant update the development text to clarify that peninsulas may count in the interior landscape requirements;
3. That the smaller trees shown on the plans in the interior landscape areas be permitted in the development text;
4. That the development text be revised to include bio-retention measures, as appropriate, in the parking lot areas to aid in stormwater management and provide for irrigation of trees. This should include the elimination of the requirement of curbed islands;
5. That the applicant continue to work with Engineering to demonstrate compliance with stormwater requirements as defined in Chapter 53 to the satisfaction of the City Engineer;
6. That the building elements and mechanical equipment areas be screened per Code in the text;
7. That the applicant work with staff to identify areas where new trees are appropriate to reduce the need for future removal during Phase 2 and update the text accordingly; and
8. That the applicant coordinate with ODOT for necessary approvals to remove any dead, invasive or hazardous vegetation along the frontage.

For Case 2, the preliminary plat for University Boulevard, approval is recommended for the following variance from the Subdivision Regulations:

- 1) To permit the creation of Lot 1 at 1.23-acre instead of meeting the Zoning Code required 3-acre minimum.

The application complies with all applicable review consideration and the intended development character of the area; therefore, approval of the plat is recommended with one condition:

- 1) The applicant ensure any minor technical adjustments to the plat are made prior to City Council submittal.

Ms. Husak noted that several discussions have occurred with the Ballantrae neighborhood residents regarding this project, some of whom are present tonight.

### **Board Questions**

Mr. Fishman stated that traffic around US33 is a significant concern with the proposed project. He inquired about the timing of the two construction phases and the roadway.

Ms. Husak responded that City Council approved a development agreement with OSU earlier this year that provides for the alignment of OSU's and the City's construction activities for 2022.

Barb Cox, City Engineering Manager, asked if his inquiry concerned potential roadway work around the US33 interchanges with Avery-Muirfield or Post Road.

Mr. Fishman responded that was part of his question.

Ms. Cox responded that traffic studies indicate there is no need for roadway work at either of the interchanges for this project at this time. As more development occurs in the WID, it could become necessary. In anticipation of the construction of University Boulevard, permitting for the relocation of the creek is occurring, as well as the design of and right-of-way acquisition for the roadway. Construction of the roadway project is scheduled to begin in the spring of 2021 with completion anticipated in 2022.

Mr. Fishman inquired if the infrastructure construction would be completed before construction of the building.

Ms. Cox replied that construction of the building and roadway would occur simultaneously.

Mr. Supelak inquired what is the extent of the roadway construction.

Ms. Cox responded that the roadway project currently under design is for improvements to Shier Rings at Avery Road, widening of Shier Rings Road up to its curve north into this property, and connection of the new roadway to Eiterman Road. Future construction phases will extend the roadway from Eiterman Road to the Ohio University campus. That construction is not currently scheduled.

Mr. Supelak stated that in the meeting materials, drawings showed three potential roundabout locations. Which drawing is the most accurate?

Ms. Cox responded that the OSU team included a traffic engineer who conducted traffic studies pertaining to this development. The City also has engaged engineers to conduct traffic studies for the infrastructure project. There is some diversity in the OSU and City drawings, and analysis and coordination of the two studies is underway. A public meeting is scheduled on October 15 to share the final roadway plan to the public. At an earlier public meeting in August, Shier Rings Road and the new University Boulevard were shown as disconnected. Following that public input, traffic studies were re-evaluated. Although there is a significant level of access management to consider, there will be a Shier Rings Road connection to the new University Boulevard.

Ms. Call inquired if the Commission would receive a full transportation plan for this area.

Ms. Cox responded affirmatively. This is a preliminary plat. A final plat will be brought before the Commission at a future meeting, and more details will be shown at that time. Due to the distance between the two ends of University Boulevard, completion of the road extension likely will occur with the development of those parcels.

Ms. Call stated that her concern is not just this particular plat. Per her review, the existing University Boulevard appears to be approximately 1.25 miles away from the new roadway. She is concerned about using a name for the proposed roadway that is already in use in the other location, although the intent is that eventually they would connect. The land between is owned by several property owners, who could or could not choose to develop it for some time.

Ms. Cox responded that the City has previous experience with disconnected streets, some of which have not been connected for many years. The City Thoroughfare Plan and Community Plan provide guidance to assure that those connections will be completed.

Mr. Supelak stated that at the Commission's previous discussion, the roadway topic was very important to both the public and the Commission. The sooner everyone can understand the final roadway plan, the better decisions the Commission will be able to make.



Mr. Fishman concurred. He also is concerned about the timing. Will the road be in place before the people and the buildings?

Ms. Cox responded that the EDA contemplated all those issues, including the synchronization of the OSU and City construction schedules so that the road will be open and available when this medical facility is ready.

Mr. Fishman inquired when the Commission would be able to see a complete plan.

Ms. Cox responded that it would be provided to the Commission with the final plat. It may be necessary to add some easements as the project proceeds through final design.

Ms. Husak stated that the Commission serves as an advisory board to City Council in terms of platting of property, but the Commission does not approve proposed infrastructure projects.

Mr. Wilson inquired if the road primarily would be on Dublin property, although private property is also involved.

Ms. Cox responded that is correct. Some right-of-way acquisition from the private property owner will be necessary.

Mr. Wilson inquired if discussion had been initiated, and the property owner was in agreement.

Ms. Cox responded that the property owner is aware of the project. However, the acquisition process cannot begin until design has occurred, plans are available, specifics land needs are known, and appraisals have been completed. Most of that work will occur next year.

Ms. Call stated that recent public meeting comments reflect a predominant concern regarding traffic. Were other concerns expressed at the last meeting that have not been addressed by the applicant?

Ms. Husak responded that a majority of the comments were infrastructure or traffic related. Some residents expressed concerns about the lighting. City Code requires lights to be lowered after certain times of the day, and a reference to that Code section is included in the development text. Details about the fixtures, lighting levels and isometric plans will be provided at the Final Development Plan review.

Ms. Call inquired if there were any items not addressed by the applicant.

Ms. Husak responded that there were not.

A rendering of the regional roadway network map was shown, depicting the proposed roadway extension to the north behind the Sports Ohio facilities and connection to existing University Boulevard near SR 161/Post Road. Ms. Cox stated that the distance is similar to the improvement from Avery Road to Eiterman Road. It will be a four-lane, divided roadway with bike lanes, consistent with the existing road section near the OU/SR161/Industrial Parkway roundabout.

Mr. Call inquired if there would be sidewalks on both sides of the roadway.

Ms. Cox responded that a shared-use path would be on one side and a sidewalk on the other.

Ms. Fox inquired if the bike lanes would have a protective barrier.

Ms. Cox responded that they would not; they would be striped lanes, similar to those on Muirfield Drive.

Ms. Fox inquired if there was any consideration for making them protected lanes.

Ms. Cox responded that was not requested by the Bicycle Advisory Task Force. However, the lanes will be widened to five feet, a foot wider than the bike lanes on the existing University Boulevard section.

Ms. Call inquired the width of the shared-use path.

Ms. Cox responded that it would be the standard 8-ft. width.

Ms. Fox stated that the WID has been under scrutiny for the last several years. Is there an updated, current Master Plan?

Ms. Husak responded affirmatively. An update of the WID Special Area Plan in the Community Plan was approved in 2017. Consultant Brian Atkins developed the plan, and public input was obtained.

Ms. Fox inquired if a master layout of the anticipated pedestrian connectivity was available, even though details could not be available until development occurs.

Ms. Husak responded that it is available to some extent. The Thoroughfare Plan, which is included in the Community Plan, shows a road connection from Shier Rings Road to the northwest. With the OSU project, that roadway will take on more importance than originally contemplated. Although a roadway was always planned, the developer or user that would eventually drive those decisions was unknown. With this infrastructure, walking paths and bike lanes will be important, particularly so in this District. OU also included this extension in their Master Plan for future development.

