



To: Public Services Committee of Dublin City Council

From: Dana L. McDaniel, City Manager

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Re: Mobility Study Update – Phase 5 Next Steps

Summary

This memo summarizes the next steps for Phase 5 of the Dublin Mobility Study.

Mobility Study Phases 1-4 Background

Implementation strategies for mobility priority areas identified in Phase 1 and Phase 2 of the Mobility Study were the focus of Phase 3, which began in September 2018 and continued through early 2020. Staff provided a summary of Phase 3 outcomes and next steps for Phase 4 to Council's Public Services Committee (PSC) on May 4, 2020. Staff and mobility consultant Nelson\Nygaard provided an update of mobility initiatives identified in Phase 4 of the Mobility Study to PSC on October 5, 2020. Phase 4 focused on expanding mobility pilots into permanent mobility programs and services.

On May 3, 2021, staff provided an update on three strategic mobility priority areas including shared micro-mobility, mobility hubs, and secondary wayfinding on shared-use paths. The Public Services Committee provided feedback and supported staff's recommendations for implementation of these strategic mobility priority areas. With respect to shared micro-mobility, the PSC was supportive of staff continuing to develop the Bird Scooters and TRIP e-bikes programs in addition to planning an expansion of the CoGo Bikes Share into Dublin. The Public Services Committee acknowledged the current Code prohibits the use of some micro-mobility vehicles. The PSC is supportive of a Code suspension and associated demonstration project for shared micro-mobility in Historic Dublin, the Bridge Street District, and along the Frantz Road corridor. The topics of curb management, education, and more bike facilities were also discussed. With respect to mobility hubs, the Public Services Committee stressed placemaking components and amenities such as Wi-Fi hotspots and shelters. The Emerald Trail and proposed secondary wayfinding on shared-use paths were well received by the Public Services Committee. Staff provided an overview of six proposed bike loops with the follow themes:

1. Public Art
2. Recreation
3. Ice Cream
4. Celtic Cocktail Trail
5. Irish Fairy Door Trail
6. Waterfalls and Natural Features

The PSC suggested the addition of a fishing-themed bike loop and asked staff to incorporate vertical signage for wayfinding.

As the Mobility Study transitions from Phase 4 to Phase 5, staff has identified the following next steps to guide implementation of mobility projects in the next phase.

Phase 5 – Next Steps

Addendum 7 of SHARE PSA

Ridership on the Share Shuttle has increased by nearly twenty-six (26) percent from January through May of this year. While working to increase ridership, the program enjoyed a thirty-six (36) percent decrease in operating costs since March, primarily due to the use of a city-owned vehicle in place of a SHARE-owned vehicle. The lower operating costs combined with an increase in mobility funding in 2021-2025 Capital Improvement Program creates an opportunity to expand services in the second half of 2021. Expansions to the service in conjunction with the Aging in Place initiatives will include fare-free transportation to non-emergency medical appointments and expanding service to Dublin's community and special events. Given the circumstances of the COVID-19 pandemic, SHARE's Professional Services Agreement was extended twice through PSA Addendums 5 & 6 in 6-month increments with a limited budget of \$100,000 for each extension. It is anticipated the cost to restore operations to pre-COVID levels and simultaneously expand the scope of services would be approximately \$200,000 for 6 months of service from July 1, 2021 to December 31, 2021.

Discussion Request

What are the next opportunities for service expansions regarding the Dublin Connector?

Figure 1: Dublin Connector Shuttle



Figure 2: Map of Focus Area

Curbside Management Program Study

A curbside management program study would define an overall plan for this valuable and flexible public space in areas such as Bridge Park and Historic Dublin. To better manage the important asset, a clear, consistent, and implementable framework for prioritizing goods and people movement is needed. A thoughtful framework will improve urban goods delivery while reducing conflicts between trucks, motor vehicles, on-street parking, handicap parking, loading zones, bicyclists, pedestrians, micro-transit operations, and shared-micro mobility.

The curbside management study is an idea resulting from the discussions with the Public Services Committee regarding paid on-street parking and designated parking areas for shared micro-mobility. The goal is to reduce tension between competing uses by analyzing existing conditions; exploring best practices; developing a series of policies and implementable recommendations; and designing pilot demonstration(s) to address the most pressing urban curbside management challenges. The study area includes Historic Dublin and the redeveloped areas of the Bridge Street District, particularly where on-street parking is in high demand. Please refer to Figure 2 for a map of the focus area.



The intent of the study is to develop an overarching plan to optimize the curbside. The plan will propose innovative and creative techniques and tools to document the occurrence, frequency, and delay caused by competing curbside uses. Staff anticipates the review of existing curbside regulations based on the City's zoning code and other relevant ordinances. Land-use and zoning maps shall be gathered to understand how land-uses relate to the curbside use trends.

The study will evaluate and categorize various solutions, and determine the feasibility of each. This evaluation will include, but is not limited to:

- Evaluate dynamic curbside management practices and how that might apply here in Dublin;
- Prioritize the movement of people and goods;
- Ability to manage conflicts and reduce delay;
- Protection of vulnerable users;
- Consideration of at least three technology alternatives as part of the solution/overall management plan and recommendations;
- Consider innovative projects, technology advances, connected and autonomous vehicle technology, changes in the supply chain and logistics industry, and changes in consumer behavior; and
- Look for trends and emerging technologies that will support the dynamic curbside atmosphere.

The study and associated deliverables are expected to be completed by July 1, 2023.