Ms. Fox stated that with the anticipated AEP project, realignment of Cosgray Ditch, construction of a new roadway, and a major new development, there is a need for a comprehensive overview of this intended "live, work, play" area. Although the development is at an early stage, it would be helpful to have an understanding of what is anticipated.

Mr. Husak pointed out that it is not possible to make 11,000 acres across the City walkable everywhere, but it does make sense for certain areas, particularly OU and OSU.

Ms. Call stated the concern is that we do not want to repeat past mistakes, for instance, those with the Perimeter Center.

Ms. Fox inquired what changes OSU had made responsive to the Commission's previous recommendations.

Ms. Husak responded that OSU had included walkability drawings for both development phases. Much work still needs to occur with the interior layout of the building, which will inform much of the exterior functionality of the site. Rules in the text address many of the items the Commission requested, and an updated drawing has been provided tonight.

Ms. Fox stated that in the parking lots, little islands with two trees remain in the plan, but there are no dedicated pedestrian walkways through the parking lots. Pedestrian benches have been added on the outer perimeter of the parking lots, but more seating areas are needed between the perimeter and the building. An Ambulatory Care Center must address the pedestrian needs.

## **Applicant Presentation**

Aaron Underhill, attorney, Underhill & Hodge, 8000 Walton Parkway, New Albany, OH stated that he is representing Ohio University Wexner Medical Center. With him are OSU representatives and consultants, Dan Like and Keith Myers.

As a brief overview, OSU took the Commission's input at the previous meeting very seriously regarding the need to be connected with the community, provide pedestrian access to/from the parking areas, and the creation of inviting pedestrian places. The reason they preferred to do a PUD was that it is difficult to provide a high level of detail at this point as they continue to design a very complex building. For instance, Ms. Fox referred to the pedestrian benches, which appeared to be located far from the front door; however, the parking lot area will have several such pedestrian seating opportunities. When they return with the Final Development Plan, they will be able to show a greater level of detail. This is the reason the two-step PUD process works well. In the first step, the rules are set; in the second, the execution of the rules is shown. With the existing zoning, they would not have had that opportunity, as it would have been necessary to show all the detail now. This is one of three facilities being designed simultaneously for the University. The building is continuing to evolve. The inside-out nature of the design affects how the building footprint works, what the architecture will be, and the exterior amenities designed for the patient experience. He believes the Commission will be pleased with the level of detail provided at the next review. They are comfortable with the rules being set. The plan is advanced sufficiently to be able to make those commitments, but it will continue to evolve between now and the time the Final Development Plan returns to the Commission. After the previous hearing, an attempt was made to evaluate opportunities for pedestrian connections and public spaces. They do not want this building to be an island in and of itself, so more progress will be made in that regard. The text is lengthy because it contains a high level of commitments. Sometimes it is hard to provide in words what can be seen on a plan, so they have tried to marry the two to the extent possible. This is a public-private partnership, which involved extensive work on the EDA, as well as the conveyance agreement. If the roadway is not operational, the medical facility will not be able to open; therefore, the timing of the two projects is important. Per the agreement, specific timelines must be met. The City did not want to sell OSU the land if they did not move forward with developing it. As soon as it is approved, the project will proceed immediately to permit designs. The university has a financial investment in the public infrastructure, as well. Although OSU is receiving an incentive package, they are purchasing the land at its per acre value and contributing 32.5% to the costs of the infrastructure. That percent was based on the amount of the infrastructure that the facility would use, per its frontage. The project began approximately a year ago and continues to evolve. When it next comes before the Commission, a greater level of detail will be available for discussion.

## **Questions for the Applicant**

Ms. Fox commended the applicant for presenting conceptual ways for providing a pedestrian perimeter for the medical facility, their clients and the community. Her request is that when Phase 2 proceeds, those pedestrian spaces are not minimized, that the natural spaces are not reduced to small spaces around walkways. With its style of architecture and the amount of landscaping contemplated, this site can become an attractive area for the community. The landscape design should not only highlight the building but also invite people to enjoy the surroundings. That is a wellness factor for their clients and for the community.

Keith Myers, Vice President of Planning, Architecture and Real Estate, OSU, stated that they agree with Ms. Fox's comments. The scale at which the improvements are drawn is deceiving, but to address the Commission's comments at the previous review, thousands of feet of trail have been added. Although not yet fully designed, seating enclaves will be added. There is a large greenspace between the parking lot and the drop-off area. They concur that the exterior spaces are as important to health as the interior spaces being constructed.

Ms. Call stated that OSU's contribution of 32.5% for the infrastructure, which is a savings for the residents, is appreciated. The Commission understands that there is an incentive package, but appreciates that OSU is willing to partner with the City on this amenity for the City.

### **Public Comment**

Randall Ayres, 5940 Roundstone Place, Dublin, Ohio, stated that he is a resident of the Ballantrae community. This project seems premature. The Planning staff member with whom he spoke indicated that staff was advised there was a need to move quickly on this project. At the public meeting in August, the Ballantrae community expressed significant opposition to the proposed plans for Shier Rings Road. Earlier, Mr. Supelak referred to several different maps that the Commission had seen. That has been the experience of the community, as well, and the residents are in opposition to most of them. Many Ballantrae residents travel Shier Rings daily, and they have been provided different information from City staff. At the August public meeting, Engineering staff informed them the project was very preliminary and that City staff would be getting back to them. This is September and neither the community nor the Commission know the plans because staff is unsure of them. The Commission is being asked to approve a project on which they have inadequate information and knowledge, and the community objects strongly to a change being made to Shier Rings Road. OSU representatives have commented on how much the University will be contributing; however, it pays neither real estate nor income taxes. While the City may be partnering with OSU, the University is not paying its share. The City purchased this property last year, and residents were told in March 2018 that this development would not happen for 5-15 years. Now this project is being pushed forward without sufficient time for people to understand any details. The Commission is being asked to make a decision without time to obtain adequate information. How will University Boulevard impact Shier Rings Road and the traffic, particularly the significant amount of eastbound traffic? How will Ballantrae residents access US33? How many roundabouts will be constructed? He has seen a map with three proposed roundabouts between Shier Rings and US33. In summary, he is concerned that the Commission is being asked to make a decision on a project about which staff is unsure of some plans.

Dan Rippeth, 5960 Roundstone Place, Dublin, Ohio stated that he is a Ballantrae resident and has lived in Dublin for seven years. He has spoken with many of his neighbors. Their biggest concern is not the hospital, but with the plans to shut down Shier Rings and the redirection of traffic up and around Eiterman Road and through a series of roundabouts. He asked for confirmation regarding the closing of Shier Rings Road.

Staff indicated that the road would not be closed.

Mr. Rippeth stated that if Shier Rings will not be shut down, that information addresses one of the residents' greatest concerns. The intersection at Eiterman Road and Shier Rings Road is dangerous. There was an accident there earlier this evening. There is a concern about the

generation of additional traffic. If Shier Rings remains with the addition of a roundabout at Shier Rings and Eiterman Roads, perhaps the traffic issue will be addressed.

Ms. Husak stated that when the project was introduced to the community at the August public meeting, significant opposition to the roadway plans was expressed by the Ballantrae residents. Consequently, the City is no longer considering the termination of Shier Rings Road. Another public meeting is scheduled for October 15 at the Dublin Community Recreation Center, and Engineering staff will be providing updated plans. Public notice of the meeting will be made. City staff has heard and responded to the residents' concerns.

Mr. Rippeth responded that the residents love living in Dublin and appreciate the fact that the City has listened to their concerns.