Discussion Request

Is the Public Services Committee supportive of the Curbside Management Program Study and what is the most pressing challenge to be considered?

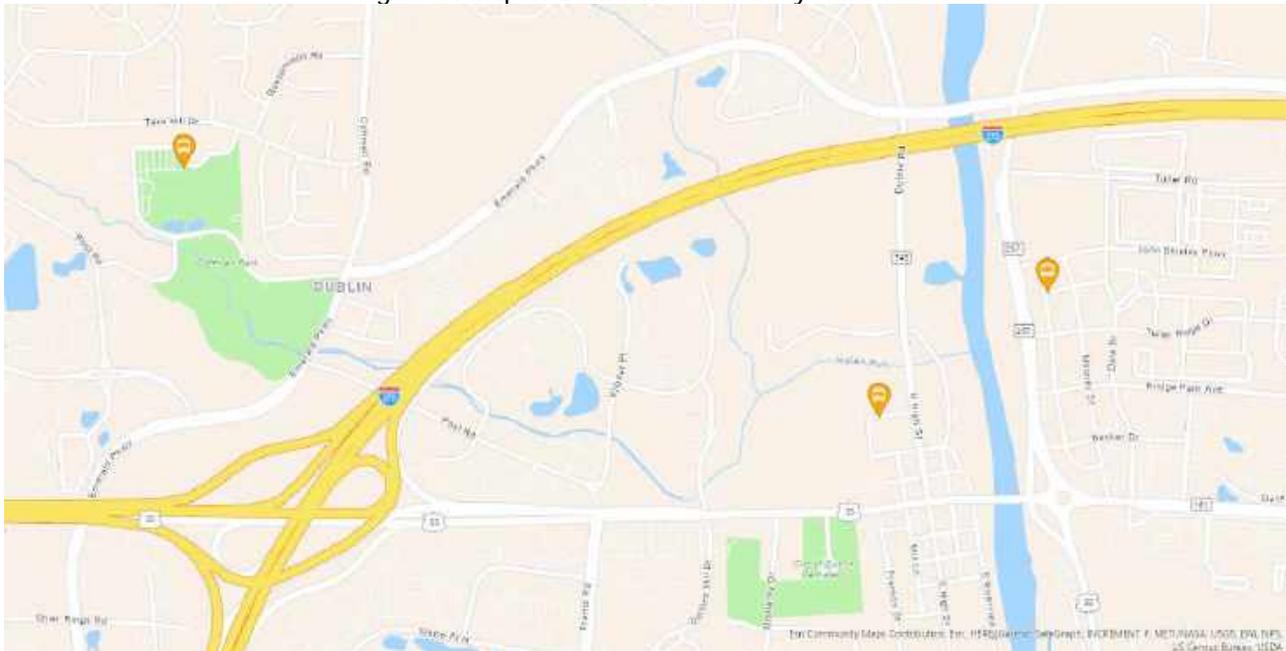
Mobility Hub Pilot

Mobility hubs designate a space to co-locate at least two modes of transportation, such as bike share and a Dublin Connector stop. They serve as transfer points between mobility options. Dublin Mobility Study Phase 2 recommended a series of mobility hub elements and typologies that could serve the Dublin community. This included considerations for hub design and programming, appropriate land use contexts, details on space requirements and infrastructure needs for each.

Dublin Staff has identified 19 proposed locations for mobility hubs in Dublin. Staff is prioritizing the installation of three hubs in ideal locations to introduce the mobility hub concept to the Dublin community. These locations combine significant opportunity to attract user activity with proximity to City or community facilities, and align with recent mobility studies such as the 2021 CoGo Northwest Expansion Study. The proposed locations for the mobility hub pilot were supported by the Public Service Committee at the May 3, 2021 meeting and include the following:

1. Dublin Community Recreation Center
2. Parking garage adjacent to the Columbus Metropolitan Library – Dublin Branch
3. North Market Bridge Park

Figure 3: Map of Locations for Mobility Hubs Pilot



The site-specific design concepts will be schematic in nature (10% level of design) and will be based on recent aerial imagery. The aerial imagery will be augmented with any available GIS or CADD base mapping. The site-specific design concepts will include COTA transit stops, car share parking, rideshare connection points, access via sidewalks, shared-use paths, and crossings. Additionally, the footprints of other elements will be identified, including bicycle parking, shared

bike/scooter staging areas, bike fix-it stations, bike rooms, and mobility kiosks. Potential locations of electrical and communicate service will be noted on the concepts.

An implementation guide for the installation of one of each prototype design will be developed during the pilot. The guide will include procurement, construction, installation, and maintenance costs and logistics for each hub concept design, based on the specifics of the proposed site and location of each proposed hub element. This will include capital and operational costs for both the infrastructure and amenities included in each hub design. Furthermore, the mobility hub pilot with the associated implementation guide will provide a foundation for a mobility placemaking plan, which will focus on placemaking components at mobility hubs.

Discussion Request

In the next phase of mobility hub expansion, should staff focus on civic destinations or other points of interest in Dublin, like recreation or entertainment?

Discussion Topics

1. What are the next opportunities for service expansions regarding the Dublin Connector?
2. Is the Public Services Committee supportive of the Curbside Management Program Study and what is the most pressing challenge to be considered?
3. In the next phase of mobility hub expansion, should staff focus on civic destinations or other points of interest in Dublin, like recreation or entertainment?
4. Other considerations.