### **Commission Discussion**

Ms. Kennedy thanked Mr. Ayers and Mr. Rippeth for sharing their opinions. Although the Commission does read the public meeting reports, it is helpful to hear directly from the citizens.

Ms. Fox inquired about preservation plans. On the City-owned property, certain trees are at risk, due to construction: Tree #299, a 56-in. diameter Silver Maple and Tree #300, a 43-in. diameter Silver Maple, are located near the entrance. Is there any possibility those trees could be saved?

Ms. Cox stated that the trees, shown on page #10 of the information, are located opposite the City Service Center. That is where the confluence of the relocated creek will be located. They are required to plant a certain number of trees with the relocation of the stream, and may be able to save those two trees. She will check into the situation.

Ms. Fox responded that the relocated stream would border Shier Rings Road, which will be a viewshed. If there is opportunity to preserve those trees with the relocation of that stream, that should be included in their design focus.

Ms. Call stated that the Commission has commented on the need for synchronization of the building and roadway construction. Is the creek relocation, a third major project, not a timing concern, as well?

Ms. Cox responded that the creek relocation has reached 50% design completion and been submitted to the Army Corps of Engineers and the Ohio EPA for permitting. Staff will be responding to their feedback and completing the design. There will be a public notice in the Columbus Dispatch on Monday, per EPA public notice requirements. Permitting should be completed in the spring of 2020, and construction of the stream relocation will commence. The roadway construction will begin in 2021. A conservation easement is noted on the plat, and protective fences will be placed on the site. The new plants around the relocated creek must be protected and monitored for five years. Due to the creek and the level of traffic on Shier Rings Road, staff has been contemplating locating construction entrances off Eiterman Road to mitigate the impact of the large building and large roadway construction occurring at the same time.

Ms. Call inquired if the standard is a 2-to-1 vegetation mitigation for the stream relocation.

Ms. Cox stated that she does not have that information with her, but getting the planting established is a major concern. The stream must be healthy and the area viable for plants and animals.

Ms. Newell requested, for the benefit of the public, that staff clarify what is considered with traffic studies. There was a comment that the decision for a new road appears to be rushed. The Commission has information from the traffic studies that is not always available to the public.

Ms. Cox responded that the City has a Thoroughfare Plan. Over-arching traffic modeling for the entire City occurs based on Future Land Use Plans in the adopted Community Plan. That is how the size of roadways, including number of lanes, is determined. With a proposed development, the developer is required to look at their uses compared to the Future Land Use Plan. There are federal guidelines regarding the traffic generation based on land use. For instance, a restaurant generates traffic at a level different from a single-family home, and the traffic generated by a medical office is different from that for a regular office building. A comparison is made between the modeling for the adopted Community Plan and the modeling for the proposed development. If the anticipated traffic will be more, plans for handling the traffic must be determined based on its origination and destination. The prediction occurs by counting traffic at the existing intersections then projecting the traffic increase based on the anticipated land use. They try to determine what improvements might be needed to the intersections, to the links of the road between the intersections, and the impact of the development. The proposed development is required to mitigate their impact. That may be made by a financial contribution or by building turn lanes, a roundabout, or another intersection improvement.

Ms. Wawzkiewicz stated that the situation is the same with this application as with a standard planning application. The traffic impact study is due when the applicant commences the preliminary development plan. This team was a little ahead of schedule, so its study is under review. The unique element involved is the additional CIP roadway project. Dates were established in the EDA with the developer to make sure the timing is on track.

Ms. Newell inquired how many years out a traffic study projects.

Ms. Wawzkiewicz responded that a study looks at a 10-year horizon from the opening day. Year 2022 is the target opening of this facility, so the 10-year horizon would be 2032. The City's traffic study projects out to year 2042.

Ms. Husak stated that it is unique to have both the developer and the City conduct traffic studies and be evaluated together. Although the applicant's traffic engineer completed a study, the City was looking at potentially terminating Shier Rings Road. That traffic study is not approved at this time, however, because more work is to be completed.

Ms. Newell thanked staff for the explanation for the public.

Ms. Call stated that the Commission is considering a preliminary development plan for the medical center and a preliminary development plat for a new roadway, University Boulevard, and not the termination of Shier Rings Road. There will be a roundabout on the southeast section of the new roadway, a roundabout immediately in front of the building, and then the roadway continues to its connection with Eiterman Road.

Ms. Husak stated the new roadway ends at the eastern boundary. Whatever happens further to the east on Avery and Shier Rings Road is not part of this consideration.

Ms. Cox clarified that the plat creates the right-of-way, the parcels around the right-of-way, and the new road. They are still working out the details regarding the connectivity of Shier Rings Road, the drive into the Dublin School transportation facility, and the City Service Center. The preliminary plat lays out the worst-case scenario. Currently, those connections are contemplated via a roundabout. The plat allows that to occur, should that be determined.

Ms. Call inquired if the Commission would see this again as a Final Plat.

Ms. Cox responded affirmatively.

Mr. Supelak requested clarification that the intent is that a roundabout will make a connection to Shier Rings Road.

Ms. Cox responded that the connectivity of Shier Rings would happen in that area. They are completing the details on the accesses to the City Service Center and the Dublin Schools bus site. They believe this layout of the right-of-way provides sufficient area for those accesses without having to change the plat.

Mr. Supelak inquired if the two trees Ms. Fox was referring to are near the proposed roundabout.

Ms. Cox stated that those two trees are outside of the right-of-way.

Mr. Supelak responded that they are slightly to the west of it within a small curve. Trees #288 through #308 are an average of 30 inches each, a total of 600 caliper inches. Is it possible to give consideration to saving those trees?

Ms. Cox responded that it may not be possible to work around all of the trees. Perhaps only a couple can be saved, because that is the corridor for the realigned creek. They prefer to save trees, if possible, but there are competing factors. There is a required landscape density for the relocated stream. EMH&T experts will be conducting the planting to ensure the requirements are met to obtain the federal permit.

Mr. Fishman inquired if City Code required that the tree caliper be replaced.

Ms. Newell stated that it does; however, Federal EPA regulations usurp City requirements.

Ms. Cox stated that the City replaces the street trees removed for a capital project. This project also will have plantings in the medians. With those plantings and those added with the creek relocation, there will be more trees than exist now.

Mr. Fishman inquired if the City arborist conducts an assessment of the trees.

Ms. Cox responded that the condition assessment of the trees has been completed, and that report was included in the packet.

Ms. Husak stated that the arborist on Planning staff verifies species, size and health. This is a Preliminary Plat. More detail will be available with the Final Plat.

Ms. Kennedy inquired if the details on the Eiterman Road access would be provided for public review and comment.

Ms. Cox responded that the details will be available for the October 15 public meeting at DCRC.

Mr. Wilson inquired if it is a ditch or a creek that will be relocated.

Ms. Cox responded that it is named Cosgray Ditch, but it has been referred to as both a ditch and a creek. It is a manmade drainage channel for the farming community.

Mr. Wilson inquired if a ditch would not move as much water as a creek.

Ms. Cox responded that there are no such definitions. The relocated stream will handle the same amount of water that it currently handles. No flooding will occur up or downstream due to the relocation. It is an open channel relocation with meanders and a large amount of plants in a tiered system – grasses, shrubs and trees, within the 135-ft. conservation easement.

Mr. Wilson stated that the stream currently is shown as running south, then east and under the roundabout, then reconnecting with the current channel. Are they still determining the new route?

Ms. Cox responded that the stream currently exists on the south side of Shier Rings Road. They looked at several potential routes before selecting the one shown. The selected route works best with the roads and utilities and will provide a buffer to the residents on the south side of Shier Rings Road from the future commercial element. They are contemplating a shared-use path on the interior, which would provide pedestrians access to that greenway and tie Shier Rings to University Boulevard. The public will be able to enjoy both sides of the new waterway.

Ms. Newell inquired if the applicant was in agreement with the conditions.

Mr. Underhill indicated that they were in agreement with the conditions.

Ms. Kennedy moved, Mr. Wilson seconded to recommend approval of The Ohio State University Wexner Medical Center rezoning and preliminary development plan to Council with the following eight (8) conditions:

1. That the development text and the sign plan be updated to address the discrepancies prior to Council review, including the correct nomenclature and permitted sizes, permitted lighting and permitted number of secondary monument signs;
2. That the applicant update the development text to clarify that peninsulas may count in the interior landscape requirements;
3. That the smaller trees shown on the plans in the interior landscape areas be permitted on the development text;
4. That the development text be revised to include bio-retention measures, as appropriate, in the parking lot areas to aid in stormwater management and provide for irrigation of trees; this should include the elimination of the requirement of curbed islands;
5. That the applicant continue to work with Engineering to demonstrate compliance with stormwater requirements as defined in Chapter 53 to the satisfaction of the City Engineer;
6. That the building elements and mechanical equipment areas be screened per Code in the text;
7. That the applicant work with staff to identify areas where new trees are appropriate to reduce the need for future removal during Phase 2 and update the text accordingly; and,
8. That the applicant coordinate with ODOT for necessary approvals to remove any dead, invasive or hazardous vegetation along the frontage.

Vote: Mr. Supelak, yes; Mr. Fishman, yes; Ms. Kennedy, yes; Ms. Call, yes; Mr. Wilson, yes; Ms. Fox, yes; Ms. Newell, yes.

(Motion passed 7-0)



## 19-055Z/PDP – The Ohio State University Wexner Medical Center

### Summary

This is a request for review and recommendation of approval to City Council of a rezoning with preliminary development plan of a ±33.9-acre site from ID-1, Research/Office District to PUD, Planned Unit Development District for the future construction of up to 550,000 square feet of ambulatory medical care, medical and office uses as well as a potential future hospital in two phases.

### Next Steps

Upon approval a recommendation from the Planning and Zoning Commission the application will be forwarded to City Council for review and final approval. The legislation will require two readings before City Council with the rezoning in effect 30 days (referendum period) after the second reading.

### Site Location

South of US 33, north of Shier-Rings Road, east of Eiterman Road and west of Avery Road.

### Property Owners

City of Dublin

### Applicant

The Ohio State University Wexner Medical Center

### Representative

Aaron L. Underhill, Underhill & Hodge, LLC

### Applicable Land Use Regulations

Zoning Code Section 153.050-153.056

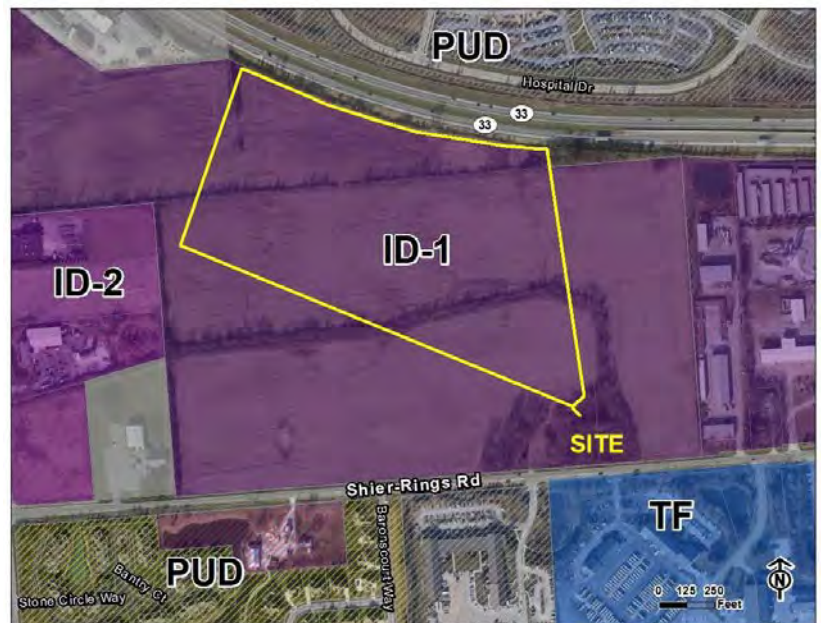
### Case Manager

Claudia D. Husak, AICP, Senior Planner/Current Planning Manager

(614) 410-4675



[chusak@dublin.oh.us](mailto:chusak@dublin.oh.us)

### Zoning Map



### 1. Context Map



|                                                                                                           |                                                                                            |                                                                                                                 |
|-----------------------------------------------------------------------------------------------------------|--------------------------------------------------------------------------------------------|-----------------------------------------------------------------------------------------------------------------|
|  <p>City of Dublin</p> | <p>19-055Z/PDP<br/>Rezoning/Preliminary Development Plan<br/>OSU Wexner Medical Center</p> | <p>0 250 500<br/>Feet</p>  |
|-----------------------------------------------------------------------------------------------------------|--------------------------------------------------------------------------------------------|-----------------------------------------------------------------------------------------------------------------|

## 2. Overview

### Background

The approximately 34-acre site includes portions of three parcels, all of which are owned by the City of Dublin. The largest parcel is approximately 60 acres and was annexed to the City in 2004 (Ord. 47-04) and the City of Dublin purchased the parcel for economic development purposes in 2016. The other two parcels are approximately 25 acres combined and were annexed in 1996 (Ord. 64-96) and purchased by the City in 2018. All three parcels were rezoned in 2011 from R, Rural District and R-1, Restricted Suburban Residential District to ID-1, Research/Office District as part of an on-going effort to aid in the implementation of the long-term vision of this area as an important Economic Advancement Zone and Innovation District. No development has been proposed on either parcel to date.

In 2019, City Council approved an Economic Development Agreement (Ord. 08-19) with The Ohio State University, which authorized the conveyance of this site to the University to establish a comprehensive ambulatory medical facility, consistent with the City's Economic Development Strategy to encourage development and create and preserve employment opportunities within the City.

On August 22, 2019, the Commission reviewed and provided non-binding comments on an application for a Concept Plan. The Commission complimented and welcomed the applicant for choosing this location for their proposed facility as well as the quality and layout of the proposal. Commissioners suggested the applicant provide additional paths throughout the parking lots to break up the large parking areas and provide additional opportunities to navigate to the main building entrances. Seating along walkways was encouraged closer to the drop off at main entry areas. The Commission also requested that the applicant provide more opportunities to walk or bike through the natural areas along the site boundaries. Another comment requested more detail for the architecture of the buildings to truly create signature building within the West Innovation District. The Commission also requested additional information regarding the planned roadwork network surrounding the site.

A representative of the Ballantrae Board of Directors provided comments regarding the neighborhood's appreciation of OSU as a neighbor to their community and requested that the connectivity of the neighborhood to the improved roadway network be considered. Additional public comment centered on the connectivity of businesses along Eiterman Road to and from the north and the potential impact of an intersection with the road serving OSU and Eiterman Road and the relationship between the anticipated traffic and the facility.

### Site Characteristics

#### *Natural Features*

The site is comprised of three parcels of unimproved, vacant, agricultural land. The property is rectangular in shape and generally flat. The southeastern portion of the subject property was formerly developed with farmstead structures, but is currently vacant.

A farm drainage ditch "Cosgray Ditch" runs east/west through the center of the western parcel, then turns south between the two southern parcels. The ditch is in a City Stream Corridor Protection Zone, which the City has studied for relocation during Fall 2016 and the US Army Corps of Engineers has issued the public notice for this application on August 16, 2019.

### *Historic and Cultural Facilities*

The site does not contain any known historically contributing structures or artifacts.

### *Surrounding Zoning and Land Use*

North: US 33

East: ID-1, Research/Office District (Avery Road Industrial Park, industrial and storage)

South: PUD, City of Dublin Service Center and TF, Tech Flex District, City of Dublin Fleet Maintenance and Dublin City Schools Transportation Department

West: Washington Township (church) and ID-2, Research/Flex District (Washington Township Administration building)

### *Road, Pedestrian and Bike Network*

The site has frontage, but no direct access to US 33 to the north. A new transportation network will be needed to access the site.

### *Utilities*

The site is currently not served by public utilities.

## **Proposal**

This is a proposal for a medical campus to be developed in two phases. The applicant has stated that the initial phase of the project is anticipated to include an ambulatory care facility, medical office space, and other related uses. The term ambulatory care facility will provide outpatient medical procedures, testing, and treatment in addition to traditional physician consultations. Phase I will include approximately 250,000 square feet and Phase II may expand the first phase by up to an additional 300,000 square feet to include additional medical and related facilities.

## **Community Plan**

<http://communityplan.dublinohiousa.gov/>

The Community Plan is the key policy guide for decision-making about the built and natural environments of the City of Dublin. The Community Plan text and associated maps contain detailed recommendations for future development including the appropriate location and density or intensity of residential and commercial uses; the general location and character of roads; the general location of parks, open space and public buildings among other information.

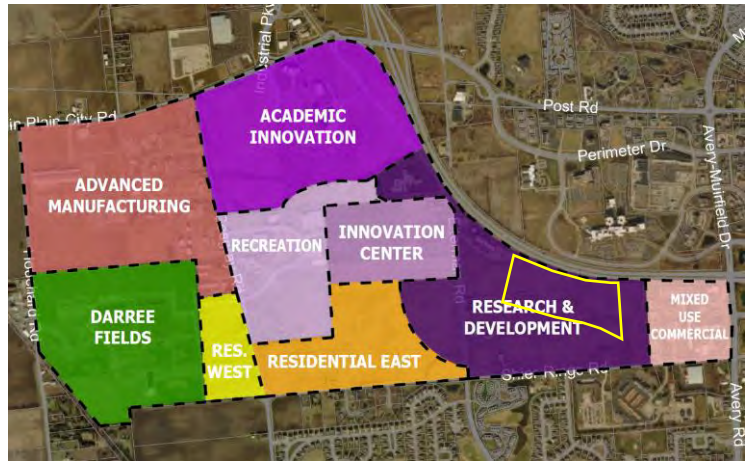
The Community Plan is a guide for City Council and the Planning and Zoning Commission as they assess the location, character, and extent of proposed public and private development in Dublin. The policies and recommendations within the Plan will be implemented over time through rezonings and subdivisions of land and the location and construction of public improvements by either the City or a developer.

Recommendations throughout the Plan are based upon a review of existing conditions and evaluation of future development scenarios for their impacts on infrastructure, roads and the fiscal health of the City. Dublin's ability to maintain high quality of services and quality of life depends on a careful review of development proposals for conformance with the Community Plan.

### Future Land Use

The West Innovation District (WID) is designated as a Mixed Use Regional Center on the Future Land Use Plan. While an important district of the City, it serves a dual role as a regional economic node.

The characteristics of the Mixed Use Regional Center for the WID include a mixed-use environment of integrated academic, research, office and advanced manufacturing uses that are supported by residential, retail, personal services, entertainment and open spaces as amenities.



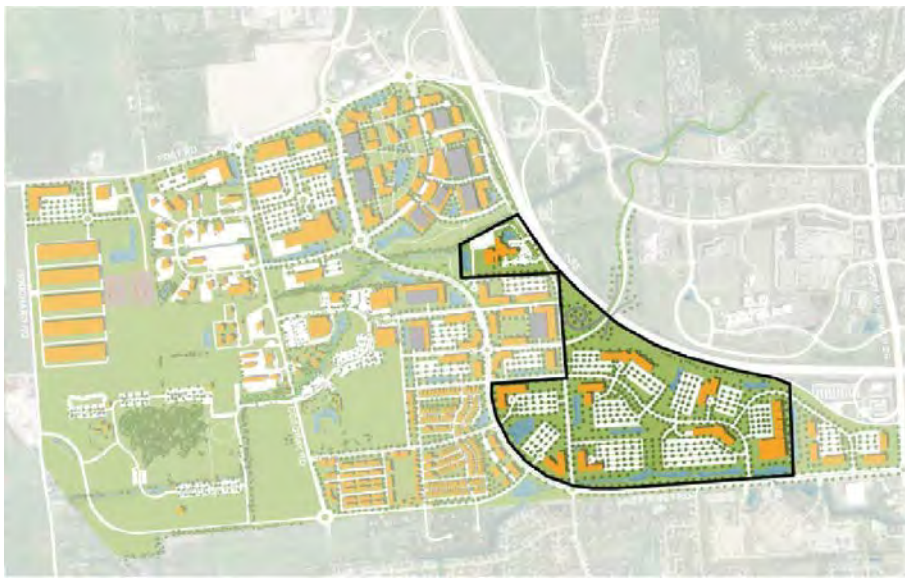
The Community Plan shows the future land use for this site as *MUR-Research and Development*, which is an area reserved for mature innovation companies that desire the autonomy of their own site and require ample space for expansion. This sub-district offers a limited number of larger sites with ample parking, as well as visibility along U.S. 33.

### West Innovation District Special Area Plan

<http://communityplan.dublinohiousa.gov/special-area-plans/west-innovation-district/>

Special Area Plans provide a more in-depth depiction of how the Land Use and Transportation Plans may be implemented in key areas of the city that are expected to undergo significant change. These elements of the Special Area Plan serve to guide decision-making regarding the appropriateness of development proposals and the infrastructure improvements necessary to support future development. Special Area Plans are conceptual and intended as general guides to indicate potential development options. The concepts are schematic only, and the actual mix of land uses, locations and configurations of buildings, parking areas, streets and access points will be determined through the public review process for individual development proposals.

In 2017, City Council approved Ordinance 69-17 for an amendment to the West Innovation District (WID) Special Area Plan as part of the City of Dublin Community Plan. The WID



encompasses 1,100 acres east of Houchard Road, west of Avery Road, north of Shier-Rings Road and south of US 33/State Route 161/Post Road.

Following adoption of this update to the WID Special Area plan, it is the intent of staff to initiate an amendment to the existing WID

zoning to ensure consistency between the adopted plan and the zoning district.

The District is comprised of primarily undeveloped land and provides vast opportunities for walkable, mixed use, research and innovation development sites that can meet a host of business needs.

The District is designated as a *Mixed Use Regional Center* in the Plan. This designation reflects the functional role this district plays from a land use perspective. While an important district of the City, it serves a dual role as a regional economic node. Characteristics of this designation include a mixed-use environment with integrated land uses, densities that support a walkable built environment, sites that are development for a variety of corporate needs that range from the traditional suburban office uses to more walkable urban centers with higher intensity of land uses.

Appropriate land uses include office, research, medical with parking and laboratories listed as secondary uses. The Area Plan recommends a maximum density of 30,000 square feet per acre and a maximum story height of three stories. At a maximum build-out of 550,000 square feet on 34 acres, the proposed density is approximately 16,000 square feet per acre, which meets the Community Plan.

The Area Plan also addresses architectural character and materiality with an emphasis on modern architecture. Architectural design should not be overly restrictive in the District relative to the composition of the architectural character of buildings that would limit creativity. Glass, metal, stone, brick, concrete and wood.

### **Thoroughfare Plan**

The Thoroughfare Plan map shows the needed width of right-of-way and identifies the number of lanes needed to accommodate year 2035 development in Dublin.

The Thoroughfare Plan recommends 100 feet of right-of-way for existing Shier-Rings Road and 70 feet for Eiterman Road. The Thoroughfare Plan includes planned roadway connections throughout the WID and currently depicts Shier-Rings Road moving north toward the Ohio University campus west of Eiterman Road. The Special Area Plan also shows this layout with a street connection from existing Shier-Rings Road east of Eiterman Road through the subject site north to intersect with Eiterman to provide access to the sites between these two roadways, south of US 33.

### **Neighborhood Contact**

The HOA leadership of the Ballantrae Community as well as a few interested residents invited City staff to their recent meeting, which took place on September 5, 2019. During the meeting, staff had a chance to provide updates on the infrastructure network changes due to the roadway proposed to serve the proposed OSU facility and other adjacent sites. Based on the feedback from the earlier public meeting, staff is now moving forward with the existing Shier-Rings Road to remain in its existing location and as a way for Ballantrae residents and others to access Avery Road from the west. Staff will continue to work on finalizing the needed improvements including access arrangements for the public and private businesses along this corridor.

The applicant and the City of Dublin held a public meeting on August 8, 2019 to introduce the proposal and necessary transportation network improvements. Attendance at this meeting was approximately 100. Residents of Ballantrae voiced concerns focusing on the potential impact of roadway network changes on their neighborhood, particularly due to a current study to terminate existing Shier-Rings Road in a cul-de-sac, east of the neighborhood. Concerns were raised that this option would cause non-neighborhood traffic to use streets in Ballantrae to travel south or east. Other concerns centered on the potential of this proposal to increase crime in the area and decrease property values. Some residents appreciated the efforts made to relocate the through movement and access to the OSU site north from existing Shier-Rings and the ability to create buffering between the medical center and the neighborhood. Some residents also requested the City study the northward extension of Baronscourt Way to intersect with relocated Shier-Rings Road.

Staff has transcribed the comments made on comment cards during and after the meeting, which are included in the packet. In addition, Public Works has received e-mail correspondence resulting from meeting attendance, which has also been added to the transcript. Lastly, emails were also received after the Planning and Zoning Commission meeting notice was published. Those emails are also included.

## **Proposal Details**

### *Layout*

The proposal includes two site plans, one for Phase I and one for Phase II. The plans show the relocated of Shier-Rings Road as the southern site boundary. The Phase I plan shows a medical office building and the Ambulatory Care Facility. The building is proposed in the center of the site along the US 33 frontage with the main entrance located along the south façade. Parking in three distinct lots is located along the western portion of the site and in the southern portion of the site north of relocated Shier-Rings Road/University Boulevard in the Phase I plan. Drive aisles for circulation to the main entrance are indicated with walking paths connecting the parking to the front of the building. Along the southern façade, the plans show ample open space and landscape area to provide outdoor space for visitors, patients and staff. Several paths are also indicated along the northern portion of the site buffered from US 33 with trees and landscaping. In the Phase I plan, there is lawn space provided where the building and additional parking for Phase II will be located.

Both Phase I and Phase II plans show stormwater retention ponds for the site in the northwest, northeast and southeast. Access is shown off relocated Shier-Rings Road/University Boulevard with a roundabout as the main access point and secondary access in the west and east.

Phase II indicates an expansion of the parking lot east of the main entrance as well as an additional parking lot to the east between the two retention ponds. A hospital building is indicated to be attached to the Ambulatory Care Facility. Phase II continues a landscape theme along the front (south) façade to provide separation between drive aisles and to create high quality greenspaces to between the parking and the building. The access off relocated Shier-Rings Road does not change with Phase II, however there is a cross access drive aisle show for the site to the east.

### *Zoning*

While the site is shown in the Community Plan Special WID Area Plan as Research and Development, the Zoning Code has not yet been updated to reflect this District and the site

retains the current zoning of ID-1, Research/Office District. All the proposed uses included in this proposal are permitted within the ID-1 District. Due to the unique nature and needs of a medical facility of this size and associated site improvements, the applicant and staff have agreed that a rezoning from ID-1 to a Planned Unit Development District (PUD) is the appropriate mechanism to ensure that the needs of the facility can be met. In addition, the City retains a higher level of involvement in ensuring the development will be sensitive to the surrounding area, provides for a high quality and cohesive development with appropriate development pattern, landscaping, and signs.

#### *Site*

As discussed above, the 34-acre site is a portion of three City-owned parcels. A new parcel and right-of-way will be created to facilitate this development. The City has prepared a preliminary plat application (Case 19-080PP) to coincide with the review of this rezoning application.

#### *Access*

Three driveways into this site from a new public roadway (University Boulevard) will provide vehicular access. A roundabout is planned at the main entrance and the other two driveways will be at traditional intersections. A Traffic Impact Study (TIS) has been performed by the applicant and submitted to staff for review. This study reviews the intersection control at the proposed driveways as well as several offsite intersections. Staff is working on the review of the study in coordination with the proposed public improvements to be constructed by the City. The applicant has agreed to participate in the funding of the public roadway improvements that will be built by the City. The site plan includes cross access to the parcels/developments that will be east and west of this site. This will aid in distributing the traffic onto the new roadway.

#### *Utilities/Stormwater*

This site will have access to public water for domestic and fire protection service by connecting public water main that will be constructed with the University Boulevard project. Two separate water meter vaults will be placed at the western most and eastern most site drives. This site will have access to public sanitary sewer service by connecting services to public sanitary mains that will be constructed with the University Boulevard project. This site is located within the Cosgray Sanitary Sewer shed.

In terms of stormwater management, this site has proposed the construction of three separate wet retention basins. Two of the basins are proposed along the US 33 frontage, the third is proposed to the east of the eastern most drive. These basins are designed to meet the requirements of the City of Dublin's Chapter 53 Stormwater Management Code, and the requirement of the Ohio EPA. A preliminary stormwater management report has been submitted to demonstrate compliance with these rules and regulations.

#### **Development Text**

The development text is the regulating document that outlines the development standards for the development including uses, lot requirements, and architecture and materials. The applicant has provided a development text that largely lays out similar to the existing zoning regulations for the West Innovation District as part of the Zoning Code.

#### *Uses*

The proposed text excluded a number of the uses currently permitted or conditionally permitted within the ID-1 District. It is the intent to develop this site with the ambulatory care



facility/medical offices/hospital uses and, in the future, allow support services on adjacent parcels. Those prohibited uses include eating and drinking, personal services, animal services, daycare, industrial uses and residential.

Permitted uses are medical office, hospital, educational as related to post-secondary education, community education or medicine. Daycare, retail, parking structures, exercise and fitness are permitted as an accessory uses as is eating and drinking. Lastly, as currently permitted in the ID-1 District, the proposed text allows a heliport as a permitted accessory use as long as the use complies with Zoning Code Section 153.094, which regulates heliports.

#### *Development Standards*

The proposed text requires a minimum lot size of three acres and limits lot coverage to a maximum of 75%, which exceeds the Code by 5%.

The WID Special Area Plan suggests a maximum height of three stories in this portion of the District, while the Zoning Code does not limit the height of buildings in ID-1, but requires larger setbacks for taller buildings (75-foot side and rear setbacks for buildings taller than 51 feet). The proposed development text permits a height of up to 110 feet and seven stories. When questioned by residents during the Neighborhood Meeting as to the need for the height of the buildings, the applicant responded that interior space planning and efficient interior circulation mandate the height, while also allowing for a smaller building footprint. The building has been sited as far north on the site as possible to provide separation and space for buffer to the south.

Setback requirements are 100 feet from U.S. 33 for parking and pavement, with exceptions for walking paths, ground-mounted mechanical equipment that is completely screened and grass pavers required for vehicular access to any equipment. Along proposed University Boulevard pavement setbacks are required to be 25 feet and building setbacks 100 feet. Side yard setbacks are to 15 feet for pavement and 50 feet for buildings. The applicant should clarify the development text that paths, outdoor amenity spaces or decorative walls are exempt from the setback requirements.

#### *Architecture*

The development text explains the unique architectural needs and building design for this type of facility as well as a facility operating under the OSU brand. As stated during the Concept Plan review, a significant amount of interior design is still to take place prior to finalizing all elevations, which will be required to be reviewed and approved by the Planning and Zoning Commission with the Final Development Plan. The applicant has provided the conceptual elevation previously shared with the Commission.

The text states that the goal of the architecture for the facility is to be high-quality with an attractive aesthetic design to meet and exceed the City's standards for quality of materials and design as a focal point in the community.

Requirements include a similar treatment of materials on all elevations with uninterrupted, blank facades prohibited if visible from the right-of-way or adjacent property. Permitted materials include brick, brick veneer, stone, stone veneer, manufactured stone, cast stone, architectural pre-cast concrete, metal, wood (including synthetic) and glass. Reflective or

mirrored glass is prohibited. Other prohibited materials include prefabricated metal and untreated masonry block.

The text permits building elements such as meter boxes, utility conduits, vents and exhaust pipes as well as dumpster to be designed, located or screened so as to minimize the visibility from off site. Planning requires that the text be revised that these elements be screened per Code in addition to the location requirements in this text. This should also apply to any mechanical equipment areas.

### *Landscaping*

The text refers to the illustrative drawings and the preliminary landscape plan to illustrate the intended landscape treatment. The text requirements align with Code requirements. The text requires open space plantings for every 2,000 square feet of open space, while the Code requires this treatment for every 1,000 square feet. Staff is aware that the Code requirement is not practicable for the development pattern envisioned in this area and supports this deviation. The applicant should include peninsulas in the interior landscape requirements of the proposed development text, as some areas may count to meet Code. Both islands and peninsulas are required to include one tree, which is shown on the plans as 1.75 inches in caliper; this should also be included in the text. The text should also include bio-retention measures, as appropriate to be included in the parking lot areas to aid in stormwater management and provide for irrigation of trees, this should include the elimination of the requirement of curbed islands.

The Preliminary Development Plan continues to include details addressing the open space locations and programming for the site. In addition, new drawings have been provided to address made by the Commission during the Concept Plan review to illustrate pedestrian movements throughout the site as well as from parking areas to the building. Entry walls are shown to be of a modern limestone design to reflect the heritage of the City while also indicating the desire for modern design and innovation in the District. A courtyard for a café and a respite garden are proposed between in the center of the building on the south side, while an entry court as well as an entry green are designed to enhance the arrival experience to the facility whether walking from the parking lots or using the drop-off area. Landscape buffers are shown along all side of the site.

### *Tree Preservation and Replacement*

The applicant does not require replacement of trees installed prior to Phase II that are removed during that construction in the development text. Staff prefers that the applicant work with staff to identify areas where new trees are appropriate to reduce the need for future removal. The text should be updated to indicate this preference. The text requires adherence to the Code for the remainder of the preservation and replacements requirements. Certain removals indicated on the plans along the U.S. 33 frontage identifies incorrect species, which should be addressed in the Final Development Plan. The applicant should also coordinate with ODOT for necessary approvals to remove any dead, invasive or hazardous vegetation along the frontage.

The Preliminary Development Plan shows significant upsizing in replacements trees planted throughout the site. While it is commendable that the applicant is attempting to provide as much replacement on-site rather than paying a fee, staff is concerned that availability and confirmation of these significant number of trees and at the sizes as currently shown will be difficult.

### *Parking*

The text requires 2.25 parking spaces per 1,000 square feet of building area for this PUD, while the Zoning Code breaks out parking requirements by each individual use. Given the unique nature of this facility and the potential for various accessory uses as well as experience the applicant has with this type of facility, Staff supports this parking requirement. Additionally, it will significantly simplify the future administration of parking should uses or the size of uses change. Bicycle parking is required at one space per every 10,000 square feet of building.

### *Lighting*

The proposed text requires adherence to the Zoning Code for lighting regulations, all of which will in detail reviewed as part of the Final Development Plan. The Code addresses height and lumen levels of fixtures and permitted levels of lighting at the property line. For parking lots exceeding 150 spaced, reduced lighting levels after 10 p.m. are required.

### *Signs*

The applicant stated during the Concept Plan review as well as shared with staff, that the sign requirement for this type of facility are unique and flexibility and user experience will play an important role to the signage approach. The text requires approval of a sign plan as part of the Final Development Plan to provide additional details. The text refers to the sign plan for permitted sizes. However, the sign plan does not match the sign types in the text. This should be addressed prior to submitting for Council review and the text and plan should be updated to include the correct nomenclature, sizes and permitted lighting.

The text includes seven sign types: Building Identification Signs, Ancillary Wall Signs, Window Signs, Primary Entry Monument Signs, Secondary Entry Monument Signs, Directional Signs and Regulated Signs.

- **Building Identification Signs:** four such signs are permitted per the development text. Per the plan, this sign type is approximately 300 square feet, which meets the maximum sign size for buildings with Interstate frontage. The text states that the final locations and dimensions of these signs (with such dimensions not to exceed those shown in the accompanying sign plan) shall be provided by the applicant with the final development plan, with the provision that one such sign shall be located on the west-facing elevation of the medical office building, another shall be located on the east-facing elevation of the same building, and the third and fourth shall be located on the south-facing and north-facing elevations of the ambulatory care facility. Each sign is permitted to be located on the uppermost story, which is the mechanical room of each building. Allowances for sign relocation are included when the building in Phase II is constructed.
- **Ancillary Wall Signs:** the text does not include a number permitted for additional wall signs but states that their purpose is to identify particular uses within the building, to provide addresses for such uses, and to promote efficient wayfinding into the building(s). The applicant defers the details of such signs to the Final Development Plan.
- **Window Signs:** window signs are permitted based on the ID-1 District allowances, which limits them to one per building or use when the use has an individual entrance.
- **Primary Entry Monument Sign:** The text permits one such sign either east or west of the main entrance to the facility. The sign plan shows the height of this sign to be approximately 12 feet and the overall size is 66 square feet. At the Final

Development Plan Stage, the applicant will have to verify that this sign does not impede visibility for this intersection.

- Secondary Entry Monument Signs: the text permits two of these types of signs at the westernmost and easternmost access drive into the site. The sign plan shows that these signs may be five feet tall and 3'6" wide for a total size of 17.5 square feet. The sign plan indicates four locations for these signs while the text limits them to two. The applicant needs to address this discrepancy.
- Directional Signs: As is the case for these large medical facilities, directional signs primarily designed for vehicular use are an essential wayfinding element. The proposed text does not limit the number of directional signs and the plan provides for a height and size that exceeds Code. The height is shown at 7'10" with a size of 17.5 square feet. All other details are deferred until the next review by the Commission. After approval of the sign plan as part of the final development plan, changes may be approved administratively.
- Regulated Signage: The text includes this sign type due to mandated specifications pursuant to federal and/or state laws and other regulations and allows such signs shall be permitted in accordance with laws and regulations while not requiring review and approval as part of a Final Development Plan.

### 3. Proposed Plan



## 4. Criteria Analysis

### Rezoning with Preliminary Development Plan Analysis [§153.055A]

- 1) The proposed development is consistent with the purpose, intent and applicable standards of the Zoning Code;  
Criterion met with Conditions. This proposal is consistent with the purpose, intent and applicable development standards of the Zoning Code requirements, except as altered in the proposed development text to create unique and specific standards for this proposal.

Sign allowances for this development appropriately deviate from the Code given that wayfinding requirements for this type of facility are unique and flexibility and user experience play an important role to the signage approach. However, there are discrepancies between the proposed development text and the sign plan, which must be addressed prior to Council review, including the correct nomenclature and permitted sizes, permitted lighting and permitted number of secondary monument signs.

- 2) The proposed development is in conformity with Community Plan, Thoroughfare Plan, Bikeway Plan, and other adopted plans or portions thereof as they may apply and will not unreasonably burden the existing street network;  
Criterion Met. Based on previous discussions with the Commission, the proposal was deemed to be largely consistent with the Community Plan recommendations, particularly given the high quality of open space areas, the site layout and architecture.
- 3) The proposed development advances the general welfare of the city and immediate vicinity and will not impede the normal and orderly development and improvement of the surrounding areas;  
Criterion met. This proposal along with planned infrastructure improvements will provide for an orderly development and improve the surrounding area as one of the first developments in the WID. It meets the Community Plan for permitted uses as well as the desired design of sites within the District.
- 4) The proposed uses are appropriately located in the city so that the use and value of property within and adjacent to the area will be safeguarded;  
Criterion met. The development is appropriately located within the City in the West Innovation District and is an example of the type of development for which the District was first established and continuously updated as a Special Area Plan.
- 5) Proposed residential development will have sufficient open space areas that meet the objectives of the Community Plan;  
Criterion Not Applicable. There is no open space provision required for non-residential development. The applicant has; however included ample open spaces within this site for different types of open space activity and patient and staff needs. In addition, plans includes, paths, benches and formal and informal plantings for open space.
- 6) The proposed development respects the unique characteristic of the natural features and protects the natural resources of the site;  
Criterion met. The development text addresses tree preservation and replacement adequately and the plans cluster development so as to retain more trees on site.

- 7) Adequate utilities, access roads, drainage, retention and/or necessary facilities have been or are being provided;  
Criterion met. The applicant has worked extensively with staff to ensure the proposal includes adequate utilities, roads, drainage, and retention facilities to serve the proposed development.
- 8) Adequate measures have been or will be taken to provide ingress and egress designed to minimize traffic congestion on the surrounding public streets and to maximize public safety and to accommodate adequate pedestrian and bike circulation systems so that the proposed development provides for a safe, convenient and non-conflicting circulation system for motorists, bicyclists and pedestrians;  
Criterion met. Access to the proposed lots will be provided from Shier Rings and Eiteman Roads via University Boulevard, which is a responsibility of the City of Dublin, per the Economic Development Agreement. University Boulevard will be an arterial street with four travel lanes and bike lanes. The applicant is providing paths from University Boulevard to the site and throughout the site.
- 9) The relationship of buildings and structures to each other and to such other facilities provides for the coordination and integration of this development within the PD and the larger community and maintains the image of Dublin as a quality community;  
Criterion met. The proposal sites the facility as far north on the site as possible while also providing open space amenities along U.S. 33. The development text includes requirements for high-quality building materials and site landscaping, screening and buffering.
- 10) The density, building gross floor area, building heights, setbacks, distances between buildings and structures, yard space, design and layout of open space systems and parking areas, traffic accessibility and other elements having a bearing on the overall acceptability of the development plans contribute to the orderly development of land within the city;  
Criterion met with Condition. The proposed layout and intensity are appropriate for this site in the West Innovation District. There are several landscape requirements in the development text and on the plans that should be revised to ensure that the open space areas thrive and the parking areas are appropriately landscaped. The applicant should include peninsulas in the interior landscape requirements of the proposed development text as some areas may count to meet Code. Both islands and peninsulas are required to include one tree, which is shown on the plans as 1.75 inches in caliper; this should also be included in the text. The text should also include bio-retention measures, as appropriate to be included in the parking lot areas to aid in stormwater management and provide for irrigation of trees, this should include the elimination of the requirement of curbed islands.
- 11) Adequate provision is made for storm drainage within and through the site so as to maintain, as far as practicable, usual and normal swales, water courses and drainage areas;  
Criterion met with Condition. The development includes provisions for stormwater management via storm sewer and retention basins. The applicant will be required to continue to work with Engineering to demonstrate compliance with stormwater requirements as defined in Chapter 53 to the satisfaction of the City Engineer.

- 12) The design, site arrangement, and anticipated benefits of the proposed development justify any deviation from the standard development regulations included in the Zoning Code or Subdivision Regulation, and that any such deviations are consistent with the intent of the Planned Development District regulations;  
Criterion met with Conditions. The proposed design, site arrangement and anticipated benefit to the City will be ensured through the proposed development text, which largely matches the Zoning Code for the ID-1 District. Certain deviations as noted below appear unintended or unnecessary and the applicant should address them prior to submitting for Council review.

The text permits building elements such as meter boxes, utility conduits, vents and exhaust pipes as well as dumpster to be designed, located or screened so as to minimize the visibility from off site. Planning requires that the text be revised that these elements be screened per Code in addition to the location requirements in this text. This should also apply to any mechanical equipment areas.

Staff prefers that the applicant work with staff to identify areas where new trees are appropriate to reduce the need for future removal when Phase 2 is under construction. The applicant should also coordinate with ODOT for necessary approvals to remove any dead, invasive or hazardous vegetation along the frontage.

- 13) The proposed building design meets or exceeds the quality of the building designs in the surrounding area and all applicable appearance standards of the city;  
Criterion met. The preliminary development includes character images for the proposed building as well as for the outdoor amenities and landscape areas. The text includes material and design standards, which will create a cohesive high quality development.
- 14) The proposed phasing of development is appropriate for the existing and proposed infrastructure and is sufficiently coordinated among the various phases to ultimately yield the intended overall development;  
Criterion met. The development will be completed in two phases and the plans and text address this intent appropriately.
- 15) The proposed development can be adequately serviced by existing or planned public improvements and not impair the existing public service system for the area;  
Criterion met. The development will be adequately serviced by planned infrastructure improvements and the completion of those is the responsibility of the City, therefore they are public projects held to the same standard as all City infrastructure projects.
- 16) The applicant's contributions to the public infrastructure are consistent with the Thoroughfare Plan and are sufficient to service the new development.  
Criterion met. All contributions to infrastructure have been agreed upon and approved as part of the Economic Development Agreement.

## 5. Recommendations

The proposal is consistent with all of the applicable review criteria contained in the Zoning Code and **Approval** is recommended with the following conditions:

- 1) That the development text and the sign plan be updated to address the discrepancies prior to Council review, including the correct nomenclature and permitted sizes, permitted lighting and permitted number of secondary monument signs;
- 2) That the applicant update the development text to clarify that peninsulas may count in the interior landscape requirements;
- 3) That the smaller trees shown on the plans in the interior landscape areas be permitted on the development text;
- 4) That the development text be revised to include bio-retention measures, as appropriate; in the parking lot areas to aid in stormwater management and provide for irrigation of trees, this should include the elimination of the requirement of curbed islands;
- 5) That the applicant continue to work with Engineering to demonstrate compliance with stormwater requirements as defined in Chapter 53 to the satisfaction of the City Engineer;
- 6) The building elements and mechanical equipment areas be screened per Code in the text;
- 7) That the applicant work with staff to identify areas where new trees are appropriate to reduce the need for future removal during Phase 2 and update the text accordingly; and,
- 8) That applicant coordinate with ODOT for necessary approvals to remove any dead, invasive or hazardous vegetation along the frontage.